

AGENDA FOR

PLANNING CONTROL COMMITTEE

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To: All Members of Planning Control Committee

Councillors : G McGill (Chair), S Briggs, T Cummings, S Haroon, J Harris, M Hayes, G Keeley, J Mason, B Mortenson, I Schofield, C Tegolo, K Thomas and D.Vernon

Dear Member/Colleague

Planning Control Committee

You are invited to attend a meeting of the Planning Control Committee which will be held as follows:-

Date:	Tuesday, 28 July 2020
Place:	Virtual Meeting via Microsoft Teams
Time:	7.00 pm
Briefing Facilities:	If Opposition Members and Co-opted Members require briefing on any particular item on the Agenda, the appropriate Director/Senior Officer originating the related report should be contacted.
Notes:	

AGENDA

1 APOLOGIES FOR ABSENCE

2 DECLARATIONS OF INTEREST

Members of the Planning Control Committee are asked to consider whether they have an interest in any of the matters on the Agenda and, if so, to formally declare that interest.

3 MINUTES OF THE MEETING HELD ON 23RD JUNE 2020 (*Pages 1 - 4*)

Minutes of the meeting held on Tuesday the 23rd June 2020 are attached.

4 PLANNING APPLICATIONS (*Pages 5 - 214*)

Reports attached.

5 DELEGATED DECISIONS (*Pages 215 - 228*)

A Report from the Head of Development Management on all delegated planning decisions since the last meeting of the Planning Control Committee is attached.

6 PLANNING APPEALS (*Pages 229 - 232*)

A report from the Head of Development Management on all Planning Appeals decisions since the last meeting of the Planning Control Committee is attached.

7 URGENT BUSINESS

Any other business which by reason of special circumstances the Chair agrees may be considered as a matter of urgency.

Supplementary information produced after publication of the agenda and released before the meeting will be included in the full agenda pack for the meeting on the Council's website.

Minutes of: PLANNING CONTROL COMMITTEE

Date of Meeting: 23 June 2020

Present: Councillor G McGill (in the Chair)
Councillors S Briggs, T Cummings, J Harris, M Hayes,
J Mason, B Mortenson, I Schofield, C Tegolo and D.Vernon

Also in attendance: Councillors R Gold and A Quinn.

Public Attendance: 2 members of the public were present virtually.

Apologies for Absence: Councillors S Haroon, G Keeley and K Thomas.

PCC.1 APOLOGIES FOR ABSENCE

Apologies for absence were submitted on behalf of Councillors S Haroon, G Keeley and K Thomas.

PCC.2 DECLARATIONS OF INTEREST

There were no declarations of interest made at the meeting.

PCC.3 MINUTES OF THE MEETING HELD ON 26TH MAY 2020

Whilst making no amendment to the 26th May minutes, Councillor Harris requested that her view be recorded that the decision to deal with application number 64955 (Margaret Haes Riding Centre, Moor Road, Ramsbottom, Bury, BL8 4NX) at the May meeting should not have happened due to the decision already being deferred at the February meeting for a site visit.

Delegated decision:

That the Minutes of the meeting held on the 26th May 2020 be approved as a correct record and signed by the Chair.

PCC.4 PLANNING APPLICATIONS

A report from the Development Manager was submitted in relation to various applications for planning permission.

Following advice from the Council's Monitoring Officer, it had not been possible to carry out actual site visits this month. Therefore under item 4 of the agenda listed as planning applications, items 1 and 2 had been removed from the business of the meeting:-

Land to the south of Moorbottom Road, Holcombe, Bury, BL8 4NS
Application No. 64786 Erection of agricultural storage building, retention of field shelter, pig arc and replacement gate & gate post

Margaret Haes Riding Centre, Moor Road, Ramsbottom, Bury, BL8 4NX Application No. 64955 Retention of welfare unit with associated landscaping to form welfare and security accommodation (Temporary consent for 5 years)

Supplementary information was also submitted in respect of application numbers 64980, 65114 and 65456.

The Committee heard representations from applicants and/or objectors in respect of the applications submitted. This was limited to three minutes for each speaker.

Councillors R Gold and A Quinn spoke as a Ward Representative in relation to planning application 65456.

Delegated decisions:

1. That the Committee **Approve with Conditions** the following applications in accordance with the reasons put forward by the Development Manager in the report and any supplementary information submitted and subject to the conditions included:-

64980 Pavetec Ltd, 8-10 Spring Vale Street, Tottington, Bury, BL8 3LR

Removal of existing buildings and erection of 2 no. units for a mixed use comprising B1 office and B8 storage.

65114 Ivy House, Bolton Road West, Ramsbottom, Bury, BL0 9RN
Erection of 1 no. dwelling.

65261 38 Deyne Avenue, Prestwich, Manchester, M25 1EJ

Construction of 1 no. dwelling at the side and forming car parking spaces to the rear.

65456 45 Bury Old Road, Prestwich, Manchester, M25 0EY

Demolition of existing dwelling and erection of a post graduate study centre (Class D1) and formation of new vehicular access from Kings Road.

PCC.5 DELEGATED DECISIONS

A report from the Development Manager was submitted listing all recent planning application decisions made by Officers using delegated powers.

Delegated decision:

That the report and appendices be noted.

PCC.6 PLANNING APPEALS

A report from the Development Manager was submitted listing all recent Planning Appeal decisions since the last meeting of the Planning Control Committee.

Delegated decision:

That the report and appendices be noted.

PCC.7 URGENT BUSINESS

No urgent business was reported.

COUNCILLOR G MCGILL
Chair

(Note: The meeting started at 7.00pm and ended at 7.59pm)

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Title	Planning Applications
To:	Planning Control Committee
On:	28 July 2020
By:	Development Manager
Status:	For Publication

Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

This report has the following implications

Township Forum/ Ward: Identified in each case.

Policy: Identified in each case.

Resources: Not generally applicable.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:
The elimination of discrimination, harassment and victimisation;
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

Human Rights: All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

Development Manager

Background Documents

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

01	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 64786
	Location: Land to the south of Moorbottom Road, Holcombe, Bury, BL8 4NS	
	Proposal: Erection of agricultural storage building, retention of field shelter, pig arc and replacement gate & gate post	
	Recommendation: Approve with Conditions	Site Visit: Y
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02	Township Forum - Ward: Radcliffe - West	App No. 64949
	Location: Asda Stores Ltd, Pilkington Way, Radcliffe, Manchester, M26 3DA	
	Proposal: Erection of drive thru coffee shop	
	Recommendation: Approve with Conditions	Site Visit: N
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03	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 64955
	Location: Margaret Haes Riding Centre, Moor Road, Ramsbottom, Bury, BL8 4NX	
	Proposal: Retention of welfare unit with associated landscaping to form welfare and security accommodation (Temporary consent for 5 years)	
	Recommendation: Approve with Conditions	Site Visit: Y
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04	Township Forum - Ward: Bury West - Elton	App No. 65293
	Location: Land at Green Street, Bury, BL8 1TF	
	Proposal: Erection of 3 no. dwellings including associated parking and groundworks	
	Recommendation: Approve with Conditions	Site Visit: N
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05	Township Forum - Ward: Radcliffe - East	App No. 65354
	Location: Radcliffe Metrolink Car Park, Spring Lane, Radcliffe, Manchester, M26 2ST	
	Proposal: The erection of an additional deck and ramp to form a second floor to car park, providing an additional 115 no. spaces; landscaping scheme and lighting	
	Recommendation: Approve with Conditions	Site Visit: N
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06	Township Forum - Ward: Ramsbottom + Tottington - Tottington	App No. 65459
	Location: Land off Claybank Drive, (off Victoria Street), Tottington, Bury	
	Proposal: Variation of condition no. 2 (approved plans) of planning permission 63275 to amend house on Plot 1 (north) by raising roof eaves/ridge by 600mm, addition of pitched roof dormer to front elevation, additional windows to attic space and removal of hipped end to roof on west elevation.	
	Recommendation: Approve with Conditions	Site Visit: N
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07	Township Forum - Ward: Whitefield + Unsworth - Pilkington Park	App No. 65465
	Location: Whitefield Metrolink, Stanley Road, Whitefield, Manchester, M45 8AB	
	Proposal: Reconfiguration of the bus turning head and the erection of an additional deck and ramp to form a second floor to car park, providing in total an additional 123 no. spaces; landscaping scheme and lightning	
	Recommendation: Approve with Conditions	Site Visit: N
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08	Township Forum - Ward: Bury East - Redvales	App No. 65469
	Location: Land adjacent to 23 Meadway, Bury, BL9 9TY	
	Proposal: Reserved matters for layout, scale, appearance and landscaping approval following grant of Outline approval ref. 61369 for 2 no. detached dwellings	
	Recommendation: Approve with Conditions	Site Visit: N
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09	Township Forum - Ward: Radcliffe - North	App No. 65569
	Location: 31 Brookfield Avenue, Radcliffe, Bolton, BL2 5QH	
	Proposal: Change of use from dwellinghouse (Class C3) to residential care home (Class C2) with additional parking.	
	Recommendation: Approve with Conditions	Site Visit: N
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10	Township Forum - Ward: Prestwich - St Mary's	App No. 65589
	Location: Land at rear of 27 Duckworth Road, Prestwich, Manchester, M25 9GF	
	Proposal: Variation of condition 2 (approved plans) of planning permission 63374 to remove ash tree (T2)	
	Recommendation: Approve with Conditions	Site Visit: N
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Ward: Ramsbottom and Tottington -
Ramsbottom

Item 01

Applicant: Mr & Mrs M Unsworth

Location: Land to the south of Moorbottom Road, Holcombe, Bury, BL8 4NS

Proposal: Erection of agricultural storage building, retention of field shelter, pig arc and replacement gate & gate post

Application Ref: 64786/Full

Target Date: 27/01/2020

Recommendation: Approve with Conditions

The application was postponed from the March and April 2020 Planning Control Committee Meetings. In response to the emergency Government instructions on Covid-19.

The application was deferred at the May 2020 and June 2020 Planning Control Committee meeting to carry out a site visit.

Description

The application site relates to a field which is used for the grazing of animals and is located on a parcel of land which is located in the Green Belt, Special Landscape Area and West Pennine Moors and is within the Holcombe Conservation Area.

The field comprises approximately 4 acres of land in a triangular form which is set on a hillside and forms the lower part of the hill which is locally known as Holcombe Hill, with Peel Monument, a Grade II Listed Building at the summit.

The access to the field is taken from the northern most corner of the field which forms the meeting of three roads, Moorbottom Road, Holcombe Old Road and Cross Lane. Holcombe Old Road runs along the eastern boundary of the field and is at a much lower level to the site. The road comprises a cobbled surface and is used by walkers and residents who live along this stretch, and where there is a row of stone built terrace cottages.

Moorbottom Road forms the western boundary and the highest part of the field and is separated by a dry stone wall. It is an unmade track used by walkers although there are some properties located along this stretch.

Cross Lane leads from the village centre along which are residential dwellings, some of a Listed status or are Non-Designated Heritage Assets (NDHA's).

The applicant currently keeps sheep and alpacas on the field and are seeking to extend the herd of Alpacas with two breeders. Also on the field is a small pig arc and shed located at the lower end of the field towards the southern boundary which are used to provide some shelter for the herd, albeit very limited. These structures do not have planning permission. The applicant also has some machinery and equipment which are needed to facilitate the keeping of the animals, as well as foodstuffs, but nowhere in which to store these, resulting in them being kept in various remote locations open to inclement weather and theft.

The application proposes the erection of an agricultural building together with the retention and relocation of the shed and pig arc. In addition, a galvanised steel gate has also been erected to the field access to the north. It is now proposed to replace this with a timber gate.

Application details

Proposed - Agricultural building - Constructed of timber boarding and barn style door with a grey sheeted monopitch roof and overhang to the frontage. The build would be 12.2m long, 6.1m wide and height between 3-4m (floor area of 74.4 sqm).

The build would be positioned against the western boundary and the stone wall, approximately half way across the field boundary. The building would provide secure and weather tight storage for equipment, machinery and foodstuffs and when needed, for animal welfare purposes during the lambing season.

Pig arc - Constructed of wooden panelled ends and a corrugated tin surround and measures 3m long, 2.5m wide and 1.2m high. Used as an animal shelter in inclement weather.

Shed - A timber boarded shed with a corrugated roof and dimensions of 3.6m long, 2.4m wide and 2.1m high. Used as an animal shelter in inclement weather.

Access gate - Located at the field access to the north. It is proposed to replace the steel gate with a timber 5 bar gate.

It is proposed to perform a minor cut and fill operation so that the agricultural building, pig arc and shed would be set into the ground and levels changed to the rear and supported by the erection of a retaining wall to be formed around this area. In front of the structures, approximately 10m further down the field to the east, it is proposed to create a minor bund or earth mound which would be formed to follow the natural contours of the field and which would partly screen the structures from some views.

The application seeks full planning permission for the erection of the agricultural building and for the re-siting and retention of the pig arc, timber shed and gate.

Relevant Planning History

02312/E - Agricultural Building

Publicity

14 letters sent on 9/12/2019 to properties at Hill End Farm, Moorbottom Road, 83-109 Holcombe Old Road and 50, 52 Cross Lane.

Site notice posted 18/12/2019.

Site notice posted 24/4/2020

Press advert in the Bury Times 12/12/2019.

6 letters of objection received from Hey House Cottage, Branch Road, 1 Lumb Carr Road, c/o 296 Holcombe Old Road, 103 Holcombe Old Road (x2), Higher Barn Farm, 155 Lumb Carr Road.

The following issues were raised:

- The proposed application is not in keeping with the Holcombe Conservation Area and does not enhance the natural beauty and character of the area. An eyesore in full view of Holcombe Village.
- This area of land used to consist of one large field where sheep grazed. There were no buildings and access was via a large wooden gate. It was a pleasing area of green. The field has now been split into three with iron gates in between, the wooden gate has been replaced with an iron gate, and the entrance widened necessitating the part demolition of the ancient stone walls either side. There are now dustbins clearly visible in the middle of the field. The area looks unkempt, even industrial, and there are considerable areas of mud resulting in the alpacas in particular looking as if they have had a mud bath.
- The proposed new building will be visible from both Moorbottom Rd and Holcombe Old

Rd and the roof lights within the building will be clearly visible at night and from Holcombe Hill.

- Negative impacts from other approval granted (not specified by the objector).
- Previously a simple pasture, such sheds, including a small wind turbine (I), are annoyingly sited in the centre of the field.
- Considerable unauthorised development has already taken place in the field bounded by Moorbottom Road and Holcombe Old Road which used to be open grass land grazed by sheep
- This is of considerable size and will be located adjacent to Moorbottom Road. This will ultimately need excavation of the bank that rises steeply to Moorbottom Road and will scar the landscape. The storage building will be clearly visible from the surrounding area and will be yet another wooden structure at the foot of Holcombe Hill. The huge barn at Hill End next door was erected 9 years ago despite considerable local opposition and has remained largely empty and unused and is a blight on the previously open landscape. The intended purpose of the new development is to breed alpacas and to store farm machinery. This amounts to agricultural development of a small field on an industrial scale. This seems to be wholly at odds with what a Conservation Area is for.
- The planning application is essentially for a new extensive farm business on a previously completely undeveloped small field at the centre of Holcombe Conservation Area. How can this possibly enhance the landscape character and nature conservation value of the countryside?
- The 'openness' of the landscape is already seriously compromised with a number of intrusive fences which are not drawn on the applicant's plan.
- The 'pig arc' is simply ludicrous, dominating the field centre with its shiny roof.
- The barn building may damage the safety of the wall
- It must be envisaged that the development would not function effectively medium to long term without the requirements for a track and surfacing
- The applicant is looking to expand a farming business - not appropriate to this site
- Needs to be demonstrated that the development complies with Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas Act) 190.
- The development would fall in direct line with the first open view for visitors of the front of the hill and the Tower.
- The builds are not of high design standard
- The agricultural building would blot out part of the dry stone wall which is characteristic to the area.
- Steep slopes are characteristic of the area - the development would detract from that
- The red edge does not include all the land needed..
- The blue edge has been drawn to include land not in the applicants ownership
- application form states there are no waste arrangements
- Elevations of the building show a central ridge but the side elevations are mono pitch
- Conflicts with UDP policies and the NPPF.
- Does not preserve or enhance the Conservation Area and is harmful to the setting of Listed Buildings.
- Surely it would make better business sense to establish this kind of business within a small holding where land is less expensive and in an area that does not have these constraints.
- Though this is an application for agricultural buildings and this seems innocent in itself to house some beautiful animals and to help with storage of agricultural equipment, the utilities installed and the long term intention should be considered by the committee.

Comments by applicant to objections raised - 8/1/20 :

- The Heritage Asset is the Conservation Area.
- Within the Holcombe Conservation Area Appraisal, there is an acknowledgement that agriculture is the main economic activity in the local area. The proposal therefore accords with the main economic activity in the locality and we are of the opinion that there will be no impact.
- 'The character of the Conservation Area largely relies upon the existing pattern of land uses, while recognizing that the nature of the use of agricultural land may need to adapt to ensure economic viability, exemplified by the number of stables within the

Conservation Area.'

- The Heritage Statement refers to the need to adapt through the use of stables which would have a larger impact on the Conservation Area than the proposed agricultural building, as a result the proposed agricultural building will have no impact on the heritage asset.
- The Bridleway will not be affected. Due to the use of Cut and Fill, the roofline of the building will be below the wall along Moorbottom Road, and will not impede the view from the Bridleway.
- The building is not shown as having a dual pitched roof on the drawings. The Western Elevation on the proposed elevation plan is still showing a monopitch roof as described, giving a visual representation of the roof lights in order to maximise natural light within the building.
- It is of the opinion that the proposal is in keeping with other gates located in the conservation area and will therefore have no impact upon it as a result.

Representation received from c/o Dawes Bank, Holcombe in response to the applicants statement:

- The Heritage Asset is also the Listed Holcombe Tower - overlooked by the applicant
- This proposal does not have 18th or 19th C character
- The application submission singularly fails to demonstrate "no impact". In fact, quite the opposite
- This application goes considerably beyond a stable and by its own admission is about setting up a base for the entire farm business holding - not merely a field shelter for livestock which could be effected very readily by a potentially modest two or three-sided roofed shelter in say a corner position.
- The proposal includes a 12m x 6m x 4m high building - cut into an engineered shelf in the rising ground. Why would a stables have a larger impact? There is no logic to the presented conclusion that the application has zero impact - this doesn't make sense...
- The submitted drawing has at its centre a roof plan specifically annotated with the words 'ridgeline' along the centre - it is this which is causing the ambiguity clearly indicating a dual pitched roof. If this is not intended this needs to be corrected.
- A timber gate, kept to original size, is an important way of keeping the humble heritage character. Why for example is the now widened gate width actually necessary? Additionally, the development at this gateway has already gone beyond mere replacement of a gate but involves a cut away level with setts stockpiled in readiness for laying a domestic style driveway surface - as opposed to humble field entrance -and electricity and telecommunications ducting laid in.
- Clearly substantially more information could be provided re cut-and-fill dimensions and the gateway - and how the entrance to the 12m shed on steeply sloping ground outside the doorway would be made practical and serviceable could be provided. However, I don't see that any further detail would overcome the profound in-principle objection to this scheme.

17 letters of support received from Nos 11,21 (x2) Cliff Mount, 9 Burrswood Avenue, Unit 3 Fernhill Street, 35 Lathom street, 75 Manchester road, 38 Haslam Street, 17 Rupert Street (x2), 15 Bridgewater Close, 39 Valley Mill lane, 78 Church Street, 8 Lepp Street, 35 Lathom Street, 28 Whitehead Crescent, 9 Bond Street, Springside Cottages.

The following issues of support were raised:

- The owners are trying to make an agricultural business work in a Conservation Area. I fully agree with the conclusions in the Design and Access Statement that the buildings are critical and necessary to effectively run an agricultural business and even more importantly for the safe welfare of the animals, especially with lambing season coming up.
- The buildings and structures in the photos are sympathetic in design and are in keeping with existing structures, also in the Conservation Area and on adjoining properties.
- The planned building is in keeping with the surroundings and will add to the rural character of the area. Not only that, but it will enable the keeping of livestock that will enhance the experience of visitors and walkers around Holcombe. The planned building

- will not block any views or cause disruption to local residents.
- Regularly walk this area and children love to see the animals
- Aware of the difficulties to manage animals with the level of rainfall in the area
- Evident the owners care for their animals and welfare
- disappointed in those who are trying to obstruct this going ahead, this structure is for the benefit of the animals i think they need to remember this.
- What makes people think that they are running a business from this land. The applicant's are purely hobby farmers.
- If anything the area would be enhanced.

Revised plans received to relocate the pig shelter and field shelter, details of cut and fill works and formation of a bund, elevation of agricultural building amended. Those who were originally notified of the application and any additional objectors were notified of the revisions by letter on 24/2/20.

One letter of objection and one comment received from No 296 Holcombe Old Road with the following issues:

- Whilst understanding the consolidation of buildings, the necessary excavation has been dramatically enlarge to achieve it. This is a very engineered approach - a technical solution but not good design.
- Details of the retaining structure work would be needed.
- The main building is less about providing weather shelter but more about establishing a whole farm operational base - wholly unreasonable to have expectations of a complete farm unit.
- Inevitable need for an access track and hardstanding surface to the entrance. Needs to be shown on the plans. Otherwise will be an enforcement nightmare.
- Should the red edge include the access track? Expect highways to be consulted.
- Service connections to the build bearing in mind the TPO's.
- Access works to the entrance gateway - still more setts are accumulating.
- Confirm this will be site visited by Committee.
- The field is grossly over stocked - am advised this is a field for 8 sheep only.
- Has Natural England been consulted as previously?
- The revised location plan does not fairly or accurately represent the spread of the development. The buildings are set further into the field and the bund is so much further out it is not even fully in the red edge.
- Can I presume that you will ask the applicant to revise the location plan and confirm the scale of the section plans as these are not actually marked with a scale.

Further letter of objection received on 10/3/20 from c/o Dawes Bank, Holcombe. Issues raised are:

- According to the Land Registry, the applicant has paid a significant amount of money for the land which suggests a potential level of intent which seem to go beyond any farming endeavour proportionate to the small area of relatively poor agricultural land they have purchased. Total area of land owned by the applicant is 1.4ha or 3.5 acres.
- In account of the cross section profiles and topographical drawings C/3956 1/1 and 1/2, the drawings show significant land engineering work to create a levelled area of c. 270 sqm, involves excavating 390 sqm of substrata.
- An exposed vertical face of 3m by 26m long would result and require a retaining wall. No details are given of a newly formed yard which would be required.
- The bund would cut into the openness of the Green Belt
- The works required would significantly affect the Green Belt and go beyond what would be appropriate and necessary.
- The agricultural justification by the applicant does not give a reasonable assessment for the scale of development involved.
- Lack of clarity for the purpose of keeping Alpacas.
- Large engineered excavation and bund would be completely out of place with the topography and land form for the location which would be at odds with the Conservation Area and would not enhance the character of appearance.
- The site falls centrally in line with key views of the Listed Holcombe Tower causing

harm to the heritage significance.

- Statutory duty of the LPA under Section 66 of the planning (Listed Building and Conservation areas) Act 1990 to consider impacts on historic interest and heritage assets. There is no convincing justification for the development.
- There is no identified public benefit for the development.
- Impacts and harm would be caused to the setting of Hill End farm - a NDHA.
- The development does not represent sustainable development.
- Conclusion - There are no public benefits from this proposal; any economic contribution would be negligible as would any contribution to national agricultural production. The disbenefits of this proposal are by contrast specific and several and in each case are significant and demonstrable – adversely affecting Green Belt and Heritage, including two designated heritage assets and one non-designated; there is also a case that even without the heritage designations these proposals are unduly harmful to landscape and nor do they represent good design. The scheme is contrary to the Borough's planning policies in each of these respects.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Borough Engineer - Drainage Section - No response received.

Environmental Health - Contaminated Land - No comments to make

Public Rights of Way Officer - No objection

Conservation Officer - No objection.

United Utilities (Water and waste) - No response received.

Natural England - Confirmed they would be satisfied with a condition for the submission of a drainage scheme. Condition 9 for the submission of a drainage scheme has been recommended.

Historic England - No objection

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN2/3	Listed Buildings
EN5/1	New Development and Flood Risk
EN8/1	Tree Preservation Orders
EN9/1	Special Landscape Areas
OL1	Green Belt
OL1/2	New Buildings in the Green Belt
OL4/5	Agricultural Development
OL7/2	West Pennine Moors
EN5/1	New Development and Flood Risk
EN7/3	Water Pollution
EN7/5	Waste Water Management
SPD8	DC Policy Guidance Note 8 - New Buildings in the Green Belt

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight

arising from the NPPF where it would otherwise be specifically mentioned.

Green Belt Policies and Principles

Paragraphs 143 and 144 of the NPPF state that inappropriate development in the Green Belt is by definition, harmful and should not be approved except in Very Special Circumstances (VSC). Planning Authorities should ensure that substantial weight is given to any harm in the Green Belt. VSC will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Paragraph 145 regards the construction of new buildings as inappropriate in the Green Belt. Exceptions to this include bullet point a) for buildings for agriculture and forestry.

Paragraph 146 states that certain other forms of development are also not inappropriate in the Green Belt provided that they preserve its openness and do not conflict with the purposes of including land in it. One such development is point b) engineering operations.

UDP Policy OL4/5 - Agricultural development requires buildings or structures to be sited in a manner as to minimise their visual impact on the landscape with regard to height, materials, landform and landscaping, should relate well to existing farm buildings and not have an unacceptable impact on amenity.

UDP Policy EN9/1 - Special Landscape Areas states that development in such areas will be strictly controlled and required to be sympathetic to its surroundings in terms of visual impact. High standards of design, siting and landscaping will be expected. Unduly obtrusive development will not be permitted in such areas.

SPD8 provides more detailed advice on new buildings in the Green Belt and in particular with regards to the siting, position, scale, materials and form of a development.

The application seeks the erection of an agricultural building and retention of the two existing field shelters for the purposes of housing the applicant's feedstuff and agricultural equipment in association with the keeping and grazing of sheep and alpacas.

The Appeal Decision at Twine Valley Farm, ref APP/T4210/W/16/3151468 for development of agricultural building for housing livestock, found that the NPPF sets no limits on the scale of agricultural buildings or requires evidence of its necessity. It is also stated that unlike some other development types stated in Paragraphs 89 and 90 of the NPPF, the effects of agricultural buildings on the openness or purposes of the Green Belt are not relevant to a consideration of whether or not they are inappropriate.

As it is one of the exceptions given in paragraph 89 it does not therefore have a harmful effect on the openness or purposes of the Green Belt. In this regard also the scheme would not conflict with OL1/2 of the Bury Unitary Development Plan.

Within the context of the current NPPF, the proposed development would constitute appropriate development in the Green Belt and would comply with the principles of bullet point a) of chapter 145 of the NPPF and with Bury UDP Policy OL1/2.

The cut and fill works would comprise an engineering operation. The proposed works would enable the agricultural buildings to sit within the landform and in doing so would soften the visual impact to ensure that openness and the character of the Green Belt would be preserved, and as described in detail in the 'Siting, design and visual impact' section below. The proposed works would not conflict with the purposes of including land within it. As such, it is considered that the proposed development would comply with point b) of Paragraph 146 of the NPPF.

The proposed bund is also considered as an engineering operation under para 146 b). The bund would be formed from earth and grass excavated from the cut and fill works and envisaged to be sculpted and moulded to emulate the gentle undulating and rolling form of the existing field. It is not intended for the bund to appear 'manufactured' or artificial within the field, or require

such significant engineering operations which would be adversely detrimental to the character or appearance of the Green Belt.

It is therefore considered that the proposed development would not impact on the openness of the Green belt and would not conflict with the purposes of including land within it, and as such considered to comply with para 146 b).

Heritage and Conservation Policies and Principles

Section 66 of the Planning (Listed Building and Conservation Areas) Act 1990 requires LPA's *in the exercise of their planning functions to have regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.*

Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires Local Planning Authorities in the exercise of their planning functions to pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area.

Paragraph 189 of the NPPF states that in determining applications, LPA's should require an applicant to describe the significance of any heritage assets affected, including any contribution to their setting. The level of detail should be proportionate to the asset's importance and no more than is sufficient to understand the potential impact of the proposal on their significance.

Paragraph 190 states that Local Planning Authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

In paragraph 192, LPA's should take account of:

- the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraphs 193 - 194 states that when considering the impact of a proposed development on a heritage asset, great weight should be given to the asset's conservation, irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance. Any harm should require clear and convincing justification.

Paragraph 195 states that where a proposed development will lead to substantial harm to a designated heritage asset, LPA's should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve public benefits. Paragraph 196 states that where a development will lead to less than substantial harm to the significance of a designated heritage asset, the harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

UDP Policies EN2/1 - Character of Conservation Areas and EN2/2 - Conservation Area Control seeks to preserve or enhance the special character or appearance of the area. In considering proposals, regard will be given to factors including the nature of the development in terms of its design and appearance and relationship to the visual qualities of the area.

EN2/3 - Listed Buildings - Actively seeks to safeguard the character and setting of Listed Buildings by not permitting works, alterations or changes of use which would have a detrimental effect on their historical or architectural character and features.

Conservation Area

In this case, the heritage asset is the Holcombe Conservation Area. The Holcombe

Conservation Area Appraisal describes the area as rural in character with settlements comprising a series of scattered hamlets. Key characteristics are identified as steeply sloping land forms and dispersed groupings of agricultural and residential buildings, dating from the 18th and 19th centuries, reflecting the predominance of agriculture as the main economic activity. The proposed site is a triangular piece of land which forms part of the eastern slope of Holcombe Hill. This part of the conservation area derives its significance from the open sloping rural landscape.

In accordance with paragraph 189 of the NPPF, the applicant is required to describe the significance of any heritage assets affected. The submitted heritage assessment adequately describes the significance of the conservation area and the impact of the proposal on that significance.

Turning to Paragraph 195 of the NPPF, and assessment of the degree of harm caused, or otherwise, in terms of the siting of the development, the proposed agricultural building would be sited against a dry stone boundary wall, behind which is a line of trees and the side of the hill which slopes steeply up to Holcombe Tower.

S336 of the TCPA 1990 provides the definition of agriculture to include the following - 'agriculture includes horticulture, fruit growing, seed growing, dairy farming, the breeding and keeping of livestock (including any creature kept for the production of food, wool, skins or fur, or for the purpose of its use in the farming of land), the use of land as grazing land, meadow land, osier land, market gardens and nursery grounds, and the use of land for woodlands where that use is ancillary to the farming of land for other agricultural purposes, and "agricultural" shall be construed accordingly. The land subject to this application is being used for an agricultural purpose, used by animals in connection with an agricultural purpose and is within Green Belt. By definition the use of the land is considered appropriate (Paragraph 145 a) of the NPPF) and is not at question. As this is the case, the applicant is not required to demonstrate need for the use of the land.

It follows that the use of land for the purpose described animals will require welfare facilities and shelter in some shape or form. Understanding the Conservation Area allocation and its value, there is a required need to balance public benefits of a proposal as set out within para 196 of the NPPF.

In appearance, the agricultural build would be a simple timber construction with a mono pitch roof and which is of a type which is commonly found in such rural settings. Likewise, the existing timber shed and low level small curved pig arc (subject to re-painting a less strident colour) are also considered characteristic to the rural setting. Following advice from the LPA, the applicant has also agreed to replace the steel gate (which was erected without planning consent) with a 5 bar timber gate. Details are to be secured by condition.

The cut and fill exercise to reduce visibility and the formation of the earth mound would further mitigate visual impacts. The pig arc and shed, re-located from the centre of the field next to the agricultural building, would sit below the wall, lessening the sprawl of built development.

It is considered that the proposed development would not represent substantial harm or total loss of the significance of the conservation area within the terms of paragraph 195 of the NPPF as the development would not relate to the whole of the conservation area or its total loss. The pig arc and field shelter are ephemeral and reversible and as such summarily considered not to justify refusal. As such, it is considered that the development would represent less than substantial harm to the significance of the conservation area.

Paragraph 194 of the NPPF requires that any harm, (concluded above as being less than substantial), should require clear and convincing justification. In occupying the land for the accepted purposes of agriculture, the livestock consisting of breeding sheep do require housing for husbandry purposes especially at lambing time as well as other animals that may be kept there. This is clearly evidenced in a letter from a specialist animal and equine veterinary surgeon/practice. As such, in Conservation principles, this would represent clear and convincing justification for the proposal, including the use of the land for agricultural purposes in any event, and would comply with paragraph 194 of the NPPF.

Therefore, it is a requirement of paragraphs 194 and 195 of the NPPF to assess the amount of harm, if any will be caused to the significance of the heritage asset or its setting. Total loss of significance would represent more than substantial harm and should be refused unless exceptional or wholly exceptional justification can be put forward. As the proposal does not represent total loss of significance, it represents less than substantial harm (Para 196 of the NPPF). That harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use. This assessment has been carried out in the above report and concluded that while there is harm there is less than substantial harm given the merits of this particular case which would justify in allowing the proposal. In carrying out the statutory duty in Section 72 (to pay **special attention** to the desirability of preserving or enhancing the character or appearance of that area by attaching considerable importance and weight to that desirability) the appropriate assessment has been made and conclusion reached.

Consideration should now be given to Paragraph 196 of the NPPF and in weighing the development against the public benefits.

Public benefits may follow from many developments and could be anything that delivers economic, social or environmental objectives as stated in the NPPF and is for each application to consider. Clearly in this instance, the siting of structures for welfare purposes has to be balanced against the Conservation Area principles and tests, and public interest can also include the perception of land, visual amenity, history, views or outlook and setting, to name a few. Therefore in accepting the principle of animals being in this location, there is a need to accept that welfare of those animals must also weigh in that balance. Therefore the location and appearance of the chattels, structures and other features required must form part of that balance.

The current location of existing structures are sited in the middle of the field. This location is open, public and visible to many from many vantage points. The application shows that the structures would be relocated to the northerly side of the field, within a minor cut into the embankment and with the creation of a bund in front of them.

The relocation in balance and consideration of public benefit would mean that the total collection of the structures would be retained to a small enveloped area of less than 300sqm at the edge of the field, where visibility of this structures would be oblique and slight from Moorbottom Road and screened and largely shielded from view from Holcombe Old Road. Thereby in a public benefit consideration, the proposed location would be far less sensitive.

The development represents limited economic development, which should be encouraged and where development is appropriately managed in a conservation area, this can preserve the character and appearance of the conservation area, ensuring that in the planning balance, impact is neutral and provides a public benefit in conservation terms. This recognises that planning policy and law is there to facilitate the management of change and not to bar it. By managing change to heritage assets appropriately, the aims to conserve significance and future is in the public benefit.

The use of conditions as set out within the NPPG where they pass the tests of usage, can ensure that development can take place in an appropriate way and can maintain relationships in sensitive areas for many reasons, including visual amenity. The recommendation includes the use of conditions that would control and mitigate for visual impacts of the development and restrict further development by the removal of permitted development rights (condition 7) and can also be used to reinstate the unlawful works to the gate and widened entrance. The condition 6 relates to the gate requiring it to be reinstated to a timber 5 bar gate and to reinstate the original width of the opening using matching construction methods and materials to the field boundary wall.

With the mitigation proposed, it is considered the development would not interrupt sensitive views across this part of the rural landscape and as such its effect on the character and appearance of the conservation area would be neutral in the planning balance. It is considered that in this case, the proposed development would be appropriately and acceptably be managed

and impacts on the character of the conservation area would not be significant and therefore would be in accordance with Paragraph 196 of the NPPF.

Setting of Listed Buildings - Holcombe Tower

Holcombe Tower is only partly visible from Holcombe Old Road. Whilst some of the proposed development would be visible from Holcombe Old Road, the topography of the landscape and intervening forms are such that views of the Tower would not be interrupted. The existing views from the various footpaths to the Tower would also not be hindered by the proposal and as such the way in which the setting of the Tower is experienced in its approach via the various footpaths would not be affected.

As such it is considered that the setting of the Tower would not be affected by the siting of the development and would comply with UDP Policies and the principles of the NPPF.

Setting of Hill End Farm - Non-Designated Heritage Asset (NDHA)

The proposed siting of the buildings when viewed from the various footpaths would not interrupt views of Hill End Farm and as such the way in which the setting of Hill End Farm would be experienced in its approach from the footpaths would not be affected. From Holcombe Old Road, the proposed development would not obstruct or interrupt views of the Farm. As such, it is considered its setting would be preserved.

Conclusion on Conservation/Listed Building issues

Conservation policies recognise that change takes place and seeks to guide the way changes are made so that the traditional character of the area is preserved. Given the above, it is considered that the proposals seen alongside the use of planning conditions would preserve the character of the Holcombe Conservation Area and protect the setting of the Listed Building and would therefore comply with paragraphs 189, 190, 193, 194, 195 and 196 of the NPPF and EN2/1, EN2/2 and EN2/3 of the adopted UDP and satisfy the obligations of Section 66 and Section 72 of the Town and Country Planning (Listed Building and Conservation Areas) Act 1990.

Siting, design and visual impact

Proposed agricultural building and retention of shed and pig arc

As concluded above, the erection of a building for the purposes of agriculture is not inappropriate development in the Green Belt according to paragraph 145 of the NPPF.

However, the development would need to be assessed against UDP Policies OL4/5 and EN9/1 which requires agricultural buildings to be sited in a sympathetic manner so as to minimise visual impact on the landscape with regards to height, materials, landforms and landscaping. High standards of design would be expected and unduly obtrusive development will not be permitted in such Special Landscape Areas.

The natural character of rural areas includes the use of land for agricultural purposes. It is not unreasonable that buildings or structures are sought to accompany such uses which as a matter of principle is confirmed by the NPPF as acceptable. The original siting of these structures was for them to be in the middle of the field. For all the reasons above (Heritage and landscape importance) their relocation has been negotiated.

The agricultural building would be sited at the top of the field and set against a dry stone boundary wall which is approximately 1.5m in height. Beyond that is a line of trees behind which the landscape slopes very steeply up towards Holcombe Hill and the Tower. The proposed agricultural building would have a mono pitched roof with the highest elevation at 4m facing open views from Holcombe Old Road. It would be a simple and uncomplicated build, constructed of a close timber boarding with a grey fibre sheeting. Sited next to it would be the pig arc with a height of 1.2m of a curved tin form which has been painted green. Whilst a low level structure and not particularly obtrusive in itself, the colour does appear to jarr against the backdrop of the earth and field and it would therefore be recommended a darker green or some such equivalent be applied to the outer part to blend within the landform. This could be sought by condition.

The field shelter at a height of 2.1m is also of basic design and constructed of timber. Both these would be re-located from the centre of the field.

The design and materials are what would be expected to be used for such type of buildings, would be ephemeral in both construction and character and which are not uncommon to rural and open field locations, and those areas which are used for the grazing of animals.

In terms of siting, a number of options have been explored, and the applicant has taken the approach to set the buildings into the sloping land in a cut and fill engineering operation. The agricultural building would be viewed against the dry stone wall and the two other structures would be lower. This would result in the structures nestling within the landform and when viewed against the boundary wall, tree line and steeper slope to Holcombe hill, views and visual impacts from the immediate and surrounding areas would not be significantly interrupted or disturbed.

In addition, it is proposed to form a bund approximately 10m to the east of the buildings. This would be moulded and shaped into the land to emulate the topography of the field and which would obscure much of the structures from views, particularly those from the east.

There is sufficient information submitted with the application to understand how the cut and fill operations and subsequent siting of the buildings and the bund would be viewed from all directions, but further detail would be needed as to how it would be made to 'bed in' with the existing land formation. It is therefore considered reasonable to include a condition that further details of this are submitted for approval to secure this.

Turning to consideration of views from the west, from the public path, views of the buildings would be limited by the dry stone wall which forms the boundary and only the upper part of the agricultural building would be visible, with the pig arc and shed much lesser so. Higher up Holcombe Hill, there would be a view of the buildings, but set against the wall and relatively small in size and scale comparative to the surrounding land, only the top of the builds would be noticeable. When viewed from above, the treatment to form the roof of a dark grey fibre sheeting and the re-painting of the pig arc would significantly minimise visual impacts.

From the north and the south, the structures would be partially seen 'side on,' and set within the ground with the land sculpted to naturally mould within the topography, views of the buildings would be limited.

It is therefore considered that the proposed siting and position of the buildings in this location would not have a significantly detrimental impact on the character and openness of the Green belt or the Special Landscape area. The builds would be sensitively sited as opposed to their central location in the middle of the field so as to minimise visual impact from both short and long range views and the design and materials appropriate for the location and uses.

As such, it is considered that the proposed development would be acceptable and comply with UDP Policies OL1/2, OL4/5, EN9/1, SPD8 and the principles of the NPPF.

Retention of gate

The replacement gate which forms the entrance to the site, is a modern 5 bar galvanised steel gate. It is located on a prominent corner position and set and attached between two historic stone pillars. The gate as currently presented, is a striking and visually prominent addition in what is a typically rural setting and appears to be a discordant feature in this prime location.

It is therefore considered that a traditional 5 bar timber gate would be more appropriate and in keeping within this rural setting and more so given its position at the centre of the meeting of 3 well traversed paths/roads. The requirement to replace the gate would be covered by a suitable condition. The condition would also include the requirement to re-instate the original width of the opening of the field boundary wall.

As such, it is considered that the proposed development would be acceptable and comply with UDP Policies OL1/2, OL4/5, EN9/1, SPD8 and the principles of the NPPF.

Impact on residential amenity - The nearest properties are those to the north of the site on Cross Lane, the terraced cottages on Holcombe Old Road to the east and Hill End Farm to the south off Moorbottom Road.

Cross Lane - The closest properties would be Nos 50-52 which are set directly to the north of the site over 130m away from the structures. The orientation of the houses are such that there would be no direct views and as such the builds would not be visible to residents.

Holcombe Old Road - These houses are set at a lower level to the field and the existing shed and pig arc are visible from these houses and on the approaches along Moorbottom Road. Relocating these structures and erecting the agricultural building higher up the field would move the development further away from these houses, to approximately 100m away. The cut and fill works and the position of the bund would screen much of the buildings from their views.

The pig arc and shed would be relatively small in size and scale and it is not uncommon to see such structures within rural settings. Whilst the proposed agricultural building would be a larger building, it is similarly the case that such timber 'shed' type builds are also commonplace in rural and agricultural settings.

It is therefore considered that the proposed development would not detrimentally impact on the amenity or views from these properties.

Moorbottom Road - The nearest house would be Hill End Farm which would be just over 85m from the site. Views of the buildings from this property would be partly obscured by the boundary wall and as the agricultural building would be cut into the land, only the upper part of the structure would be perceptible. The pig arc and shed would not be visible, as they would be lower in height and set within the cut of the land. It is therefore considered that impact on this property would not be significantly damaging.

In consideration of the distance between the proposed buildings and the surrounding residential properties, together with siting the structures within the landform, formation of a bund and external treatment of the buildings, it is considered that impact on the amenity of local residents would not be compromised or detrimentally affected by the proposed development.

Access - There is an existing access into the field via the gated entrance at the meeting of Cross Lane, Moorbottom Road and Holcombe Old Road. Apart from the replacement gate, assessed above, there would be no changes to how the field of the structures would be accessed.

There would be no impacts on publicly accessible routes and footpaths close to or around the site and the Public Rights of Way Officer has raised no objection.

Natural England (NE) - Natural England had initially raised concerns about the proximity of the West Pennine Moors SSSI to the proposed site and how surface water and animal waste would be controlled so as not to affect this area.

The site is in proximity to, but not part of the SSSI, being separated by Moorbottom Road and the dry stone boundary wall. The site is at a lower level to the SSSI and slopes downwards away from the SSSI from west to east.

The field is already being used for the purposes of grazing and agriculture and the proposed agricultural building would predominantly be used for storage purposes. The concerns by NE can be addressed by conditions, for the submission of a drainage scheme and treatment for animal waste to be submitted for approval.

Natural England have confirmed they would be satisfied with a condition for the submission of a drainage scheme and this has been included at condition 9.

Condition 10, for the submission of a construction management plan would ensure the construction of the development would not impact on the SSSI.

Response to objectors

- Given the cut and fill operation and the significant rise in gradient of Holcombe Hill, the proposed structures would not be viewed against the backdrop of Holcombe Hill or the Grade II Listed Holcombe Tower to adversely impact on the setting of the Listed Building.
- The site is an open field and can be used for the grazing of animals without the need for planning permission.
- Chattels can be erected without the need for planning permission.
- Conditions would be recommended to seek further details of how the land would be formed around the proposed cut and fill works and the proposed bund.
- Permitted Development Rights would be removed to restrict any further developments on the site.
- The site/proposals do not fall within the requirement to consult with Natural England.
- Issues relating to openness, impacts on Heritage assets and the Conservation area, landscape, siting, design, materials and appearance have been considered in the above report.

Update and responses to the representations received to the application since the deferment of the 24th March 2020 and 21st April 2020 Planning Committee Meeting.

Publicity

Further letter of objection received from c/o Dawes Bank, Holcombe which raises the following issues:

- There seems to be at least 3 solid grounds for refusal based on - the engineering operation in the Green Belt, Heritage and design.
- A Conservation Area assessment should not treat a proposal just like any other but should reach a higher plane.
- Critical of the applicant's heritage assessment - a fundamental misunderstanding of what constitutes the Holcombe Conservation Area and what comprises its heritage significance.
- The Conservation officer's comments are incomplete and in places contradictory.
- Incomplete in its application of the NPPF and the advice should not be exclusively relied on.

Objection received from 1 Lumb Carr Road which raises the following issues:

- The Conservation Area Appraisal 2010 states that the Council will resist all proposals to develop open spaces within the Conservation area with building.
- Already this field is compromised by both the hut sited in the field centre and number of fences and openings.
- Good to know that the buildings are recommended to be re-sited and grouped and the gate is to be replaced by one more appropriate.
- Still have reservations to the proximity near to the wall and impact on views from and to the Conservation Area.
- To view this simply as an agricultural matter could be misplaced. Alpacas are not part of the UK food chain, - are they personal pets or is the wool marketed - there is no economic benefit to the public purse.
- Retrospective applications put the LPA on the back foot.
- Locals already see the alpacas being harnessed and trained for 'walking on the lead' - a feature of other alpaca petting stations for children.
- Should these concerns as to the true nature of this project be valid, there is no obvious sensible infrastructure to support visitors which could be numerous. Cross Road is not suitable for extensive parking.
- Make no mistake - if approved this business could totally change the character of this central area of Holcombe and be a huge trouble to the community.
- Important note - NO HOLCOMBE RESIDENTS' SUPPORT
- Looking more closely at the Conservation Officer's scrutiny, as a long term resident of Holcombe, I disagree with various points.
- I can see no substantial benefit to the public, nor does the questionable economic value

outweigh the Conservation mandate of enhancement. The large barn and the unpleasant bizarre bund would very much impact on views from Holcombe Old Road, and will be noticeable from Moorbottom Road as an ugly structure with no historical precedent in the area.

- The character of the previous open pasture will be substantially changed for the worse. Clearly the original development done without permission, was judged to be in total breach of the Conservation regs.
- Recently there has been an attempt to lay an electrical service to this proposed new small holding. The Community was amazed at a Highways Order to close Cross lane for electrical works, only to discover these were for the business proposed.
- Personally concerned over possible lighting being claimed for 'security' when no flood lighting is needed. In my view conditions need to be applied strictly to exclude such and protect the heritage landscape.
- Either these eccentric animals are suitable for such a pasture without extra non vernacular structures to blight the landscape or if not surely, major spoiling of the latter is not justified just to indulge a personal whim. Mains Water supply for hygiene of any visitors is also key; is this service to hand as salmonella is a known risk, especially for children ?
- Photos provided of floodlights at Daril Uloom
- Recommendation of approval is most disappointing; , depressing given the Conservation Area regs.
- There is no condition recommended for either protection for the nearby residents to prevent the applicant starting a Visitor Centre or condition to limit lighting of this new building to the interior and not flood lighting.
- Troubled by the significant earth bund - This will change the topography of the field markedly. Whereas the Conservation Appraisal emphasised the natural open rolling nature of the fields was part and parcel of the medieval field patterns, and should be retained. Such bunds are generally for flood defence use in civil engineering - but here I can see no sensible reason for this construction, nor can I source any explanation in the documents submitted. I am suspicious that this is merely being used to enable the applicant to 'hide' all the earth due to the large scale excavation for the barn foundations, rather than remove it from the site, an expensive exercise, and leave the natural terrain unspoiled.
- Recommend the Conservation Officer to revisit the initial judgement - 'Saxon works' are simply no part of a previously beautiful open pasture.

Objection received from No 145 Holcombe Old Road which raises the following issues:

- The development in respect of which the application has, belatedly, been made has already been substantially effected without permission. From a positive point of view this has demonstrated the effect of the development on the landscape in the Holcombe Conservation area. Inspection reveals that the effect has been completely adverse and has created and will, if permitted, have a continuing detrimental effect on this very important conservation area.
- Previously, the land was a beautiful meadow grazed by sheep which, as sheep do, cropped the grass perfectly and without damage to the underlying terrain. Now there are random and unsightly buildings, churned up land and an incomplete and unnecessary car parking bay created at the junction of Cross Lane and Moorbottom Road .This conservation area is a favourite walking area in the Borough and is also the main point of access to Holcombe Hill , Holcombe Moor and the National Trust Estate. The view for walkers is now appalling.

Objection from 155 Lumb Carr Road which raises the following:

- Further to my previous objection, since the applicant has made alterations to the application. I still stand by the reason that this development is not in line with the appraisal of the Holcombe conservation area 2010 and neither with Article 4. It is at present ruining a meadowland and aspect of the Greenbelt area. The council will uphold the article 4 for residents but should bear this in mind when considering this drastic change.

Objection from Higher Barn Farm which raises the following:

- Yet another Trojan horse in our midst ! 41 ft building that supposedly will be hidden from view by placing an earth bund 30 meters in front of it our very own Holcombe Hobbit Land.. supported entirely by people whom don't live in the village. One wonders why ?

Objection from

103 Holcombe Old Road which raises the following

- The application now involves excavation works. As I said in my original objection this application is completely at odds with The Conservation Area Appraisal (2010) and the Bury Unitary Plan. In addition, successful lambing has now occurred in the field without the need for an enormous shed.

Objection from 109 Holcombe Old Road which raises the following

- Completely at odds with the Conservation Area Appraisal and Bury UDP.
- The revised plans will not make the view any more appealing; what was an idyllic view will be forever an eyesore. Being an avid animal lover myself I have noted the animals have lived outside quite happily during their placement. I do not feel that there is a requirement for such an imposing structure to be validated.

Objection from 115 Holcombe Old Road which raises the following

- I live on the Old Road . We all love animals but comments in favour do not take account of the scale of disruption to Holcombe Hill which is (see the addresses) used by people from all over Bury.
- This new plan will butcher a small and pretty field on the lower slopes in a Conservation Area
- It seems Bury is allowing this area to become a site of working farms not an area of beauty & calm used and loved by many. This cannot be right.

Comment received from Home Meadow Barn, Lumb Carr road:

- No objection to field shelter, pig shelter, fencing and gate. However, I do question the size of the agricultural building at 12.9 metres (over 42 feet) and 6.09 metres (almost 20 feet) wide. It is not justified for the size of the pasture which is relatively small at around 4 acres . To have a building with the potential to store a quad bike, trailer , hurdles, chain harrow, 6 deep straw/hay bales, hard feed etc is huge bearing in mind the small number of animals. In recent years, the Council has been extremely strict as to limiting the size of outbuildings in Holcombe area. Being in a conservation area any urbanisation is an issue.
- The location of shelters and storage in one location on an edge of the pasture seems a better solution than spread around the site.
- The land owners have a right to enjoy the land, animals and I am sure the present owners are careful custodians of our countryside. However, the size of storage seems to large.

Additional objection received from c/o Dawes Bank with the following issues:

- Have a personal interest in the area of Holcombe from family history to growing up. It is right and important that any planning decision affecting the Conservation Area and Green belt deserves to be carefully and properly assessed.
- Green belt - perplexed by the conclusions in the report - the extent of excavation and size of development cannot be considered anything other than intrusive and encroachment.
- Conservation Area - (i) the apparent contradiction of finding the effects both harmful and neutral; (ii) the assertion that because you can see beyond and around the development it therefore somehow does not adversely impact on the overall character and setting; (iii) the assertion that the scheme brings public benefits because it incorporates measures to mitigate the adverse impacts - there are no public benefits.
- The report's finding - erroneous in my view - of no harm to the Listed Building setting of Holcombe Tower and similarly Hill End (non-designated heritage asset) and consequently the absence of any balanced weighting of this harm as would be required by the NPPF.
- A mis-representation in the report of the degree to which the Moorbottom wall would mask the development - the wall is stated as 1.5m high but in fact is at or less than c.0.5m high along much or all of the length of the development site.
- Why has Natural England not been consulted as the Government website indicates that it should be owing to being adjacent to the West Pennine Moors SSSI?
- What PD rights could be removed and how beneficial could this be, given that the planning site area red edge is limited and does not include the whole field?
- Given the very specific and precise details now submitted regarding the excavation and bund, what additional information is sought by condition 3? Does this not reveal that the

- submitted scheme is actually not found acceptable?
- The planning balance - duty to find solutions versus duty to balance harm - Ultimately, not all applications can or should be approved - there are no public benefits and the development would adversely affect the Green Belt and heritage, are harmful and represent poor design. The scheme is contrary to policies in all respects.
- These grounds of objection are fully defensible at appeal.
- Failure to take reasonable steps to ensure that the statutory site notice publicity took place for the required 21 days despite advising at the time that a replacement notice would be displayed.
- Statutory requirement to consult with Historic England on site area above 1,000m² affecting Conservation Area character and appearance - has this been undertaken?
- Refusal of the LPA to divulge the pre-application advice for public scrutiny.

Objection received from No 12 Helmsore Road:

- I firstly must point out that I am totally against this - as it is the start of a process in which the owner of the land has bought it strategically in order to at some point apply to build a house on the land - having livestock is the start.
- We must preserve our green belt and especially when it is a SSSI and conservation area. There is no need to erect any further structures in or around the site as there weren't any there previously. The sheep only need a temporary structure when they are lambing.

Objection received from No 103 Holcombe Road

- As this large development lies in the Conservation area it is critical that a site visit takes place so that the Committee can see the effect the application will have. The present lockdown will prevent this and a virtual site visit would not work. There is no reason the application cannot be delayed until we are out of lockdown.
- There would appear to be no animal welfare issues as successful lambing has already taken place in the field.
- Site notices were placed adjacent the site around Christmas but these disappeared quickly. This was brought to the attention of the case officer then. Planning notices for a revised application were again displayed adjacent to the field on the morning of Friday 24th April and these had disappeared by 5.30pm. This means that there has been no adequate public display of planning notices. Another reason to delay the May Committee Meeting for this application. Further signs need to be displayed and left in place.
- One of the Ward Councillors will not be able to attend the meeting due to the pandemic and we therefore have no Councillors to speak for us.
- The application is contentious with no local support on the Planning website.
- There are procedural problems which need to be addressed. The best way to achieve these would be to delay the Committee Meeting for this application until there is a physical site visit, adequate display of site notices and a Councillor voice to speak.

Response to additional objections

- The LPA is required to assist an applicant where it can remedy unauthorised works and this includes the ability to submit applications retrospectively and dealing with them (article 35(2) TCP (General Development Management Procedure)(England) Order 2015. The Planning Act 1990 makes it clear in s179 that "it shall be a defence [for the perpetrator] to show that he did everything he could be expected to secure compliance" - this can include discussions, actions and including the submission of an application under s73A (retrospective - ie planning permission for development already carried out). The applicant has been responsive in this respect.
- It is considered a simple refusal of the planning application would not have assisted in any positive recourse or solutions. The LPA has worked with the applicant to progress proposals which would provide the best and most positive solution to the situation.
- That the objectors do not agree with the Conservation officer response is not a reason to refuse the application. Views on matters can be different.
- Natural England were consulted on 24/4/20
- A further site notice was posted 24/4/20. the application has been advertised in the press, letters have been sent to nearby neighbouring properties and site notices have been posted on two separate occasions. The requirements of publicising the application have therefore

been fulfilled.

- Due to the current Covid-19 situation, the government has introduced new legislation that enables local authorities to resume, as far as possible, their usual business, using both flexibility and modern technology. This includes holding 'virtual' Planning Committee Meetings and carrying out 'virtual' site visits.
- Representations by Ward Councillors on behalf of local residents can still be made through submission of a written response.
- Historic England have been consulted and have no comments to make on the application proposals.

Further objection received from c/o Dawes Bank, Holcombe on 14th May 2020.

Very concerned about aspects of the LPA's Conservation and Heritage Assessment in the report and has led to me independently commissioning the views of a Heritage Conservation specialist, Kathryn Sather and Associates (KSA) (report dated April 2020)

The KSA Report states two issues - the applicant's inadequate understanding of the significance of the Heritage Assets and their setting; the incomplete and inadequate assessment of the impact of the proposals on the Holcombe heritage assets.

KSA have assessed each element of the proposed new development using the ICOMOS Heritage Impact Assessment Methodology.

The KSA Report can be summarised as follows:

- KSA states that the applicant's Heritage Statement identifies only two heritage assets (Peel Tower and Holcombe Conservation Area) but is deficient in discussion of their setting and significance. Other listed buildings and NDHA in the vicinity are not mentioned.
- The applicant's later submission shows their understanding of the Conservation Area remains partial and selective.
- Removal of pig arc and field shelter from current location - Would constitute a slight visual change which is characterised as a minor change. Therefore the significance of impact is slight, would constitute an improvement to the setting and views so would have a slight beneficial impact.
- Cut and fill engineering works - It is not clear the bund would screen the building from all viewpoints (the oblique angles). Neither the applicant or LPA make reference to introducing this artificial feature into the open slope. The engineering works would result in a visual change to a few key historic aspects of the landscape character - namely a minor change - which would have a slight adverse impact.
- New agricultural building - Problematic in terms of scale and materials which are not characteristic of the area. This element is assessed as having a slight adverse impact.
- Relocation of the field shelter and pig arc - Due to the excavation, they will have reduced visibility from Moorbottom Road. Constitute a slight adverse impact.
- Gate - Already conditioned to be timber rather than steel. But no reference is made to a condition to re-instate the opening width or the stone walls.
- The ICOMOS methodology summarises the impact of the elements as follows - relocation of the pig arc and field shelter would have a slight beneficial impact on the character and appearance of the Conservation area and setting of Peel Monument. All other elements would have a slight adverse impact on the heritage assets.
- The main heritage considerations whether the proposed works would preserve or enhance the character or appearance of the Holcombe Village Conservation area or cause harm to the setting of Peel Monument.

Conclusion of the KSA Report:

- The Applicant's Heritage Statement does not adequately describe the significance of the Conservation Area and the impact of the proposals. It is partial and selective in its approach to the point that it is misleading.
- The LPA fails to draw attention that the field shelter and pig arc were erected without permission - hence retrospective consent.

- Does not identify the need to 'make good' the widening works carried out to the gateway and wall.
- The proposed works will have a slight adverse impact on the character and appearance of the Conservation Area - the proposal is for a new group of buildings in a location where there were previously none and in materials and grouping not in character of the Conservation area.
- No reference has been made to the impact of introducing the bund.
- Regarding views within the Conservation Area and setting of Peel Monument - the proposed development will be experienced by walkers along Holcombe Old Road and affect views from Lumb Carr Road.
- Para 193 of the NPPF states that 'great weight should be given to the asset's conservation, irrespective of whether the potential harm is substantial or less than substantial'. To justify approval of a proposal of less than substantial harm, the case for public benefit needs to be clear and convincing - para 194. The LPA's report maintains it constitutes limited economic development, managed in such a way that impact would be neutral - this is not the case.
- 'Minimal impact' is adverse impact, and therefore harmful.
- The KSA report concludes that the proposal will cause less than substantial harm to the setting of Peel Monument and the character of the Conservation Area and should not be permitted. In addition, enforcement action should be taken to restore the widened opening to its earlier appearance.

In response to this objection and the KSA Report, the Officer Report has been updated above.

Update and responses to the representations received to the application since the deferment of the May Planning Committee meeting.

Publicity

Objections received following the deferment at the May Planning Committee meeting.

Objections received from Higher House, Holcombe, 3, 93, 95 Helmsshore Road, 12 Mitchell Street, Tower View, Higher House Cottage, 1 Lumb Carr Road, The Byre Hey House Mews, 5 Cross Lane, 302 Lumb Carr Road, The Croft, Meadow Heys, Hey House Cottage, 31-33 Chapel Lane, Rainbow Cottage, -

- Regularly walk around this area and I have not seen any notices.
- It is an affront to the Green belt and will have a detrimental, negative and damaging impact on views and the visual amenity of the area.
- Strikes me as odd why the Council and its officers would go against the sound advice from the Heritage Consultants instructed by the objectors.
- They highlight the PERMANENT and devastating impact this development would have.
- The same Heritage Consultants have previously advised Bury MBC in the past regarding conservation and protection of this area which has helped from the current policy.
- Invite the Councillors to reject the application.
- Further to the application of the Alpaca Farm in Holcombe I am absolutely amazed this can be allowed when residents of the Village, myself being one for many, many years have jump through hoops to have any refurbishment done to our homes unless keeping with the heritage of Holcombe.
- First of all I was appalled when the lovely wooden gate to the field (which is now the Alpaca field) was ripped off, tossed aside and replaced with an ugly metal one. I had been told I could only replace my gate with wooden yet this is directly where everybody walks past to climb Holcombe Hill – an absolute eyesore. Yet Bury Council did nothing. The field now houses more animals than I think the field should and there seems to be not enough grass and just looking more and more tired with every week. Another eyesore.
- Holcombe Village was a beautiful tourist spot for the Borough of Bury (and still could be) but gradually it is becoming the total opposite, wheelie bins everywhere – planning permission being granted without the long term effect being taken into consideration. Once again I ask that you oppose this destruction of Holcombe Village.
- Given the fact this is part of a conservation area, granting planning permission must be seen

as a complete contradiction to the conservation mandate of enhancement. The erection of a barn and 'bund will be hugely intrusive on what was once a pleasant, traditional pasture. Previous grazing did not necessitate the erection of sheds or a wind turbine.

- This building would damage the appearance of this area. My family and I visit the hill and cannot see how this would be a good thing.
- Conservation Area and any buildings will spoil the environment and possibly lead to change of use.
- I was in conversation with an agricultural knowledgeable person on Cross Lane this morning. He instigated talk on the Alpaca Filed development nearby, as to his view that the field was over stocked, contra to the agricultural regs for animal welfare. He was amazed to hear of the application in train. Clearly another consequence of the lack of public notification - due to the removal of your notices within hours of their installation. He went in to say that he intends to check out the livestock regs per acre, to object too, if he could discover in time. He also remarked that the grass was almost bare and that no water was laid on for the stock so the owner has to cart in this service for his animals which does seem out of order
- Hence I believe BMBC has legal duty to check out the regs on agricultural compliance to safeguard Health and Safety of animals before releasing approval for this, perhaps misguided, venture.
- The wind turbine is particularly regrettable to a Community who fought against and won against development of the huge wind farm opposite.
- The outbuilding proposed on a 4 acre piece of land with a small flock of lambs and alpacas is excessive at over 12.2mx6.1mx4m= 297.7sqm. (Note: The average 4 bed-detached house in the UK stands at between 149-200sqm);
- The mound of earth being suggested to disguise the huge outbuilding will not obscure our view of the building from our garden, which stands at a higher elevation than Holcombe Old Road (+2m);
- The proposed outbuildings and the ugly wedge of earth will be visible from the other 3 elevations of the site, which includes Moorbottom road (a popular walking path for residents & visitors) and the view from the Peel Tower Monument itself;
- If the application has to include a huge excavation of a pretty meadow to disguise it from public view then does this not confirm that the outbuilding is totally at odds/not complaint with conservation policies.
- I invite the officers to view the site from our garden.
- We are concerned that, given it's size, there would be a future opportunity to apply for change of use to convert it into a dwelling which is clearly against the intention of the protection of the conservation area.
- The scale of the earthworks involved are mind boggling for the middle of a conservation area and completely inappropriate.
- The works will be a blight on the environs and approach to Peel Tower, a cherished recreation destination for thousands of walkers.
- It will be reminiscent of Offa's dyke and future generations of archaeologists will puzzle as to its purpose.
- There can be little doubt that the applicant has a long term plan for the site, this is clearly a Trojan Alpaca designed to be the first move to breach the walls of rural Holcombe.
- There are many fields in less prominent locations which would be far more suitable for this operation. However I do concede that should areas would not as attractive for the possible future uses of the site envisaged by the applicant.
- Not aware of it until I saw it in the Bury Times - disgraceful suggestion to vandalise Bury's most significant beauty spot.
- The numbers of grazing sheep and animals is determined by agricultural regulations.
- Since the applicant now has 9 ewes, 5 alpacas, plus numerous lambs, clearly this is almost twice the previous grazing level. Water is not sufficiently available for these animals - and again the applicant brings in water routinely. The current position is possibly unsustainable, and the animals are at risk of suffering, if moorland water levels diminish, and no mains supply is to hand.
- Very large pile of animal dung now accumulated in the bottom half of the field.
- I urge Bury Council to check out compliance to these agricultural regulations prior to the meeting, and consider the risk that, by their officers encouraging the Members of the Committee to approve, they lay themselves and the Members open to possible prosecution.

Ignorance is no excuse in law for all parties.

- Building disproportionately large of the field.
- Better location would be a smaller building at the entrance and so the field is not damaged by delivery vehicles for feed etc.
- preferable to remove the large mound of earth.
- If approval granted its use should be restricted relating to storage for managing livestock.
- The Council notices were removed by persons unknown, within hours of being posted, but those who removed them, must have had a vested interest in so doing, to prevent passers by from being made aware of the proposed action.
- The Holcombe countryside is one thing that attracts so many visitors from our urban areas, indeed, the one thing that the owners said most attracted them to buy the field in the first place ! This is an area of green belt and conservation, which needs to be protected and enhanced, not destroyed.
- Brought to my attention just now is
- "On the planning portal the this applicant has applied before for a dwelling house and barn where he was breeding the alpacas too. The first one was Radcliffe and the second one was Ringley road, so u may be right re future intentions but funny how it was declined in past albeit it was for something different"
- This seems to reinforce my previous objections as to his possible future intentions.
- The proposed application will significantly disrupt the nature and appearance of a highly valued part of the borough's conservation area which sits within the Green Belt and is also a SSSI. The erection of the barn and earth works is proposed adjacent to an ancient stone wall, putting it at risk of permanent damage, and the visual impact of what is proposed will significantly undermine the amenity value of the area, which is enormous and enjoyed by local residents and visitors alike from further afield. Holcombe Hill and Peel Tower have an iconic value across Bury and it is very important to conserve this, consistent with the status of the area as a designated conservation area, and not seek to introduce buildings and changes to the landscape which will detract from the area's appeal and its value for so many.
- I wish to object to this application on the grounds that it does not enhance the conservation area in any way and on the face of it appears to be another example of development via the backdoor.
- If this is a business enterprise as is claimed, as far as I am can see it is a business which does not advertise its services or products.
- Furthermore, the siting of the buildings appear to be in an area which commands the best view of the surrounding area... which not only makes such buildings more visible to visitors and residents of the area but clearly makes the potential of conversion to residential dwellings more attractive.
- Officers and elected members should search previous applications regarding "Alpaca Farms" to see that applications for dwellings are often included or follow soon after.
- Finally works on this scale will irreversibly destroy an area in the centre of Holcombe village and the conservation area as a whole.

Response to objections

- Issues regarding the visual impact, Conservation area assessment and impact on vires have been covered in the main Officer report.
- To clarify, condition 6 requires the removal of the steel gate and replacement with a wooden gate.
- The reference to animal welfare and compliance to safeguard Health and Safety of animals is facilitated under separate planning legislations and not a material planning consideration.
- The wind turbine does not form part of the planning application.
- The area of the proposed structure is calculated as the length x width - 12.2m x 6.1m sqm = 74.42. Volume is calculated as length x width x height - 12.2m x 6.1m x 4m = 297.68 cubic m.
- Any change of use would require a planning consent.

Update and responses to the representations received to the application since the deferment of the June Planning Committee meeting.

Publicity

Objections received following the deferment at the May Planning Committee meeting

No 103 Holcombe Old Road,

- A professional heritage assessment of this planning proposal was commissioned from Kathryn Sather Associates - the same heritage conservation consultants employed by Bury to produce the Holcombe Conservation Area Appraisal as endorsed and accepted by the Planning Control Committee in 2011.
- This recent assessment concludes that the planning proposal will cause 'harm to the setting of the Peel Monument and to the character and appearance of the Conservation Area and should not be permitted'

1 Lumb Carr Road

- With regard to the real site visit now pending I wish to request that the Committee members view the messy Field Buildings in existence plus the new Barn proposed site from both Holcombe Old Road and Moorbottom Road to gather the overall impact on both significant Public Rights of Way in Holcombe, used routinely by both walkers and tourists.
- Without protection the poor terrain would be damaged.
- Not sufficient available land for the number of animals - inadequate animal husbandry
- Need to check out compliance with agricultural regulations
- Legitimately worry that development of a full scale farm base if allowed would create a further case for on-site accommodation

Leeside

- This property lies at the junction of 2 ancient routes, and the first edition of ordnance survey maps shows a criss crossing of public footpaths on the property
- It also lies within the historic Tottington Lower End and Royal Manor of Tottington.
- Recent archaeological excavations within 2 KM have found evidence of Anglo Saxon and Medieval occupation, and the ancient paths may have a connection with the transportation of goods from the area, particularly with Monk Bretton Priory with which it has a documented connection dating to 1172.
- As a condition, a magnetometry survey should be undertaken and subsequent archaeological excavation. I assume this application has been referred to the county archaeologist.

Support received from 21 Cliff Mount

- Unfounded accusations are being made in the objections
- Stick to the facts - the application is for a small barn, gate and pig arc. The metal gate is to be reverted back to timber despite a metal gate not 10 metres away. The pig arc is movable. I would suggest both these inclusions are unnecessary in the first place. Leaving the small barn which is of the right size to use for animal husbandry.
- Somehow this application and the one at the Riding School needs yet more site visits and scrutiny, despite both being recommended for approval.

Response to objections

- There are no historic environment records for the application site. As such, an informative will be included that if any archaeological remains are found, the applicant is advised to contact Greater Manchester Archaeological Unit and the LPA.
- Any further development would require planning permission.
- Agricultural husbandry and welfare are not material planning considerations and are regulated under separate environmental regulations.

To clarify, a total of 49 representations have been received.

31 objections, 17 in support , 1 comment

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. Within 28 days of the development hereby approved, a scheme for the implementation of the development, including a timetable of works, shall be submitted to and approved by the Local Planning Authority. The approved scheme only shall be implemented thereafter.
Reason. In the interests of visual amenity and to protect the setting of the Holcombe Conservation Area pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas and OL4/5 - Agricultural Development.
3. Notwithstanding the provisions of the approved plans, no development shall commence unless and until full details of the proposed cut and fill works and the height and form of the bund, including a methodology, together with details of the appearance of any exposed structures, grading and resulting topography of the land on all sides of the cut and fill works and the bund has been submitted to and approved by the Local Planning Authority. The approved details only shall be implemented and approved land form and topography only shall be retained as approved.
Reason. In the interests of visual and residential amenity and to preserve the setting of the Holcombe Conservation Area, Area of Special Landscape and the character of the Green Belt pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas, OL1/2 - New Buildings in the Green Belt and OL4/5 - Agricultural Development.
4. Within 28 days of the development hereby approved, the pig arc shall be coloured matt black and thereafter maintained in that colour.
Reason. In the interests of visual amenity and to preserve the openness of the Green Belt and protect the special character of the area pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas, OL1/2 - New Buildings in the Green Belt and OL4/5 - Agricultural Development.
5. The agricultural building hereby approved shall be constructed of a timber boarded material with a dark grey sheet roof and thereafter maintained.
Reason. In the interests of visual amenity and to preserve the openness of the Green Belt and protect the special character of the area pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas, OL1/2 - New Buildings in the Green Belt and OL4/5 - Agricultural Development.

6. Within 28 days of the development hereby approved, a timber gate to replace the existing 5 bar steel galvanised gate to the north access of the field, as shown located on approved plan 'Proposed Location Plan Version 3', together with details of works to re-instate the original width opening to the field boundary wall shall be submitted to the Local Planning Authority for approval. Within 56 days of the approval of the details, the approved timber gate only shall be used to replace the existing gate, and the works carried out to re-instate the opening, and to be thereafter retained as approved.
Reason. In the interests of visual amenity and protect the special character of the area pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control and EN9/1 - Special Landscape Areas.
7. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Class B of Part 6 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To safeguard and protect the visual amenity and special character of the area to ensure that future inappropriate development does not occur pursuant to policies of the Unitary Development Plan listed.
8. This decision relates to drawings numbered Proposed Location plan V3; Proposed agricultural building C3956-1-1A; section plan C3956-1/2A; Volume results plan and topographical plan C3956-1/1-A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
9. Within 28 days of the development hereby approved, details of a drainage scheme and the treatment for animal waste shall be submitted to and approved by the Local Planning Authority. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented on first use of the development hereby approved and thereafter maintained.
Reason. To ensure there would be no adverse impact from pollutants from the development and the site on the natural habitat of the area in accordance with chapter 15 - Conserving and enhancing the natural environment of the NPPF.
10. Prior to any works commencing on site, details of a Construction Management Plan' shall be submitted to and approved by the Local Planning Authority and shall confirm/provide the following:
 - Type of vehicle movements;
 - Access points to the site;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
 - Parking on site of operatives' and construction vehicles together with storage on site of construction materials;
 - Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dirt and dust nuisance caused by the operations

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction periods. The areas identified shall not be used for any other purposes other than the access/turning/parking of vehicles and storage of construction materials.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent

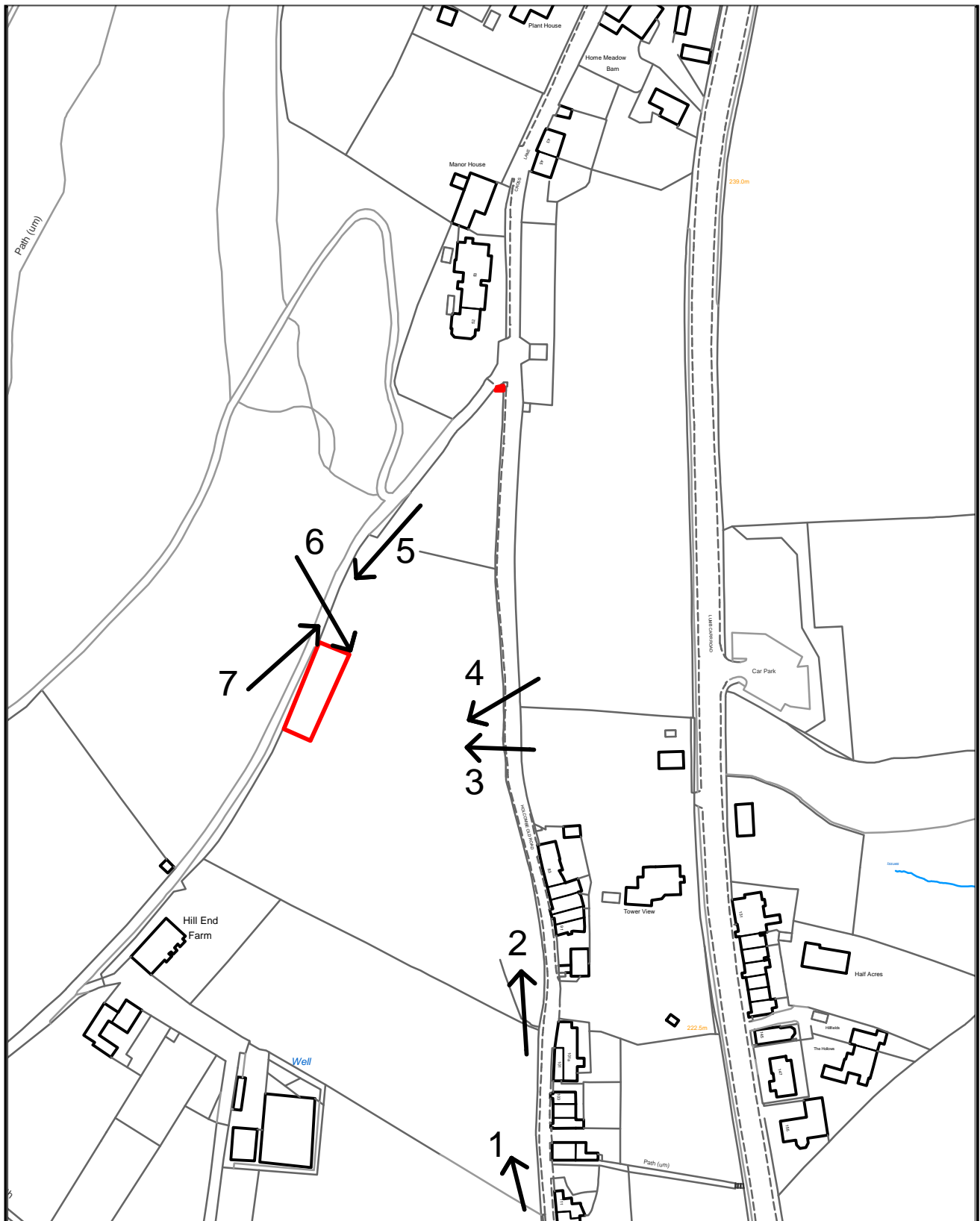
area and ensure adequate access and parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas and OL4/5 - Agricultural Development.

11. Should the land cease to be used for the agricultural purposes or the structures hereby approved cease to be required or used, the approved structures shall be removed from site and the land re-instated to its former state, and within 3 months of it ceasing operation.

Reason: In the interests of the visual amenity pursuant to Policies EN1/1 - Visual Amenity, EN2/1 - Character of Conservation Areas, EN2/2 - Conservation Area Control, EN9/1 - Special Landscape Areas and OL4/5 - Agricultural Development.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 64786

**ADDRESS: Land to the south of Moorbottom Road
Holcombe**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

64786

Photo 1



Photo 2



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Photo 3



Photo 4



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Photo 5



Photo 6



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Photo 7

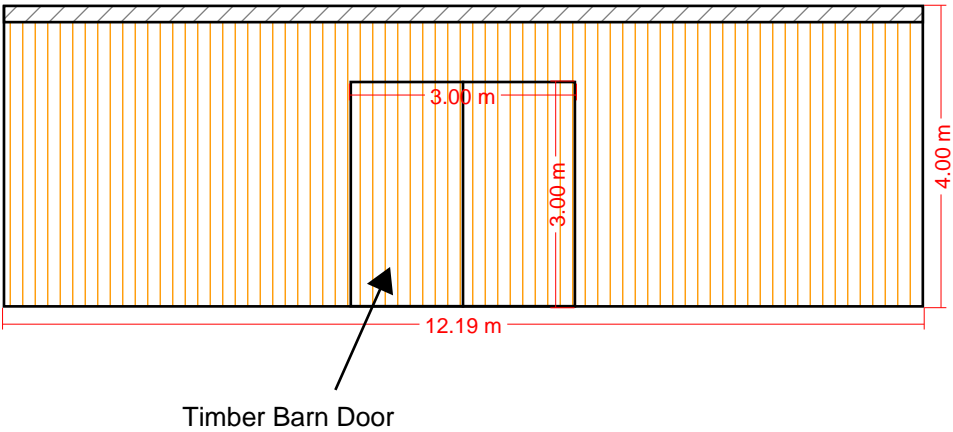


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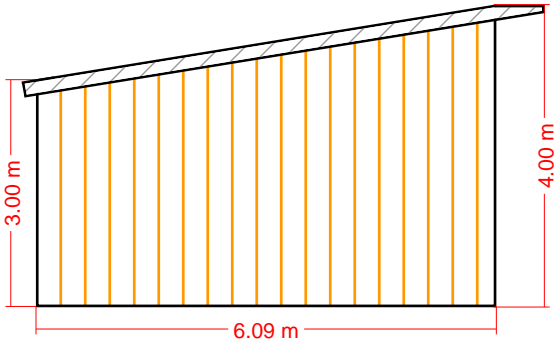
Photo 8



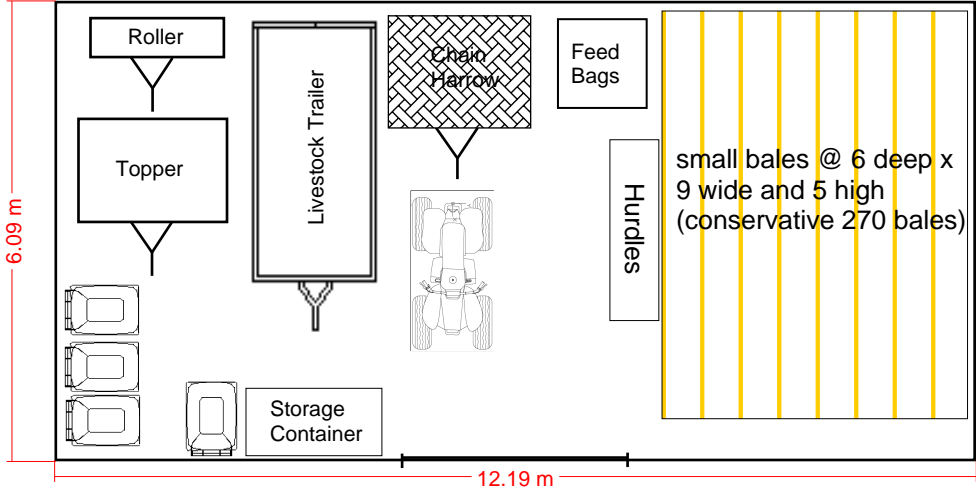
Eastern Elevation



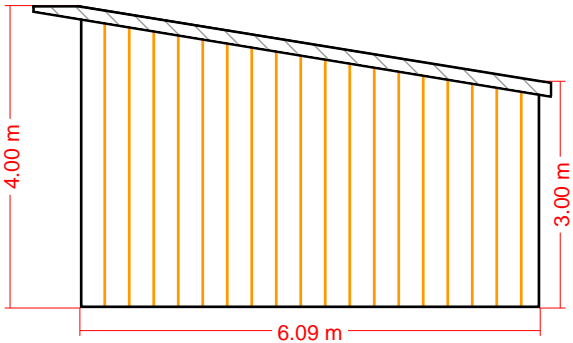
Southern Elevation



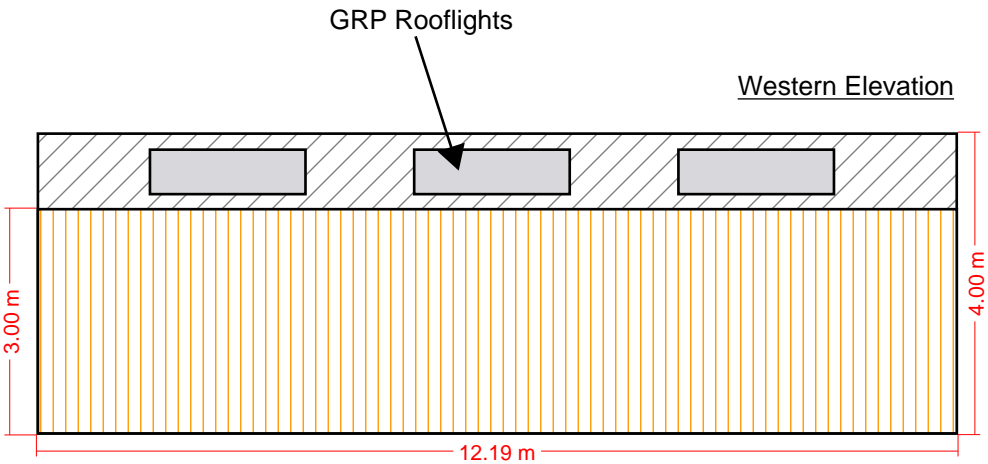
Roof Plan



Northern Elevation



Western Elevation



Do Not Scale From This Drawing

Notes:

THIS DRAWING IS STRICTLY FOR THE PURPOSE OF ASSISTING THE PLANNING AUTHORITY TO MAKE A DECISION ON THE PROPOSED DEVELOPMENT. THE PLANNING DECISION NOTICE SHOULD BE CONSIDERED AS BEING THE DEFINITIVE DOCUMENT WITH REGARD TO THE DIMENSIONS OF THE BUILDING. UNDER NO CIRCUMSTANCES SHOULD THIS DRAWING BE USED AS DEFINITIVE AND ALL CRITICAL MEASUREMENTS MUST BE CHECKED ON SITE.

SPEC:

Structure: Timber Framed

Side elevations: tanalised timber close boarding.

Roof details: Natural Grey fibre Cement Sheetting with GRP Rooflights.

0.6m overhang to front elevation

15deg roof pitch complete with crown ridge, barge flashings and foam ridge fillers. All eaves to have trimline box gutters complete with PVC fall pipes.

Client:







Mr. & Mrs. M. Unsworth

Drawing Title:

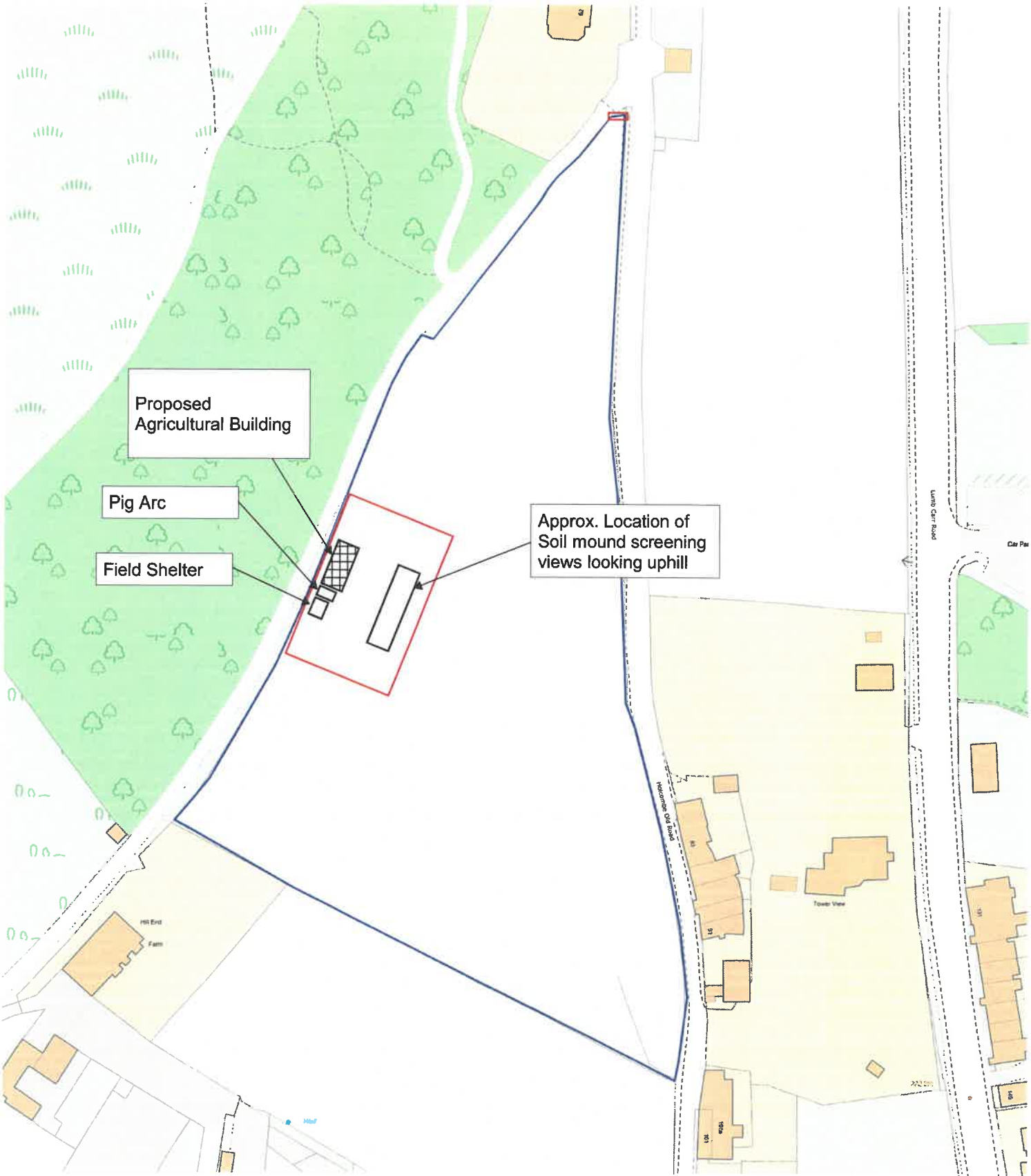
Proposed Agricultural Building

Project Address:

Land off Moorbottom Road, Holcombe, Bury

DRG No. C3956-1-1A		Scale 1:100 @ A3	
Drawn TW	Checked MU JU		Approved TW
<div><div></div><div><p>p wilson & company</p><p>chartered surveyors</p></div></div>			
<div><div></div></div>			
Date 18.02.2020		www.pwcsurveyors.co.uk 01772 882277	
		Revision C	

Proposed Location Plan V3



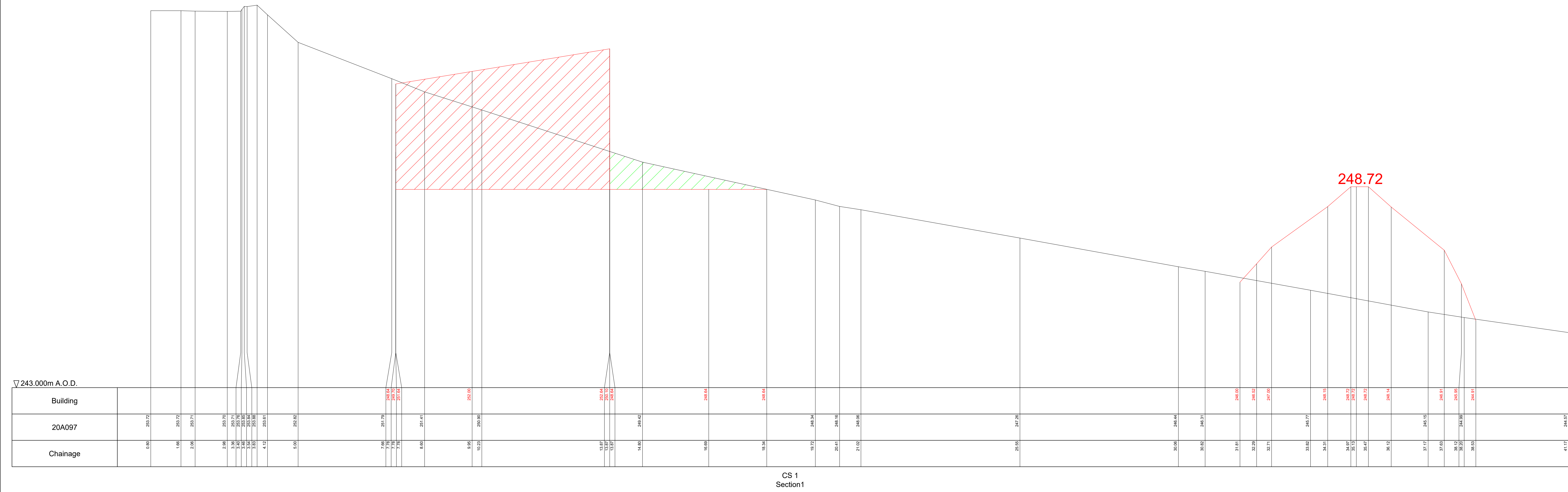
Volume Results
Cut 388.9m³
Fill 329.5m³

Balance Cut 59.4m³

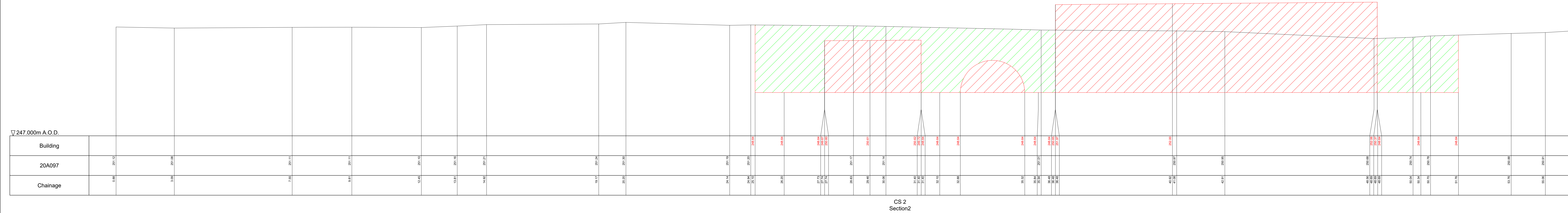
With this proposal retaining walls
would be required



Approximate point for view uphill + 2m
from ground Level obtained from Lidar



AREAS HATCHED GREEN/RED TO BE CUT



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Ward: Radcliffe - West

Item 02

Applicant: Euro Garages Limited

Location: Asda Stores Ltd, Pilkington Way, Radcliffe, Manchester, M26 3DA

Proposal: Erection of drive thru coffee shop

Application Ref: 64949/Full

Target Date: 10/02/2020

Recommendation: Approve with Conditions

Description

The application site relates to part of the car park for the Asda store and is located in the north eastern corner. The site contains 78 parking spaces and a trolley store. The site is accessed from Sion Street via the junction with Pilkington Way, Sion Street and Dale Street. There is a pedestrian access to the north, which connects to Pilkington Way and a pedestrian access to the east, which connects to a riverside path.

There is a supermarket to the south west with industrial buildings beyond. The River Irwell marks the boundary to the east with industrial buildings beyond. Pilkington Way is located to the north with a supermarket and the town centre beyond. There is a hot food takeaway to the west.

The proposed development involves the erection of a building, to be used as a coffee shop (Use Class A1) and drive thru facility on the north eastern corner of an existing car park. The building would be single storey and would be constructed from timber boarding and composite panels. The site would be accessed through the car park from Sion Street.

Relevant Planning History

None relevant.

Publicity

Site notices were posted on 20 December 2019.

17 letters have been received from the occupiers of 233, 235 Lever Street, 34 Abbey Court, 38 Bridgefield Drive, 58 Bury Street, 123 Higher Dean Street, 10 Lynton Lea, 23 Parkside Close, 42 Heatherside Road, 39, 134 Outwood Road, 47 Blackburn Street, 1, 16 Great Hall Close, 7 Rectory Lane, which have raised the following issues:

- There are many independent cafes & coffee shops that are already struggling.
- Asda should not have the run of the town.
- Was bad enough to put in Timpsons, when there are already local shops/markets that supplier these services.
- There are enough coffee shops in Radcliffe. We don't need another.
- This will be harmful to small businesses.
- Radcliffe town centre has been trying to regenerate for sometime. A drive through coffee shop is just going to take business away from our small businesses.
- There is no need whatsoever for this. Macdonalds do drive through and serve coffee and I think that's enough.
- People are struggling anyway without putting a high street coffee shop on the same side as Asda.
- Will create extra traffic which at peak times is already horrendous.
- Too many cafes already. This will lead to job losses.
- This will encourage people to use cars.

- I will boycott Asda if this is passed. Asda and Bury Council will be detrimentally targeted on social media.
- The site only has 1 access and egress point, which is frequently congested at peak times.
- The town centre has a number of cafes, which serve the area well.

The objectors have been notified of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objections in principle. Further comments to be reported in the Supplementary Report.

Drainage Section - No response.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land and electric vehicle charging points.

Environmental Health - Commercial Section - No response.

Environmental Health - Pollution Control - No comments.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and surface water drainage.

Pre-start Conditions - Applicant has agreed with pre-start conditions

Unitary Development Plan and Policies

EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/7	Throughroutes and Gateways
EN7	Pollution Control
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
EN7/5	Waste Water Management
S1/2	Shopping in Other Town Centres
S2/6	Food and Drink
HT2/4	Car Parking and New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
Area	South of Pilkington Way
RD3	
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (Retail) - The proposed development involves the erection of a coffee shop (Class A1), including a drive thru facility on the car park to Asda.

Paragraph 85 of the NPPF states that:

'Planning policies and decisions should support the role that town centres play at the heart of local communities, by taking a positive approach to their growth, management and adaptation. Planning policies should:

a) define a network and hierarchy of town centres and promote their long-term vitality and viability - by allowing them to grow and diversify in a way that can respond to rapid changes

in the retail and leisure industries, allows a suitable mix of uses (including housing) and reflects their distinctive characters.

The proposed coffee shop would be classed as retail (Class A1) under the use classes order. The application site would be located in the town centre and retail is a use that should be located in the town centre. The proposed coffee shop would complement the existing retail uses within the immediate vicinity. Therefore, the proposed development would be acceptable in principle and would be in accordance with the NPPF.

The proposed drive thru facility would be similar to that of a hot food takeaway establishment and as such, Policy S2/6 is considered to be applicable.

Policy S2/6 states that the the Council when considering all proposals which involve restaurants, hot food takeaways, cafes, snack bars, wine bars, and public houses, together with any other uses within Class A3, will have regard to the following factors:

- the amenity of nearby residents by reason of noise, smell, litter, and opening hours;
- whether or not the proposal would result in an over concentration of Class A3 uses, which could adversely change the nature or character of a centre as a whole;
- parking and servicing provision associated with the proposed development and its effects in terms of road safety, traffic generation and movement;
- provision for the storage and disposal of refuse and customer litter;
- the environmental impact of any ventilation flues and/or ducting.

The proposed development would be located on a car park within the town centre. The other uses on the car park are a food supermarket and a fast food restaurant. The proposed development would complement these uses and would not lead to an over concentration of such uses. The issues relating to amenity, parking, service provision and refuse will be discussed later in the report. Therefore, the proposed development would be in accordance with Policy S2/6 of the Bury Unitary Development Plan.

Design and layout - The proposed building would be located in the corner of the existing car park near to Pilkington Way. The proposed building would be single storey with a flat roof. The proposed design is simple and modest in appearance. The proposed retail building would be constructed from timber boarding and composite panels, which are considered to be appropriate. There would be glazing to all four elevations, but the north west and south east elevations would be predominantly glazed, providing an active frontage. As such, the proposed development would be appropriate in terms of height, scale and form and would not be a prominent feature in the streetscene. Therefore, the proposed development would be in accordance with Policies EN1/2 and S2/6 of the Bury Unitary Development Plan.

Noise - The proposed coffee shop would be open on a 24 hour basis. The existing supermarket is open on a 24 hour basis, as is the adjacent hot food takeaway. The proposed development would not generate any more noise than the current use of the site. The Pollution Control Section has no objections. Therefore, the proposed development would not have a significant adverse impact upon the amenity of the neighbouring properties and would be in accordance with Policy EN7/2 of the Bury Unitary Development Plan.

Highways issues - The proposed development would be accessed via the existing junction at Sion Street, Pilkington Way and Sion Street and through the existing car park. A tracking diagram has been submitted, which shows that the delivery vehicles can manoeuvre safely in the car park. While there is no clear pedestrian route between the proposed coffee shop and the supermarket, there are connections between the car park and a riverside path, which would provide a safe pedestrian route between the two buildings. The Traffic Section has no objections in principle and further comments will be reported in the Supplementary Report.

Parking - SPD11 states that the maximum parking standards for an A1 retail use would be

1 space per 25 square metres, which would equate to 8 spaces.

The proposed development would remove 78 spaces and would provide 12 for the proposed coffee shop, resulting in an overall loss of 66 parking spaces.

A drone survey has been carried out for the whole car park and shows that the maximum number of spaces occupied was 252. This would tally with the TRICS analysis, which suggested a peak parking on a Saturday with 247 spaces occupied. The proposed development would result in 435 spaces across the whole car park, which would be able to accommodate the parking demands from the three buildings on site (supermarket, coffee shop and hot food takeaway). Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

The proposed development would provide 12 spaces for the proposed coffee shop and would result in an overall loss of 66 parking spaces at the site.

Response to objectors

- The issue of competition is not a material planning consideration and cannot be taken into consideration.
- The issues relating to traffic have been addressed in the report above.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 190756-PLNG8, 190756-PLNG9A, 190756-PLNG7B, 190756-PLNG6B, 190756-PLNG5B, 190756, 190756-PLNG2C, 190756-PLNG1B, 4363 01, 0000/ME/001, 3D visuals and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The external finishing materials for the proposal hereby approved shall be those noted on plan reference 190756-PLNG9A.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.
4. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;

- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

6. Prior to the commencement of the development hereby approved, a scheme for the provision of electric vehicle charging points shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented prior to occupation of the building hereby approved

Reason. In accordance with paragraph 35 and 124 of the NPPF, to encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life.

7. No development shall commence unless and until surface water drainage proposals have been submitted to and approved in writing by the Local Planning Authority. The scheme should be in accordance with the submitted Surface Water Sustainable Drainage Assessment and must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented prior to first occupation and thereafter maintained.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

8. Foul and surface water shall be drained on separate systems.

Reason. To provide a satisfactory means of drainage pursuant to Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan.

9. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the building hereby approved being brought into use.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policy HT2/4 - Car Parking and New Development of the

Bury Unitary Development Plan.

10. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved by the Local Planning Authority and shall confirm/provide the following:
- Hours of operation and number of vehicle movements;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
 - Parking on site of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials;
 - Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations

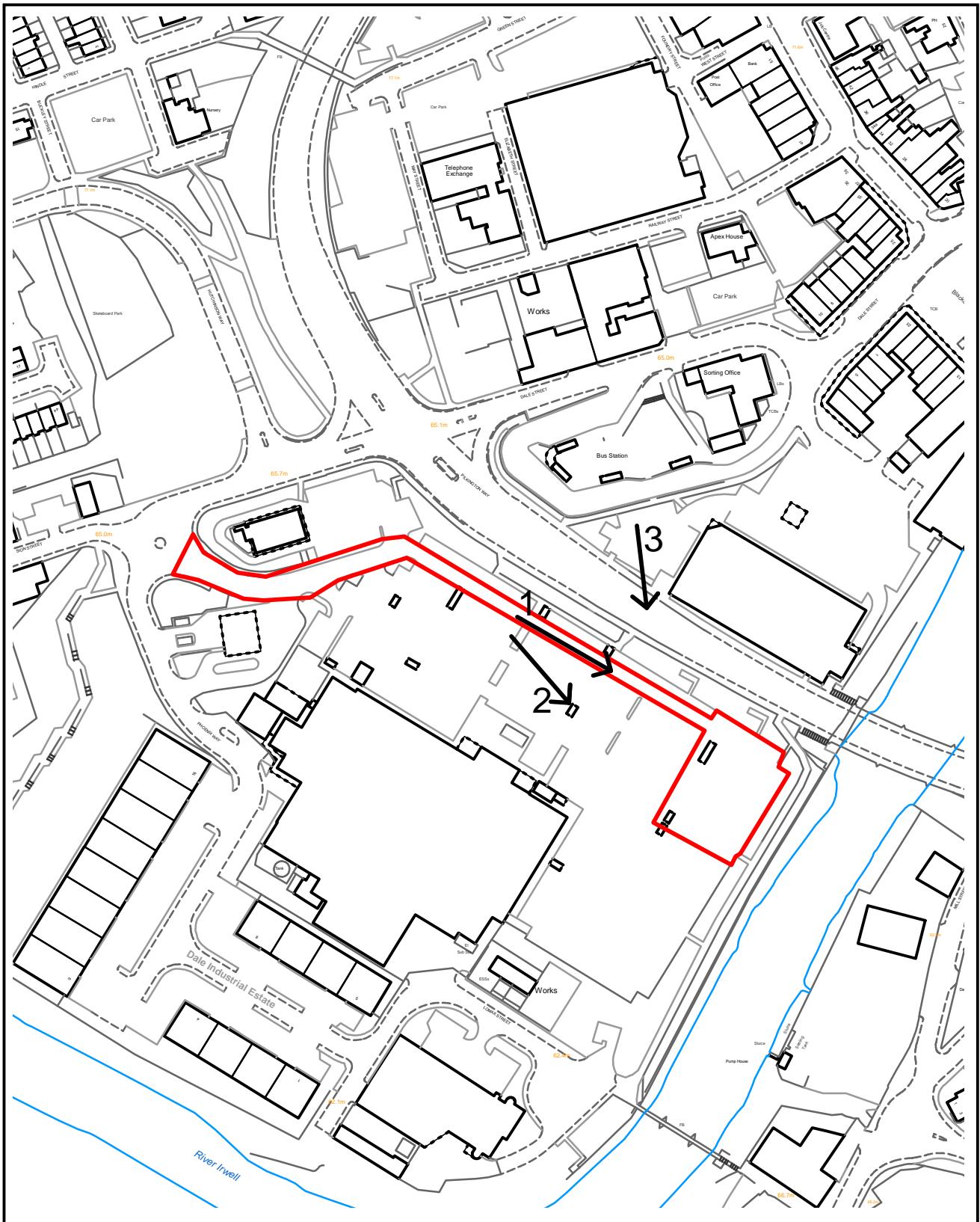
The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

11. The deliveries to the site shall only be carried out in accordance with the delivery management plan, dated June 2020.
- Reason. In the interests of highway safety pursuant to Policy S2/6 - Food and Drink of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Leach** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 64949

**ADDRESS: Asda Store, Pilkington Way
Radcliffe**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

Photo 1

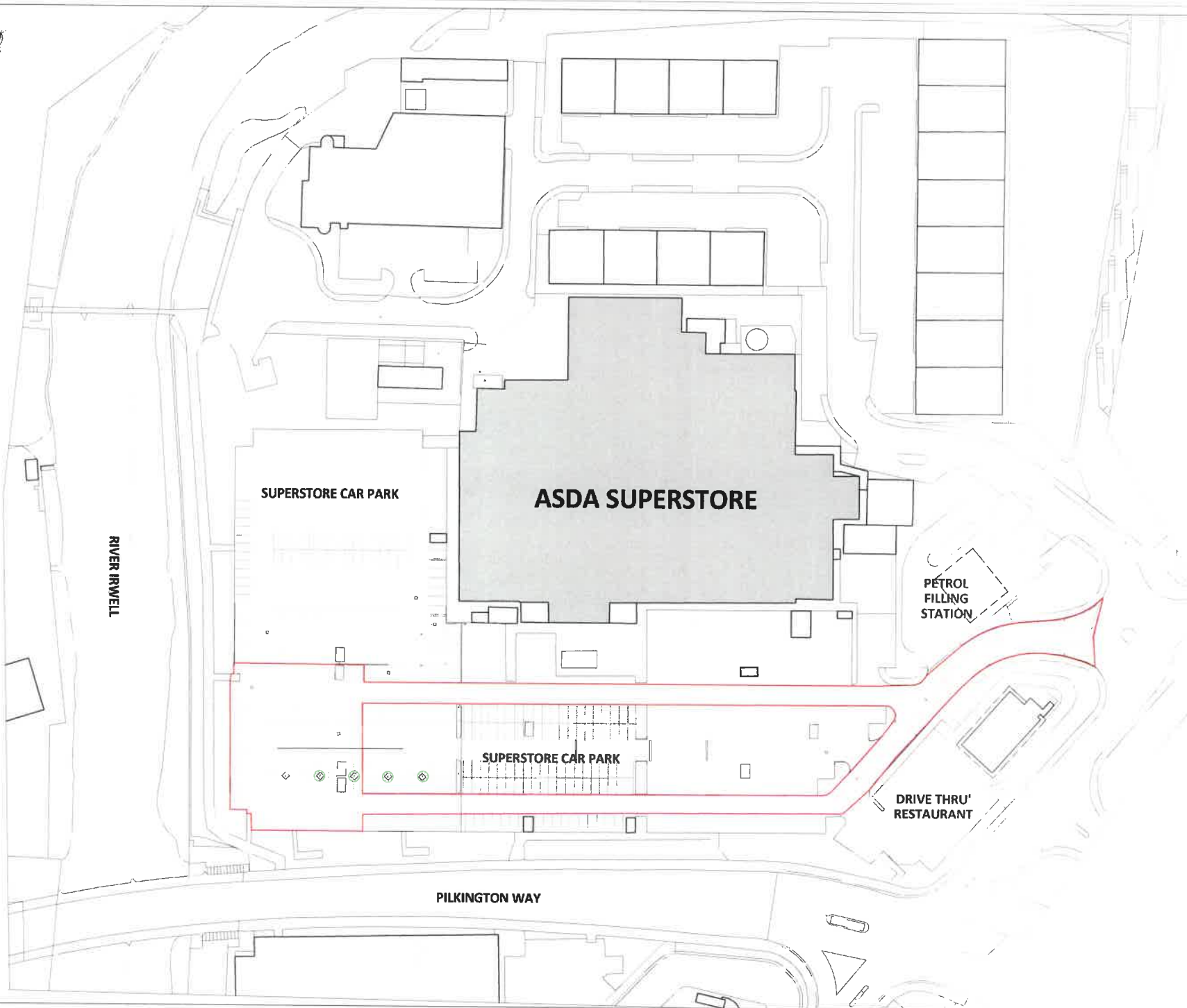


Photo 2



Photo 3





GENERAL NOTES

1. This plan has been prepared for planning submission only.
2. Any advertisement shown is subject to a separate application.

1	11.07.20	Red line boundary updated to include site area.	JS
2	11.11.19	Red line boundary updated to include site access arrangements.	MDR
3	19.11.19	Drawing title corrected	MDR

Rev	Date	Comments
STATUS	PLANNING	
PROPOSAL	Drive Thru Coffee Shop	
SITE LOCATION	ASDA RADCLIFFE Riverside Retail Park Radcliffe, Manchester, M26 3DA	
TITLE	EXISTING LAYOUT 500 SCALE	

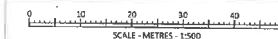


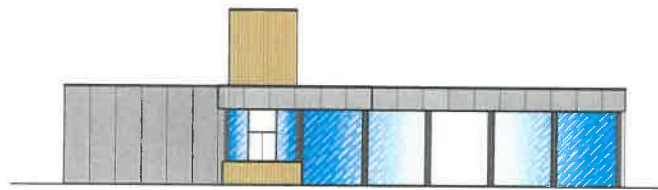
JENNINGS DESIGN LIMITED

York House, Valley Court, Canal Road,
Bradford, West Yorkshire, BD1 4SP,
TEL No. 01274 395422
FAX No. 01274 395427
E-mail office@jen305.com

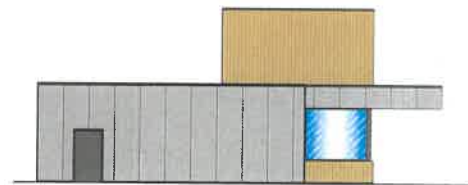
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Date:	November 2019	Scales:	1:500 @ A1

PLAN NO.	190756 - PLNG3C
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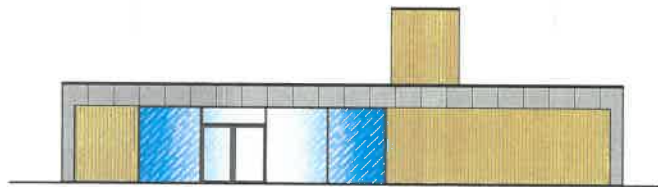




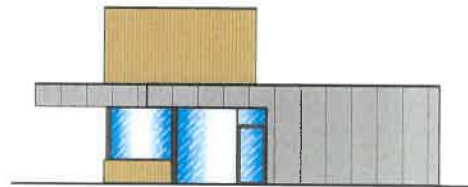
REAR ELEVATION



SIDE ELEVATION



FRONT ELEVATION



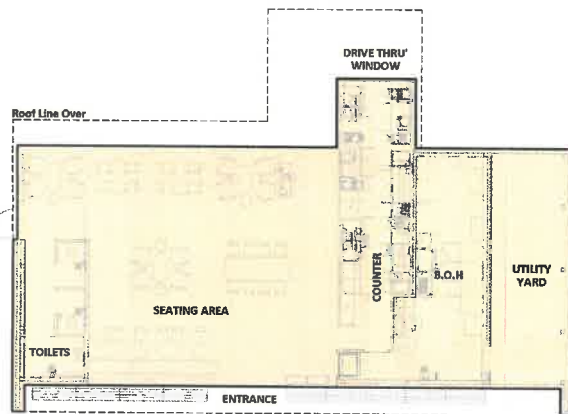
SIDE ELEVATION

GENERAL NOTES

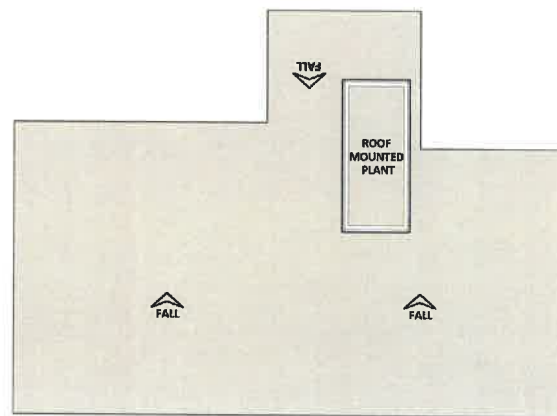
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MATERIALS LEGEND

- MOONSTONE RAL 7015 65100 AS
VERTICAL KINGSPAN WALL PANELS
- UMBRA GREY RAL 7022 - CAPPING,
STEEL & FLASHINGS
- ALUMINIUM FRAMED AND GLAZED
UNITS. FRAMES FINISHED IN 7036.
EXTERNAL DOORS FINISHED TO MATCH
- TIMBER CLADDING - COLOURS TBC



FLOOR LAYOUT



ROOF LAYOUT

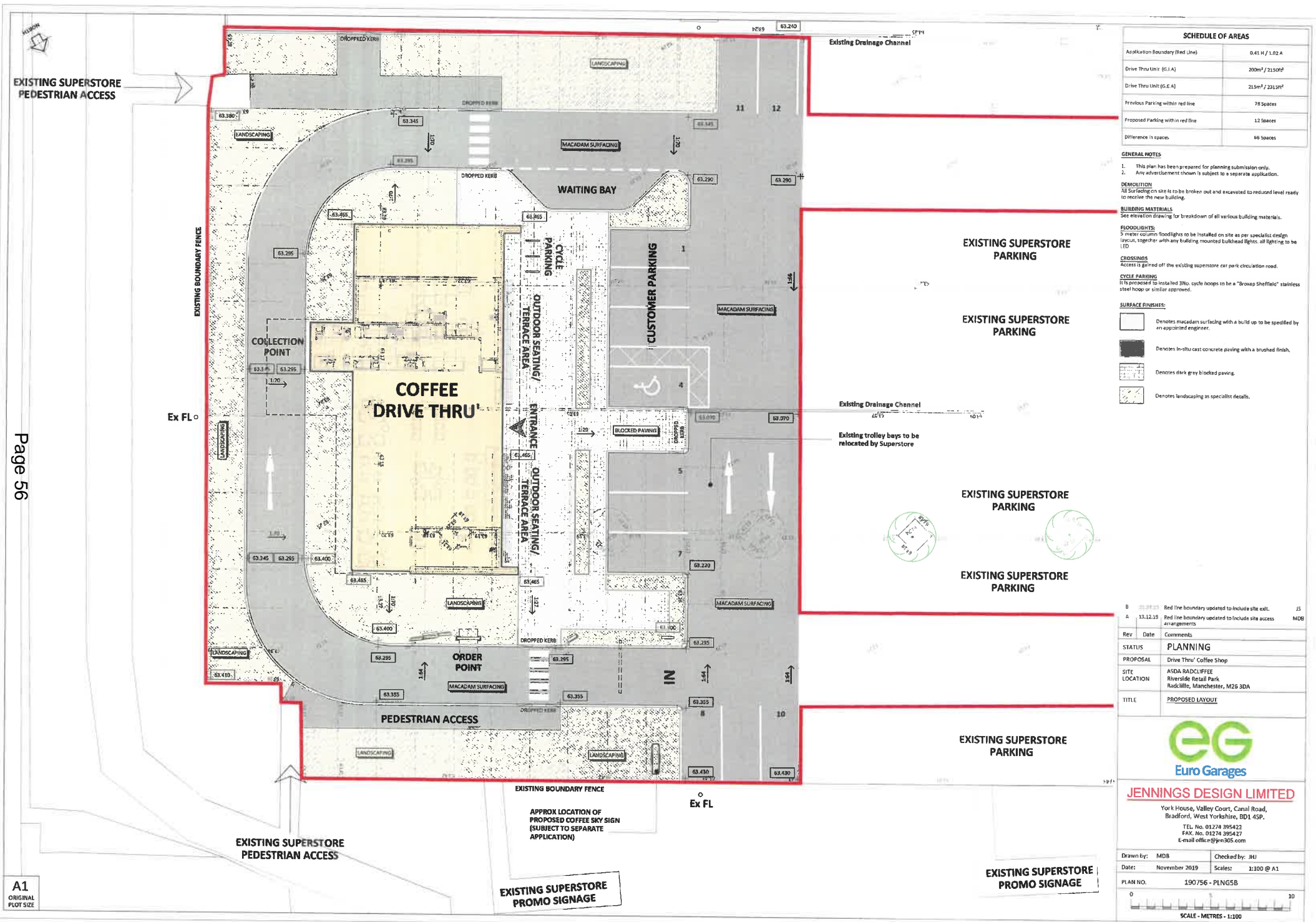
Rev	Date	Comments	MOB
A	06.12.19	Side elevation added.	MOB
STATUS: PLANNING			
PROPOSAL: Drive Thru Coffee Shop			
SITE LOCATION: ASDA RADCLIFFE, Riverside Retail Park, Radcliffe, Manchester, M26 3DA			
TITLE: PROPOSED BUILDINGS LAYOUT & ELEVATIONS			



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Date: November 2019	Scales: 1:100 @ A1
PLAN NO. 190756 - PLNG9A	
SCALE - METRES - 1:100	



SCHEDULE OF AREAS	
Application Boundary (Red Line)	0.41 H / 1.02 A
Drive Thru Unit (G.I.A)	200m ² / 2150ft ²
Drive Thru Unit (G.E.A)	215m ² / 2310ft ²
Proposed Parking within red line	78 Spaces
Proposed Parking within red line	12 Spaces
Difference in spaces	66 Spaces

GENERAL NOTES

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- Any advertisement shown is subject to a separate application.

DEMOLITION:
All Surfacing on site is to be broken out and excavated to reduced level ready to receive the new building.

BUILDING MATERIALS:
See elevation drawing for breakdown of all various building materials.

FLOODLIGHTS:
5 meter column floodlights to be installed on site as per specialist design layout, together with any building mounted bulkhead lights, all lighting to be LED

CROSSINGS:
Access is gained off the existing superstore car park circulation road.

CYCLE PARKING:
It is proposed to install 3No. cycle hoops to be a "Brouap Sheffield" stainless steel hoops or similar approved.

SURFACE FINISHES:

- Denotes macadam surfacing with a build up to be specified by an appointed engineer.
- Denotes in-situ cast concrete paving with a brushed finish.
- Denotes dark grey block paved paving.
- Denotes landscaping as specialist details.

B	13.12.15	Red line boundary updated to include site exit.	J5
A	13.12.15	Red line boundary updated to include site access arrangements.	MD8
Rev	Date	Comments	
STATUS: PLANNING			
PROPOSAL: Drive Thru Coffee Shop			
SITE LOCATION: ASDA RADCLIFFE Riverside Retail Park, Radcliffe, Manchester, M26 3DA			
TITLE: PROPOSED LAYOUT			



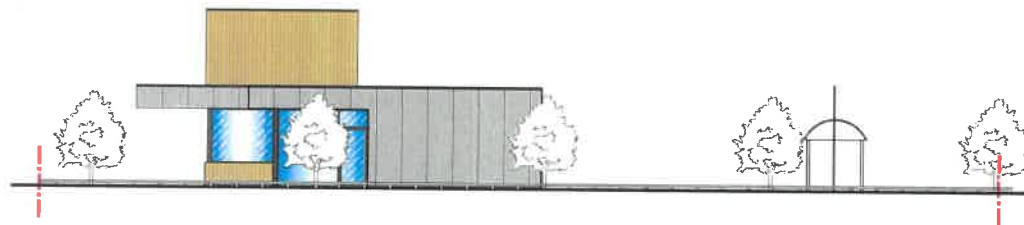
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 E-mail office@jen305.com

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Date: November 2019	Scales: 1:100 @ A1
PLAN NO. 190/756 - PLNG5B	
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SCALE - METRES - 1:100	

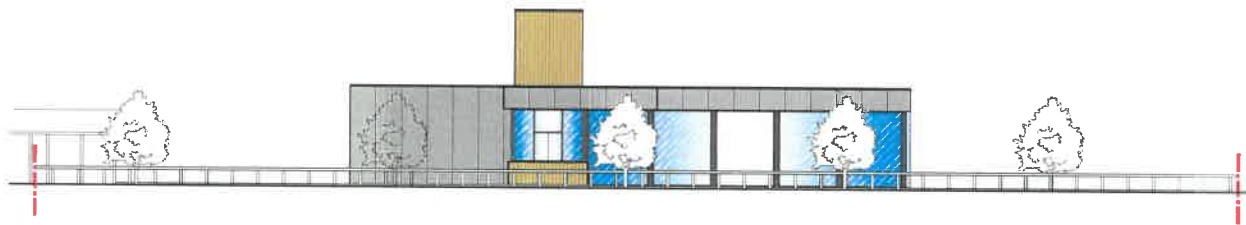
A1
ORIGINAL PLOT SIZE

GENERAL NOTES

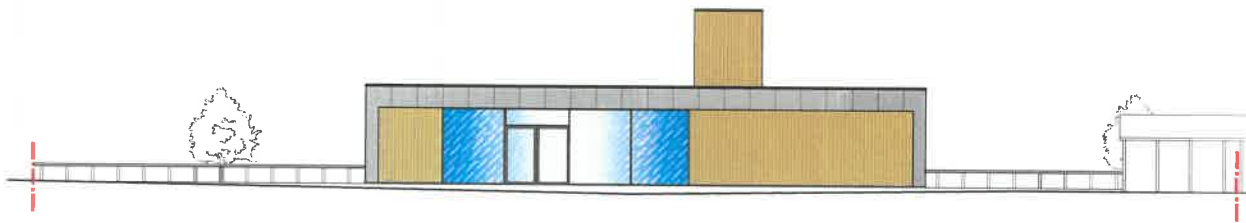
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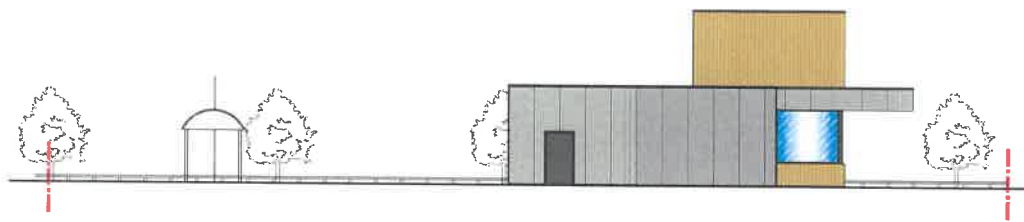
NORTH ELEVATION



EAST ELEVATION



WEST ELEVATION



SOUTH ELEVATION

Rev	Date	Comments
STATUS		PLANNING
PROPOSAL		Drive Thru Coffee Shop
SITE LOCATION		ASDA RADCLIFFEE Riverside Retail Park Radcliffe, Manchester, M26 3DA
TITLE		PROPOSED ELEVATIONS



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Date:	November 2019	Scale:	1:100 @ A1

PLAN NO.	190756 - PLNG8
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SCALE - METRES - 1:100

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Ward: Ramsbottom and Tottington -
Ramsbottom

Item 03

Applicant: Margaret Haes Riding Centre

Location: Margaret Haes Riding Centre, Moor Road, Ramsbottom, Bury, BL8 4NX

Proposal: Retention of welfare unit with associated landscaping to form welfare and security accommodation
(Temporary consent for 5 years)

Application Ref: 64955/Full

Target Date: 27/01/2020

Recommendation: Approve with Conditions

The application was deferred at the February 2020 Planning Control Committee meeting to carry out a site visit.

The application was postponed from the March and April 2020 Planning Control Committee Meetings. In response to the emergency Government instructions on Covid-19.

The application was deferred at the May 2020 and June 2020 Planning Control Committee meeting to carry out a site visit.

Description

The application site relates to a riding centre which is located on the edge of Holcombe Village and is within the Green Belt, Special Landscape Area and West Pennine Moors and the Holcombe Conservation Area. The site is accessed via Moor Road, an unmade and unadopted access road which is a designated bridleway and leads to residential properties, Higher Barn House and Higher barn being the closest to the north of the site. To the east and at a lower level is the Emmanuel C of E Primary School which fronts Helmsshore Road and to the south is Higher House Cottage and Higher House at the lower end of Moor Road. To the west, the land rises steeply from the site and is part of Holcombe Hill.

Part of the riding school when viewed directly to the east from Moor Road is elevated and separated by a steep embankment which forms the boundary. As Moor Road rises northwards, the access into the site becomes level with the road.

The riding school comprises a number of buildings and facilities which would be expected of a facility of this type, namely an indoor and outdoor riding arena, stables and tackroom. The buildings are located more or less centrally within the site, close together around the indoor and outdoor arenas and there is a concrete stable yard which leads from the access into the site.

The applicant has sited a green metal container on a concrete apron located on the eastern edge of the site directly adjacent to the embankment with Moor Road without planning permission. The container is 9.75m long, 3m wide and 2.7m high.

Following enforcement processes, the application has been submitted retrospectively. The application originally sought retention of the welfare unit in its current position. Following negotiations with the LPA, the scheme has been amended to re-locate the build and make changes to the external appearance.

It is proposed to re-site the container approximately 10m west, adjacent to the outdoor arena and behind the line of the tackroom and stables. The new location would require minor re-profiling and grading of land to form a base which would be level with the riding yard.

It is also proposed to re-clad the entire exterior of the building with timber boarding and form a mono pitch grey felt shingle roof with an overhang canopy at the front, akin to a stable or shed type structure.

The riding school specialises in offering lessons and activities for those with special needs as well as the able-bodied, and of all ages. The applicant states that the development is required to provide an ancillary resource in association with the riding school for a small administrative office and to enable better and secured welfare facilities for staff, visitors and users of the riding facility. The building would also enable natural surveillance over the entrance to the site which is particularly important and crucial to the well-being of the children and other vulnerable users who attend the school.

For clarity, the applicant has confirmed that the timber framed building which is located between the indoor and outdoor arena is an existing build is to be converted to provide accessible changing rooms and toilet facilities and covered area for parents and carers to spectate both arenas. It does not form part of this planning application and planning permission is not required for the conversion works.

Relevant Planning History

47759 - Erection of building to include new loose boxes, equipment store and disabled wc's
- Withdrawn by Applicant 01/05/2007
48135 - New loose boxes plus w.c. for the disabled - approve with conditions 27/06/2007
19/0292 - Enforcement case - Storage containers

Publicity

Letters sent on 9/12/2019 to 11 properties on Moor Road, Helmshore Road and Holcombe Village.

Site notice posted 18/12/2019

Press advert in the Bury Times 19/12/2019.

15 Letters of objection received in total.

The following commented on the original submission - Nos 12, 14, 18 Helmshore Road, Harcles Hill farm, 155 Lumb Carr Road, 1 Lumb Carr Road, 103 Holcombe Old Road, Higher Tops barn, Higher Barn Farm, Higher Tops Farm, c/o Dawes Bank Holcombe Old Road, Higher House, Tower View.

The issues raised were:

- Object to the size, industrial appearance and position on the edge of the lane - very visually prominent.
- Conflicts with the landscape and question whether it is in keeping with conservation regulations.
- Possibility of the relocation of the unit with appropriate screening of the unit at the very least?
- Not ideal construction in an SSSI and Conservation Area and does not enhance the character of the area.
- Large amounts of earth and other materials have been added on top of a very steep bank that is only supported by a dry stone wall - parts of the wall have already fallen down as a result and any slippage in the ground would result in more wall and possibly the bank falling onto Moor Road which is the only access to my property and other farms. Dangerous location.
- Works should be carried out to ensure the bank is properly supported and earth/sand/hardcore would not wash down the bank and into the drainage ditch below, which would result in further silting up of a drainage system already causing problems.

- Disappointed the applicant has not consulted the Holcombe Society of which he is a member.
- Looking at the plans the location of trees has been changed on a photograph which does not show true representation at all of the situation. All in all this is something which should not have been placed in this area and need to be removed and a relevant aesthetically pleasing natural material construction be put forward.
- Contravenes the special Article 4 Conservation Area character
- One side appears to have some artificial foliage attached, but the sides remain ugly and out of keeping with the rural aspect.
- I bought a house in a conservation for a reason and not to look out on an industrial unit that is totally incongruous with the natural environment. I am very concerned the charitable nature is only a ruse to secure residential accommodation and either way this should not be allowed to go forward. Our countryside needs protecting as well as the integrity of applications.
- Bury Council should send out a very strong message to the owner of the land, that you simply cannot put any unsightly units on your land, whatever the use.
- There are no contracts for the riding schools attached to the deeds - this is an obvious exit plan for the owner - your ability to grant permission paves the way for it to change to a residential development.
- My main concern is the underpinning legality of the application which I would strongly recommend that the Planning Officers investigate with legal specialists if need be. I do not believe that BMBC can permit an application if there is any doubt that the applicant is neither the owner nor the tenant in question.
- I strongly recommend that any Planning Permission granted is strictly tied to the Riding School as tenants (with the caveat of course that this is proven) and cannot roll on to benefit change of the use of the land to say an urban development.
- Visually ghastly
- The location of the container is incorrectly depicted on the plan.
- Inconsistent with the claim for needing security as it is so distant from the access slope.
- The recently constructed floodlights cannot be justified.
- It is in full view of houses and the iconic Peel Tower.
- Would be better as custom built next to the original stables.
- What is the other new block specified on the plans but not referred to in the application?
- Are water mains available for the facility?
- An inaccuracy of the application is the access track up a slope from Moor Road to the Riding School on the agents plan which suggest this is all the long term property of the Riding School under the terms of the lease.
- Strongly recommend a site visit by Councillors and officers
- Describing the site as 'brown field' is false. It is in a conservation area, is green belt and borders National Trust Land. It also adjoins a main route used weekly by hundreds of walkers going to Bury's true iconic Peel Tower.
- Policy is incorrectly stated in the application
- Am sure everyone would like the centre to have secure facilities but the location of the container is not appropriate.
- The landscaping is a concern as the banking to the boundary with Moor Road is already compromised.
- If permission is to be granted it should be temporary to allow for long term more suitable plans.
- Does not preserve the openness of the Green Belt would conflict with the NPPF and therefore very special circumstances would be needed.
- Heritage - Grade II Listed Higher House and Holcombe Church - considered to harm the setting.
- If approved a condition should be attached to require its removal in the event it is no longer needed.
- As a separate matter, apparent/potential planning breached at the overall site should be investigated.
- Concerns are compounded by the fact that the agent for this application has a track record of large scale development and no record of working with charitable organisations (see their website).

- I would have no objections if such were built in a safe and sympathetic manor in keeping with the surroundings.
- Should take into account the line of sight judgement as on other applications

Revised plans received to relocate the welfare unit in the site together with proposals to timber clad the exterior of the building and form a mono pitch to the roof.

Those originally notified and those who have made previous representations to the application were re-notified of the revisions by letter on 28/1/2020.

Letters of objection received from Nos 1, 155 Lumb Carr Road, Harcles Hill Farm, 12 Helmshore Road, Tower View Lumb Carr Road, Higher Barn Farm, Higher House

- After consideration of changes, it is still a grotesque structure which has no place in a conservation area.
- This will change the outlook of the view in this village which should be cherished.
- The prime issues here are Green Belt stringent regs. coupled with such a High Grade Conservation Area protection, balanced against possible exceptional circumstances which would qualify breaking these stringent constraints legally. I, as an amateur, can see no case for such.
- To reason that a Shipping Container, clad or not clad, with no proper foundation - hence certainly not a 'building', no conventional heating in such an exposed position, no sanitation, sewerage, toilets or running water (food is specified as being consumed, with no facility for washing hands after dealing with horses !) can be classed as a welfare unit for the disabled is frankly ludicrous. Far better for the Riding School Chair and Officers to have constructed a purpose built unit, most favourably within the footprint of the original building (plenty of space), incorporating all such services under one roof perhaps even with showers and hot towns water to service. Investment seems to have been mismanaged.....
- And further two other such temporary constructs of truly ugly character, quite inappropriate to our treasured hill have come to my attention already on site. Should these be included in a planning application at this very late stage ?
- The site owner is acting as the Chair of the riding school so there is clearly divided loyalty.
- Still have serious reservations regarding the application. Nothing has been done at all to address the risks to the steep bank overlooking Moor Road, which is the sole access for myself and other residents to our properties. The addition of extra weight (including large areas of concrete, which are not even mentioned in the application, and several wagon loads of hardcore and sand under them) has already resulted in part of the bank starting to slide down.
- There is also a section of the wall that has buckled out and is at risk of collapse into the drainage channel and onto the road.
- I will be contacting the Chief Planning Enforcement Officer to request a site inspection.
- The applicant is completely out of touch in the way they are trying to call the section of Land 'Brownfield' - this section of land is within the conservation area and therefore needs to be protected vigorously.
- Alarm bells ring as the owner is looking for a residential development on this site
- Its charitable status is only reason why it is viewed favourably.
- Unfair no enforcement action has been taken.
- Two other containers have appeared on site.
- Sanctioning the container clad or unclad would set a dangerous precedent for the future.
- Historically all local residents and other enterprises have been required to comply with the most stringent and rigorous planning regulations - allowing it would be inequitable and an insult.
- Cladding the container is a quick fix, cheap accommodation - should utilise and improve existing facilities and restore dilapidated buildings on site makes more sense economically and environmentally friendly.
- H&S concerns of safety and the concrete foundations.
- New location would be more visible from Moor Road from the entrance.

- Insufficient time for representations to be made
- the original plan and elevations bear no resemblance at all to the actual shipping container which had been placed on the site and therefore it is highly unlikely in my submission that the new proposal will bear any resemblance to the drawings.
- The new proposed siting of the shipping container is also still within the line of sight from Helmsshore Road, it was a pre-condition for a previous application (48135) that no structure should be visible.
- Should be a new application as the changes are material.
- Refer the committee to aerial photo which shows the concrete has been extended to a large area.
- It appears that the Council have been providing advice as to how the applicant can be amended.
- Floodlights have also been installed on the site which also should be considered by the council, no permission or consultation has been sought by the applicant for this.
- Strongly urge the committee to visit the site in order to appreciate the extent of the changes which have been made, and the council should bear in mind all the time that the applicant only has made this retrospective application in response to a visit by an enforcement officer.
- Failure to address the concern this is a permanent application for a non permanent need. The appropriate application should be for a temporary structure.
- Bury MBC has a duty to protect and preserve the Green Belt and conservation nature of this area.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection or comments to make

Borough Engineer - Drainage Section - No response received.

Environmental Health - Contaminated Land - No comments to make

Conservation Officer - No objection.

Public Rights of Way Officer - No PROW issues

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EC4/1	Small Businesses
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN9/1	Special Landscape Areas
OL1	Green Belt
OL1/2	New Buildings in the Green Belt
OL4/7	Development Involving Horses
OL7/2	West Pennine Moors
HT5	Accessibility For Those With Special Needs
HT5/1	Access For Those with Special Needs
SPD8	DC Policy Guidance Note 8 - New Buildings in the Green Belt
SPD10	Planning for Equestrian Development
EN7/2	Noise Pollution

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are

considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Green Belt Policies and principles

Paragraphs 143 and 144 of the NPPF state that inappropriate development in the Green Belt is by definition, harmful and should not be approved except in Very Special Circumstances (VSC). Planning Authorities should ensure that substantial weight is given to any harm in the Green Belt. VSC will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Paragraph 145 considers the construction of new buildings as inappropriate development in the Green Belt. Exceptions to this include b) the provision of appropriate facilities (in connection with the existing use of land or a change of use) for outdoor sport, outdoor recreation etc as long as the facilities preserve the openness of the Green Belt and do not conflict with the purposes of including land within it.

UDP Policy OL1/2 - New Buildings in the Green Belt states that the construction of new buildings is inappropriate development unless it meets one of the listed exceptions, which includes b) essential facilities for outdoor sport and recreation which preserve the openness of the Green Belt. Examples of such facilities include small changing rooms or unobtrusive spectator accommodation or small stables.

UDP Policy OL4/7 - Development Involving Horses states that the keeping of horses for recreational purposes or as part of commercially based equestrian activities will be considered acceptable where it would not have an adverse effect on the appearance of the rural areas. In particular, high standards of design, construction and maintenance will be expected as part of any development proposals.

UDP Policy EN9/1 - Special Landscape Areas states that development in such areas will be strictly controlled and required to be sympathetic to its surroundings in terms of visual impact. High standards of design, siting and landscaping will be expected. Unduly obtrusive development will not be permitted in such areas.

SPD 8 - New Buildings and Associated Development in the Green Belt states that essential facilities for sport and outdoor recreation can be viewed as being an appropriate use in the Green Belt

SPD 10 - Planning for Equestrian Development - Whilst mainly concerned with stables and riding arenas, SPD 10 also provides guidance on storage areas and other forms of equestrian-related development, advising that they should be sympathetic to their surrounding area in terms of siting, appearance and future maintenance of the Green Belt and other sensitive open land areas, and ensuring that the environmental quality and amenity is not harmed by inappropriate equestrian development. New buildings should be sited close to boundaries or adjacent to existing buildings and well designed with mono pitch or sloping roofs and of timber construction.

The proposed development would provide an ancillary building in association with an existing recreation facility. The welfare unit would be 9.75m in length, 3m wide and 3.5m at the highest point. It would be sited directly in front of the existing outdoor arena, close to and viewed against the backdrop of existing stables and the indoor arena. It is therefore considered that the proposed siting of the unit would preserve the openness of the Green Belt and would not conflict with the purposes of including land in it.

The proposed building would be 'free standing' and there would be limited ground works required to re-locate the building and being constructed of a metal material, for all intents

and purposes, the make-up of the structure is more of temporary type build.

It could be removed from the site at any time and therefore not a build which would permanently harm the openness of the Green Belt. Given the sensitivities of the location of the site, it is therefore considered that a grant for a temporary consent for a period of 5 years would be a reasonable approach and a condition to this effect would be recommended.

It is therefore considered that the proposed building would be appropriate development in the Green Belt to provide a facility in connection with the existing riding centre. The proposed development would continue to preserve the openness of the Green Belt and would therefore comply with point b) of para 145 of the NPPF and would therefore be acceptable.

Heritage and Conservation principles

Paragraph 189 of the NPPF states that in determining applications, LPA's should require an applicant to describe the significance of any heritage assets affected, including any contribution to their setting. The level of detail should be proportionate to the asset's importance and no more than is sufficient to understand the potential impact of the proposal on their significance.

Paragraph 190 states that Local Planning Authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

In paragraph 192, LPA's should take account of:

- the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- the desirability of new development making a positive contribution to local character and distinctiveness.

UDP Policies EN2/1 - Character of Conservation Areas and EN2/2 - Conservation Area Control policies only support proposals where it can be demonstrated that development would preserve or enhance the special character of the area. Proposals will encourage measures to retain, replace and restore features of historical and architectural interest, remove dereliction and bring unused land or buildings back into beneficial use, have regard to the special character of the surrounding area and in the case of re-use of buildings or introduction of new uses, the impact of the proposal on the character or appearance of the area and the fabric of the existing building.

In view of para 189 of the NPPF, the applicant has been asked to provide a Heritage Assessment of the development which has been submitted as an addendum to the Planning Statement.

The Holcombe Conservation Area Appraisal summarises the special characteristics of the Conservation Area, identifying Moor Road as a key feature of the area and which is described as a main focus of historic development and an old packhorse route. The Appraisal also refers to the riding stables as being included within the Conservation Area with views of the site are both close to and from wider vantage points.

The re-siting of the welfare unit back away from its exposed position would place it in the context of the existing stable buildings. With the proposed timber board cladding and mono-pitched felt roof it would essentially have the appearance of a stable building within a group of others. The use of the building would be consistent and appropriate to the

context of the site and would provide a facility of benefit to both staff, visitors and users of the riding school, enabling the facility to continue to contribute a valuable facility for the local and wider communities, and in compliance with para 192 of the NPPF.

The physical size and scale of the development would be relatively small scale within the context of the wider Conservation Area and the site itself and in considering the proposed relocation of the building and visual conversion, it is considered there would be no harm caused to that part of the conservation area and as such would preserve its character and appearance.

In terms of its proximity to Listed Buildings, those nearest are Holcombe Emmanuel Church over 140m away to the east and Higher House 65m to the south, with a number of Non Designated Heritage Assets (NDHA) located to the north, east and south. The development would not be viewed within the setting or proximity of any of these Heritage Assets. It would be a significant distance away and separated by intervening landforms and other properties. It is therefore considered the development would not have a detrimental effect on the historical character or importance of the Listed Buildings or NDHA's.

It is therefore considered that the proposed development would continue to preserve the special character of the heritage assets of Holcombe and would comply with the principles of the NPPF and UDP Policies EN2/1, EN2/2 and EN2/3.

Siting and visual impact - The unit currently sits in a prominent and highly visible location when viewed not only from both directions and approaches along Moor Road which runs along the eastern perimeter of the site but also from the west where the land is at a much higher level on Holcombe Hill.

It is proposed to re-locate the unit in front of the outdoor arena and close to the stables and tack room within the stable yard. The land on which it would be sited is on a slight rise and banking. It is proposed to excavate and re-grade the land to form an area level with the existing concrete hardstanding yard on which the unit would stand.

Once moved, the unit would have a much lesser visual impact when viewed directly from the eastern part of Moor Road which is at a significantly lower level and screened by vegetation, and when viewed approached from further away along Moor Road to the north and south, views of the unit would be far less perceptible.

To the west and the land which is much higher on Holcombe Hill, the unit would be a more visible feature. However, for the most part, it would be the roof and front elevation only which would be seen and given its setting and proximity to the other built forms on site, together with the proposals to re clad and re-roof the build (see section below), it would be viewed as a simple, modest and appropriate addition to the riding school.

It is therefore considered that the proposed siting would not have a detrimental, negative or damaging impact on views and visual amenity of the area and would comply with UDP Policies OL1/2, OL4/7, EN9/1, SPD8 and SPD10 and the principles of the NPPF.

Size, design and appearance - The existing building is currently a green metal shipping type container, and of a somewhat incongruous and discordant feature within the landscape and more so due to its prominent siting. OL4/7 - Development Involving Horses and SPD's 8 and 10 clearly state that development should not have an adverse impact on the appearance of rural areas and in particular, high standards of design would be expected. Green Belt, Special Landscape and Conservation policies stress the importance of sensitive development and again with high expectations of siting, design and appearance.

The application has been revised from the original submission and it is now proposed to entirely clad the unit in a timber boarding, adding a grey felt mono pitched roof and overhang to the frontage. The facility would appear as a timber shed or stable and one

which is commonly found and seen in such rural and countryside settings.

The unit is 9.75m long, 3m wide and 3.5m maximum height and of a similar size and scale to a stables or storage type building which would be typically associated within a riding stables. Located within the site and nestled close to the other equestrian facilities, it is considered that the unit would not be an incongruous addition or incompatible with the existing buildings and therefore considered acceptable.

However, it is acknowledged that for all intents and purposes, the build is a metal container type and one which is not permanently fixed by foundation works within the ground. It is therefore considered to be a more temporary solution to other builds which are located within the Riding School site. As such, a temporary consent for 5 years would be a considered and balanced approach and this would be recommended by condition, to which the applicant has agreed.

It is therefore considered that the proposed works to the externality of the unit would sensitively acknowledge and sit comfortably within the setting of the riding arena and within its locality. A suitably worded condition would be recommended to ensure that the works to re-locate and overclad the exterior of the building would be carried out within a given time. It is considered that due to the technical and practical matters involved of moving and re-cladding, 90 days is considered a reasonable and achievable timescale for completion, to which the applicant has agreed.

It is therefore considered that the proposed development would therefore comply with UDP Policies OL1/2, OL4/7, EN9/1, SPD8, SPD10 and the principles of the NPPF.

Impact on residential amenity - The nearest residential properties are those to the north, at Higher Barn Farm approximately 85m away and Higher House and Higher House cottage 65m to the south. To the east, the Emmanuel Primary School is located 50m away and at a lower level to the riding school and beyond are houses in Holcombe Village which front onto Helmshore Road.

The building would be sited within the riding school and screened from views from the north by the existing structures and from the south by the intervening landform and landscaping. These houses are also a significant distance away from the site and it is considered that their amenity would be not be detrimentally affected by the development.

From the east, the difference in levels and topography of the land, the steep gradient of the embankment and landscaping would also substantially screen the unit from this area of the village and it is considered that impact on views and outlook would be relatively insignificant.

The building would be used as a welfare facility to provide for the existing staff, parents, carers and visitors and as such would not generate any more trips or activity to the site than already exists, and therefore would be negligible in terms of any additional noise and disturbance created within, from or to the site.

It is therefore considered that the proposed development would not have a detrimental impact on residential amenity and would comply with UDP Policies EN1/1, EN1/2 and EN7/2 and OL4/7.

Response to objectors -

- The LPA is required to assist an applicant where it can remedy unauthorised works and this includes the ability to submit applications retrospectively and dealing with them (article 35(2) TCP (General Development Management Procedure)(England) Order 2015. "The Planning Act 1990 makes it clear in s179 that "it shall be a defence [for the perpetrator] to show that he did everything he could be expected to secure compliance" - this can include discussions, actions and including the submission of an application under s73A (retrospective - ie planning permission for development already

carried out). The applicant has been responsive in this respect and discussions have been on-going up to this time of presenting the application before the Planning Committee.

- It is considered a simple refusal of the planning application would not have assisted in any positive recourse or solutions. The LPA has worked with the applicant to progress proposals which would provide the best and most positive solution to the situation.
- The NPPF states, at Para 38, that LPA's should approach decisions on proposed development in a positive and creative way.....and work proactively with applicants to secure developments that will improve economic, social and environmental conditions of the area. Decision makers at every level should seek to approve applications for sustainable development where possible.'
- This application is not in connection with any proposals for the redevelopment of the site for any other purposes.
- All issues with regards to the siting and appearance of the build and impacts on the Green Belt, Conservation Area and Listed Buildings have been covered in the above report.
- With regards to the other 2 containers referred to. One is used as a secure tack room. The applicant states that this container was brought on several months ago to replace an existing. Notwithstanding this, the replacement container requires planning permission and this is being investigated. The other a smaller container type which appears to be a more recent addition. Again, the applicant will be advised to apply for a planning consent for its retention or remove it, if it has not been removed already.
- In terms of the safety of the embankment, the applicant has refuted that the siting of the container has resulted in earth and stone slippage from the embankment and onto Moor Road. Close inspection and photographs submitted show that part of the container has been positioned on concrete blocks on the upper part of the embankment to form a sturdy base. There does not appear to be any harm caused to the safety of the embankment and therefore it would not be reasonable to require the applicant to carry out a full survey or strengthening works. What would be reasonable, would be to include a condition that a scheme for the landscaping of the area be submitted for approval, that once the container is moved.
- In relation to application reference 48135 in 2007, this current application is judged on its own merits.
- The floodlights are not part of the current application and subject to an enforcement matter. The applicant has, however, clarified that lamps have been attached to existing structures and no new poles or lighting have been erected.

Update to the application since the deferment of the 24th March 2020 Planning Committee Meeting.

Publicity

Letter of support received from No 9 Astley Hall Drive with the following comments:

- The riding centre is a fantastic asset to the community as whole.
- My disabled daughter has been going to the centre for nearly 20 years with Jigsaw, another great community organisation.
- Over the last few years the facilities have slowly been improving with the work of the charity that runs the centre and the welfare unit is much needed, especially in inclement weather, which is not unknown in Holcombe!
- The centre have taken note of the comments of the local residents and re-sited the unit so it has limited visibility and will clad it so it looks like a stable.
- The development such is supported by local and national policy, particularly NPPF Policy 145 b) on Green Belt, which has an exception allowing buildings for recreational purposes and policy 96 promoting facilities for sport and recreation.
- The application has my wholehearted support, given the invaluable experiences they deliver to the disabled community and its compliance with policy.

Objection received from No 1 Lumb Carr Road with the following comments -

- Troubled that in the Supplementary information feb 2020 LPA meeting the case officer

recommends - "Permission is hereby granted for a limited period only, namely for a period expiring 5 years from the date of this decision notice.....unless a valid application is received by the LPA for its retention."

- To me this leaves the door open for future retention, and does not seem to completely exclude such, as I have been advised by officers was their intention. Their statement that ' A Shipping Container is always a Shipping Container' , which they claim to always be a temporary construction and never a true building seems at variance with the wording of this condition (Item 02). I strongly urge hence that the lack of opportunity to 'roll on' timber clad or not timber clad' over the 5 year restriction is tightly specified. And this clause removed to emphasise the regulated terminal time limit of this ugly artefact.

Response

The condition has been amended to read:

Permission is hereby granted for a limited period only, namely for a period expiring 5 years from the date of this decision notice, and the building and use comprising the development for which permission is hereby granted are required to be respectively removed and discontinued at the end of the said period and the land reinstated to its former condition.

Update to the application since the deferment of the May 2020 Planning Committee Meeting.

Publicity

Comment from Higher House, Moor Road:

- When this application was first raised it was decided that a site meeting would be conducted before a decision on planning was made. We are now living with Covid 19 restrictions and whilst I appreciate that the business of council must go on I question why THIS application has to go forward at this time. Particularly when a physical site inspection cannot now take place. There is no urgency as this structure has already been on site for at least 9 months.
- I firmly believe that a site inspection would enable the councillors to see whether what has been introduced on the site is really necessary and appropriate for the needs of the disabled riders. It would also give the committee the opportunity to speak directly to staff and volunteers within MHRS to ascertain if this structure is really needed and wanted by the charity, rather than simply to rely on the view and wishes of the landowner.

Response

The application was deferred from the May 2020 Committee meeting to carry out a site visit prior to the June Planning Committee meeting.

Update following the deferment of the application at the June 2020 Planning Committee meeting.

Publicity

Objection received from 1 Lumb Carr Road -

- Trying to convince us that a Shipping Container is an appropriate Welfare Unit, pretending it is urgently needed , when it has not been used for a year now, breaching Green Belt and Conservation Regs, attempts to suggest it is like a stable, though not accessible for horses - and now the danger of risking a string of ponies with disabled riders and carers/pedestrians in disarray in a new confined space of narrowed exit (horses hate) to fall on a new hard concrete surface already laid without permission..... Remote sanitation facilities though designated as an eating area.
- Parallel the attempt to convince that a security unit is essential - when to my knowledge there are no records of any thefts or intruders.
- Disrespectful to both the Community and the Disabled ; the latter it should be specially safeguarding.
- Far better to have a decent design incorporated within the footprint of the existing buildings, within easy reach of washrooms and toilets, and enabling Social Distancing

appropriately.

- With regard to the real site visit now pending I wish to request that the Committee members view the Shipping Container from Helmsshore Road itself to understand the strong objection of residents. Further I would request the Members actually walk up Moor Road so they appreciate the impact on this tourist path, and also on the School playground just below.
- Also that though deciduous trees partially mask this eyesore, for at least 6 months of the year these will not be in foliage.

Response to objections - a site visit will be carried out by the Planning Committee prior to the meeting. All other issues raised have been covered in the main officer report

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

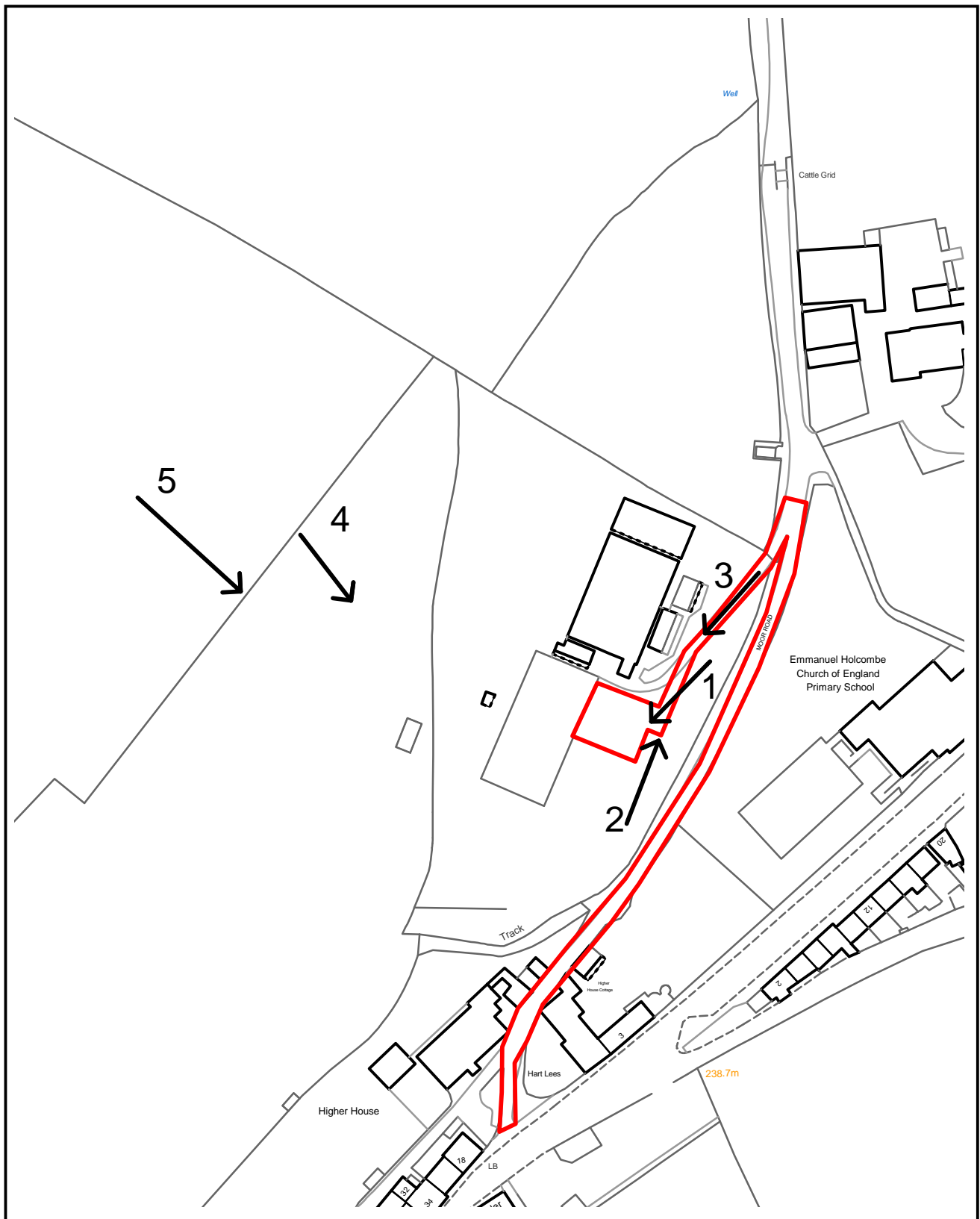
Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings - Site location plan 10910 01 D; Proposed site plan (red edge) 10910 L04 A; Proposed site plan 10910 L05 A; Proposed plan and elevations 10910 E02 B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3.
 - The welfare unit shall be relocated to the approved siting as shown on plan number 10910 L04 Rev A within 60 days of the date of this permission.
 - The recladding to the welfare unit as shown on plan number 10910 E02 Rev B shall be carried out within 30 days of being relocated and thereafter maintained as such.
 - A scheme relating to the landscape (hard and soft) reinstatement of the former site and timetable for implementation shall be provided within 28 days of the relocation of the welfare unit and be carried out in accordance with the approved details and timetable.Reason. In the interests of visual amenity and to ensure preservation of the special character of the area pursuant to Policies OL1/2 – New Buildings in the Green Belt, OL4/7 - Development Involving Horses, EN9/1 - Special Landscape Areas, EN2/1 - Character of Conservation Areas and EN2/2 - Conservation Area Control of the Bury Unitary Development Plan and the principles of the NPPF.
4. Permission is hereby granted for a limited period only, namely for a period expiring 5 years from the date of this decision notice, and the building and use comprising the development for which permission is hereby granted are required to be respectively removed and discontinued at the end of the said period and the land reinstated to its former condition.

Reason. In view of the temporary nature of the building hereby approved and having regard to the particular nature of the site and surroundings pursuant to Policies OL1/2 – New Buildings in the Green Belt, OL4/7 - Development Involving Horses, EN9/1 - Special Landscape Areas, EN2/1 - Character of Conservation Areas and EN2/2 - Conservation Area Control of the Bury Unitary Development Plan and the principles of the NPPF.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 64955

**ADDRESS: Margaret Haes Riding Centre, Moor Road
Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

64955

Photo 1



Photo 2



64955

Photo 3

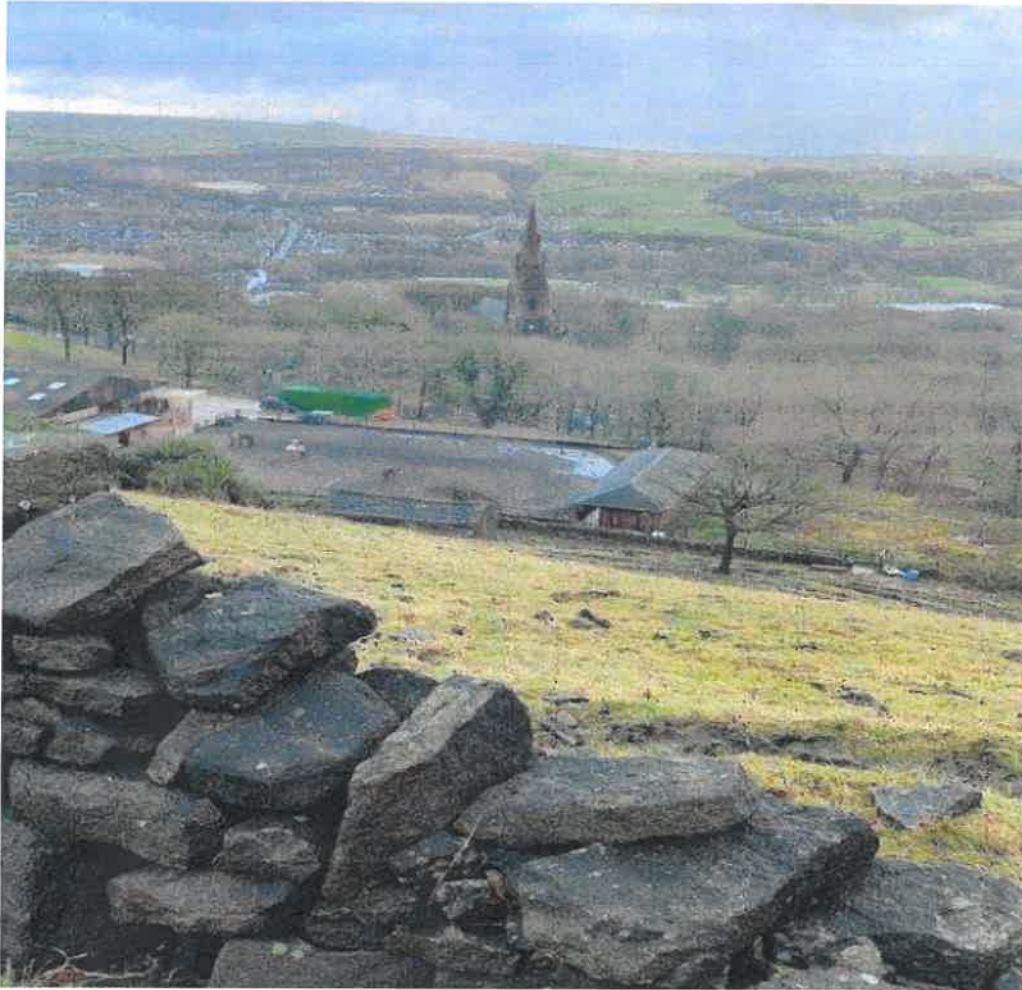


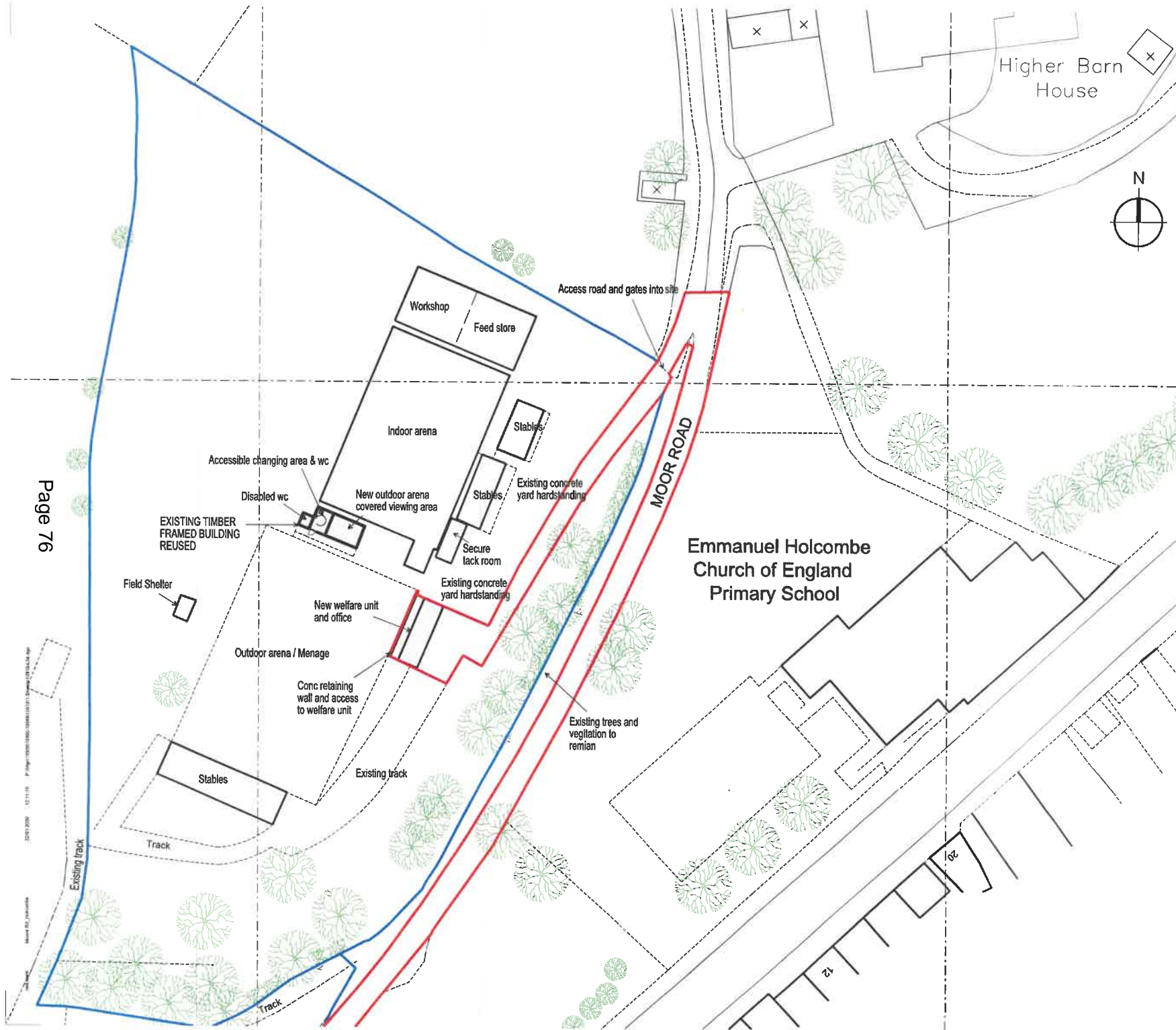
Photo 4



64955

Photo 5





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SITE SPECIFIC HAZARDS

IN ACCORDANCE WITH THE REQUIREMENTS OF THE COM REGULATIONS 2015 THE FOLLOWING SIGNIFICANT RESIDUAL HAZARDS HAVE NOT BEEN DESIGNED OUT OF THIS PROJECT AND MUST BE TAKEN INTO CONSIDERATION BY CONTRACTORS PLANNING TO UNDERTAKE THE WORKS SHOWN ON THIS DRAWING:

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NOTES

— ASSUMED SITE BOUNDARY
— OTHER LAND IN THE OWNERSHIP OF THE CLIENT

SCALE
0 5 10 15 METRES

REV. A 22.01.20 UPDATED TO CLIENT COMMENTS RSL
REV. DATE NOTES INIT.

CLIENT / PROJECT
JEREMY BUCKLEY
HOLCOMBE RIDING SCHOOL
MOOR ROAD, HOLCOMBE
DRAWING TITLE
PROPOSED SITE PLAN

STATUS
PLANNING

DATE 21.01.20 DRAWN RSL SCALE @ A3
1:500

PROJECT NUMBER	UNIT / BLOCK	CI / SPN CODE	TYPE & NUMBER	REVISION LETTER

DRAWING NO.
10910 L04 A

Site Location Plans	L	GA Plans	P	Elevations	B
Sections	S	Details	D	Profile Colour	C

THE RATCLIFFE GROVES PARTNERSHIP
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www.rgp.co.uk

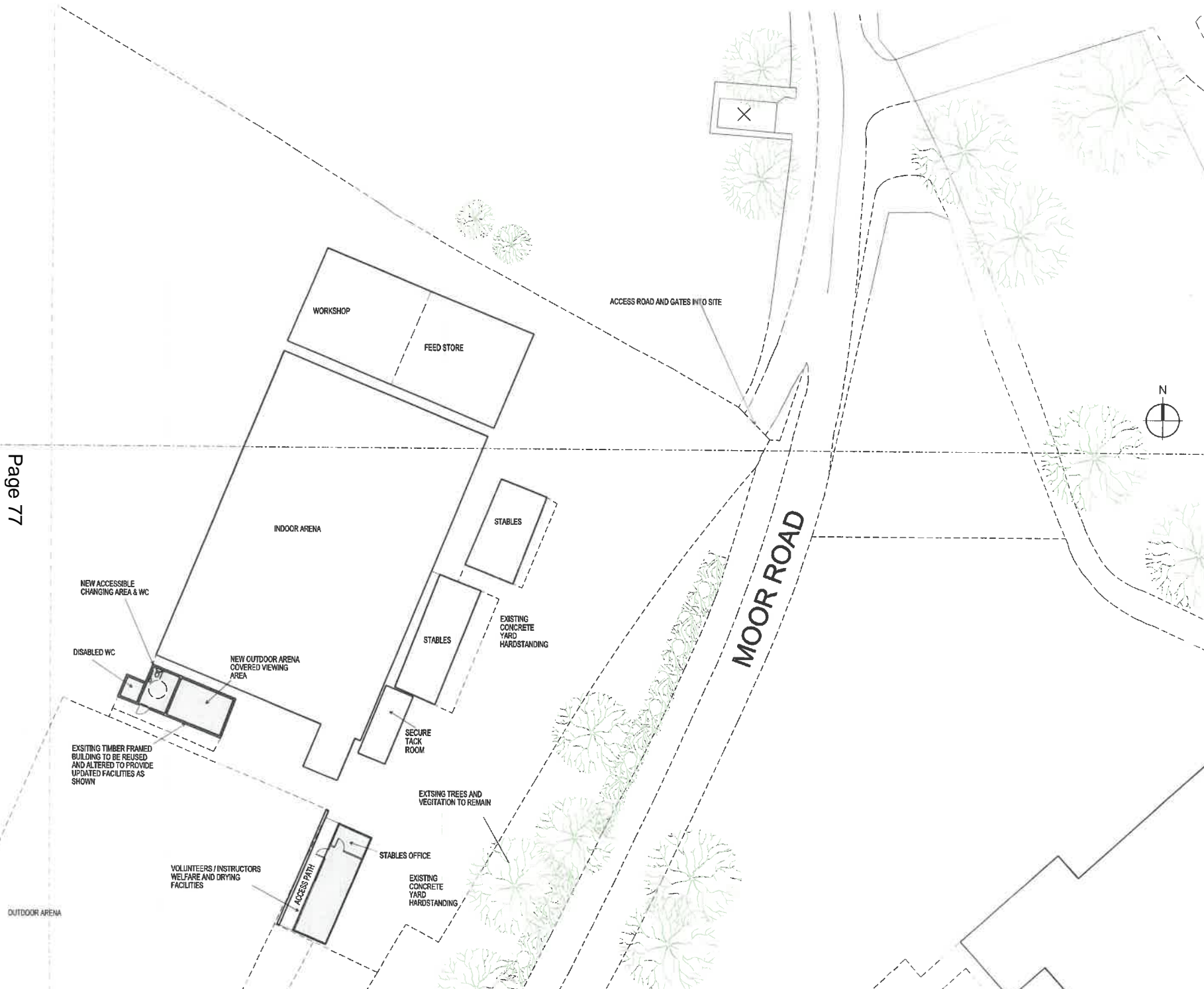
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SITE SPECIFIC HAZARDS

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REV.	DATE	NOTES	INIT.
REV.A	22.01.19	UPDATED TO CLIENT COMMENTS	RSL

CLIENT / PROJECT
JEREMY BUCKLEY
HOLCOMBE RIDING SCHOOL
MOOR ROAD, HOLCOMBE

DRAWING TITLE
PROPOSED SITE PLAN

STATUS			
PLANNING			
DATE	DRAWN	SCALE	INIT.
21.01.20	RSL	1:200	
PROJECT NUMBER	UNIT / BLOCK	CH / SP / CORE	TYPE & NUMBER
10910			L05 A

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RGp
 architects

SITE SPECIFIC HAZARDS

ALL WINDOWS TO BE SHUTTURED FOR
SECURITY



STEPS AND RAMP



TIMBER BOARDED FINISH
TO CABIN WITH GREY
FELT SHINGLE ROOF

EXISTING MENAGE WITH
TIMBER POST AND RAIL
FENCING

215MM CONC BLOCK RETAINING WALL TO
ALLOW GROUND LEVELS TO BE LOWERED
ADJACENT MENAGE WITH STEPPED ACCESS



MONO PITCH FELT
SINGLE ROOF FINISH

TIMBER BOARDED FINISH
TO CABIN WITH GREY
FELT SHINGLE ROOF



DOTTED LINE INDICATES
EXISTING GROUND
LEVEL

SIDE ELEVATION

CLIENT / PROJECT

CLIENT / PROJECT
JEREMY BUCKLEY
HOLCOMBE RIDING SCHOOL
MOOR ROAD, HOLCOMBE

DRAWING TITLE
PROPOSED PLAN AND ELEVATIONS

STATUS **PLANNING**

DRAWING NO. _____

10910 E02 B

Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Profile Colour	C

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Ward: Bury West - Elton

Item 04

Applicant: Mr Sanghani

Location: Land at Green Street, Bury, BL8 1TF

Proposal: Erection of 3 no. dwellings including associated parking and groundworks

Application Ref: 65293/Full

Target Date: 01/07/2020

Recommendation: Approve with Conditions

Description

The application site relates to a plot of land located on Green Street, a short cul de sac off Tottington Road. The land was formally the car park for the adjacent Globe Pub which has recently been granted permission for conversion to 2 residential units with an office element in part of the ground floor. It is understood conversion works are underway. The car park is now in a separate ownership and no longer associated with this property.

The site is separated from 'The Globe' by a setted paved access which leads to a single storey commercial outbuilding currently being used for storage which is located behind the site. The south east side of Green Street is characterised by traditional terraced properties, with No 1 Green Street in a former commercial use, now vacant. To the south west of the site, directly adjacent and separated by a timber panelled boundary fence are Nos 2-6 Green Street, a pair of more modern semi detached and a single detached property. To the rear of the site are houses on Lomond Drive which are screened by trees and a fence along this boundary.

Planning consent was granted for a pair of 4 bedroom semi-detached dwellings in May 2019. The proposed dwellings were two storey with a rear dormer for a room in the roofspace. Each property had its own access off Green Street which would lead to a driveway for 2 cars and the single garage. At the rear, a patio and garden area would provide private outdoor amenity space. Access would be retained to the outbuilding at the rear of the site which is outside the application area.

The proposed development involves the erection of 3 dwellings on the site. The proposed dwellings would be two storeys and there would be one detached and 2 semi-detached dwellings. Each property would have its own access off Green Street and there would be parking for 2 cars per property. Access would be retained to the outbuilding at the rear of the site, which is outside the application area.

Relevant Planning History

62205 - Erection of 4 dwellings at land north of Green Street, Bury. Withdrawn - 16 January 2018

63095 - Erection of 4 dwellings at land north of Green Street, Bury. Withdrawn - 13 September 2018

63834 - Erection of 2 semi -detached dwellings at land north of Green Street, Bury. Approved with conditions - 22 May 2019.

Adjacent site

61527 - Conversion of public house/bistro into 2 no. residential units at The Globe Inn, 325 Tottington Road, Bury. Withdrawn - 21 July 2017.

61758 - Change of use of former public house/bistro (Class A4) to 2 no. residential units (Class C3) and change of use of part of ground floor to office (Class A2) with external alterations at 325 Tottington Road, Bury. Approved with conditions - 14 September 2017

Publicity

The neighbouring properties were notified by means of a letter on 19 May 2020.

4 letters have been received from the occupiers of 1, 5 Green Street, 327 Tottington Road and Prospect House, which have raised the following issues:

- Parking on this street is very difficult because of the vehicles from existing residents and businesses on Green Street and Tottington Road.
- On week days, every parking space on both sides of the street is taken.
- The last application was for 2 dwellings, this is for 3. The new dwellings will remove on-street parking for the existing residents.
- The owner should provide 7-8 spaces for use by the residents with a residents only permit scheme.
- At present the residents of Green Street are having to park, half on the pavement and half on the road to allow vehicles enough room to get up or down the road, in single file. If we parked legally there would not be enough room for vehicles to pass.
- This would also mean that the residents would be physically unable to park on the driveways of the proposed new properties, as their ability to turn in to the new proposed drive ways would be too sharp of a turn, and risks damage to their own vehicle and legally parked residents' vehicles.
- 4 trees were destroyed by the previous owner and these were to be replaced on the previous application.
- It seems from the drawing that they intend to infringe on the back street cobbled road which is used for parking and access to the rear of all the property.
- This would cause a lot of problems for residents of Green street and Tottington Road as parking is already a problem.
- The proposed development encroaches on our land. The developer has removed our fence, which we have reinstated.
- The proposed plans develop over the rear street that we have right of way over.

The neighbouring properties have been notified of revised plans by means of a letter on 26 June 2020.

1 letters have been received from the occupiers of 13 Green Street, which has raised the following issues:

- I object to this development as there is very restricted access and parking on the street, the majority of the time there is no parking and you have to find backstreets to park in, with the new houses the street will feel claustrophobic and create less parking
- Also the months of noise, dirt, dust, building work, and machinery blocking the street will cause issues for residents.

The objectors have been notified of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objections in principle. Further comments to be reported in the Supplementary Report.

Drainage Section - No response.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Waste Management - No response.

United Utilities - No response.

Pre-start Conditions - Agent has agreed with pre-start conditions.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN5/1	New Development and Flood Risk
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/3	Water Pollution
EN7/5	Waste Water Management
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - The National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Bury's Strategic Housing Land Availability Assessment sets out the latest housing supply position, which is made up with sites that have an extant planning permission and sites that have potential to obtain planning permission in the future. This shows that there are a number of sites within the Borough with the potential to deliver a significant amount of housing. However, not all of these sites will contribute to the five year supply calculations as many sites will take longer than five years to come forward and be fully developed (e.g. some large sites could take up to ten years to be completed). As such, latest monitoring indicates that the Council is unable to demonstrate a five year supply of deliverable housing land and this needs to be treated as a material factor when determining applications for residential developments.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area and there is residential development to the east, west and south. As such, the proposed development would not conflict with the surrounding land uses. The site was previously used as a car park with the former adjacent pub and as such, would be previously developed land. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

Design and layout - The proposed dwellings would be 2 storeys in height and would be located in a pair of semi-detached dwellings and a detached dwelling. The proposed dwellings have been lowered in height and would be the same height as the existing

dwelling to the south of the site. It is therefore considered that the height and scale of the dwellings would maintain the rhythm and symmetry in the street scene.

The proposed dwellings would be constructed from brick and grey concrete roof tiles. Headers, cills and front canopies would add visual interest to the elevations and not dissimilar in design to the adjacent houses, Nos 2 - 6 Green Street. There are a variety of materials in the locality comprising brick, render and painted blockwork and as such, it is considered that the proposed materials would be acceptable and characteristic of the surrounding area. The proposed dwellings would also be set back from the pavement on Green Street by 7.88 metres and as such, they would not be dominant and overbearing to the street scene.

Therefore, the scale and design of the proposed dwellings would be appropriate within the context of the area and would comply with UDP Policies H2/1 - The Form of New Residential Development and EN1/2 - Townscape and Built Design.

Impact upon residential amenity - SPD6 provides guidance on aspect standards between residential properties and would be relevant in this case.

The property's opposite which would face the development would be Nos 1 and 3 Green Street. No 1 is a vacant property, and has a historical use as a workshop with part of the building a shop front type appearance, also vacant. Above, there are windows to what are either associated storage/offices or residential flats. It is unclear if these are used/habited. No 3 Green Street is a terraced property and has a window at ground and first floor to habitable rooms.

The proposed site plan demonstrates there would be 16.5 metres between the proposed dwellings and Nos 1 and 3 Green Street. It is noted that the windows on the front elevation would relate to a lounge and bedroom. These rooms are classed as habitable and requires a justification for a reduced aspect standard.

There are already sub-standard distances on Green Street, with a separation of 14.2 metres between Nos 2-6 Green Street and the terraced houses opposite. Nos 2 - 6 have a conventional layout like the proposed dwellings, with habitable room windows at ground and first floor on the frontage. As the proposed dwellings would be set back a further 2.3 metres, equating to a separation of 16.5 metres, the relationship to the properties opposite would be less harmful to privacy than already exists on the street.

The proposed development would be set back from Green Street by 7.88 metres and the front gardens, driveways and intervening street would give some perception of separation and space between the site and the properties opposite.

As there would only be one window in each of the new properties to a habitable room, and given there would be 16.5 metre separation and that there are already sub-standard distances between the other dwellings on the street, it is considered that overlooking and privacy would not be significant to adversely affect the residential amenity of the properties opposite. The proposed development would reflect the existing street pattern. On balance and in view of these factors, it is considered that the proposed development would be acceptable.

Beyond the rear boundary of the site are Nos 1 and 3 Lomond Drive which are set at an oblique angle to the site. There would be a separation distance of over 20 metres between the rear elevations of each property, which would exceed the aspect standards. There would be a minimum of 7.7 metres between the rear elevation of the proposed dwellings and the boundary with the properties on Lomond Drive, which would exceed the 7 metre aspect standard.

The property to the east was formally a public house and has planning permission for its conversion to 2 no. residential units and part ground floor office use. The approved plan

demonstrates that the rear window which would face the site would be to a bedroom. SPD6 requires a distance of 6.5m between a 1st floor habitable room window and a single storey blank wall, which in this case would be the side of the proposed dwelling. There would be a distance of 6.7 metres and 7.2 metres between the two properties, which would exceed the aspect standards.

As such, it is considered that there would not be a significant detrimental impact on residential amenity and the development would be in compliance with UDP Policies.

Highways - Each property would have a driveway off Green Street and 2 parking spaces would be provided. The proposed dwellings have been located in the middle of the site, which allows for an extended driveway of 6.8 metres, which would ensure that there is sufficient space to manoeuvre a car on and off the drive, with vehicles parked on the opposite side of Green Street. Whilst Green Street is a fairly narrow cul de sac, it is considered that 3 additional dwellings would not significantly increase traffic on the street and would not adversely impact on highway safety.

Access to the outbuilding located to the rear of the site would be maintained and would be unaffected by the development proposals.

The Traffic Section has no objections in principle and further comments will be reported in the Supplementary Report.

Parking - SPD11 states that the maximum number of parking spaces is 2 spaces per 3 bed dwelling. This equates to a maximum of 6 spaces.

The proposed site plan indicates that 2 parking spaces would be provided within the driveway area, which totals 6 spaces. This complies with the parking standards. A number of objections relate to the problems of parking already experienced on Green Street, especially as the adjacent pub on Tottington Road has been converted and that the development would add to parking issues on the street. Objectors also raised the issue that the development should provide parking for the residents and public in the area.

It is not in the gift of the residents on Green Street to use this land for parking nor for the development to provide parking for residents on Green Street. The residents of the terraced houses on the street have always had to park on street and this would not change. The fact that some may use the land at the moment is not a material planning consideration as it is not in their ownership to do so.

In terms of the pub conversion, this application was assessed on its own merits and it was concluded that parking for the residential units would not be necessary due to the sustainable location of the property, 'buyer beware' and that the then closed pub with accommodation could re-open at any time without the need for planning permission and without the car park. A space was provided for the office use.

It is therefore considered that given the development would provide in-curtilage parking for each property, its close proximity to Tottington Road which has good access to public transport and is in a sustainable location, the level of parking provision would be acceptable. Therefore, the proposed development would be in accordance with Policies H2/2 and HT2/4 of the Bury Unitary Development Plan and SPD11.

Response to objectors

- The issues relating to parking and access to the building at the rear have been addressed in the main report.
- Condition 8 relates to a landscaping scheme, which requires replacement trees.
- The issues relating to noise and disturbance during construction are not material planning considerations.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered RAD/2115/20/2/RevA, RAD/2115/20/1/RevA and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
5. Details/Samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policies EN1/2- Townscape and Built Design and H2/1 - The Form of New Residential Development.

6. Surface water shall be drained in accordance with the hierarchy of drainage options in national planning practice guidance. In the event of surface water discharging to public sewer, the rate of discharge shall be restricted to the lowest possible rate which shall be agreed with the statutory undertaker prior to connection to the public sewer.

Reason. To promote sustainable development and reduce flood risk and pollution pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

7. Foul and surface water shall be drained on separate systems.

Reason. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

8. A landscaping scheme shall be submitted to, and approved by the Local Planning Authority prior to the commencement of the development. The contents of the plan should include native tree and shrub planting and the provision of bat bricks/tubes within the new development and bat and bird boxes. The approved scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree planting season;; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

9. The car parking indicated on approved plan reference RAD/2115/20/2/RevA shall be surfaced in a material, or incorporate measures, that prevents the discharge of surface water onto the adopted highway and be made available for use prior to the development hereby approved being occupied and thereafter maintained at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

10. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved by the Local Planning Authority and shall confirm/provide the following:

- Access route for construction traffic from the highway network and restricted to a size of vehicle that can be accommodated on Green Street that serves the site;
- Hours of operation and number of vehicle movements;
- Parking on site of operatives' and construction vehicles together with storage on site of construction materials, including any requisite phasing of the

development to accommodate this.

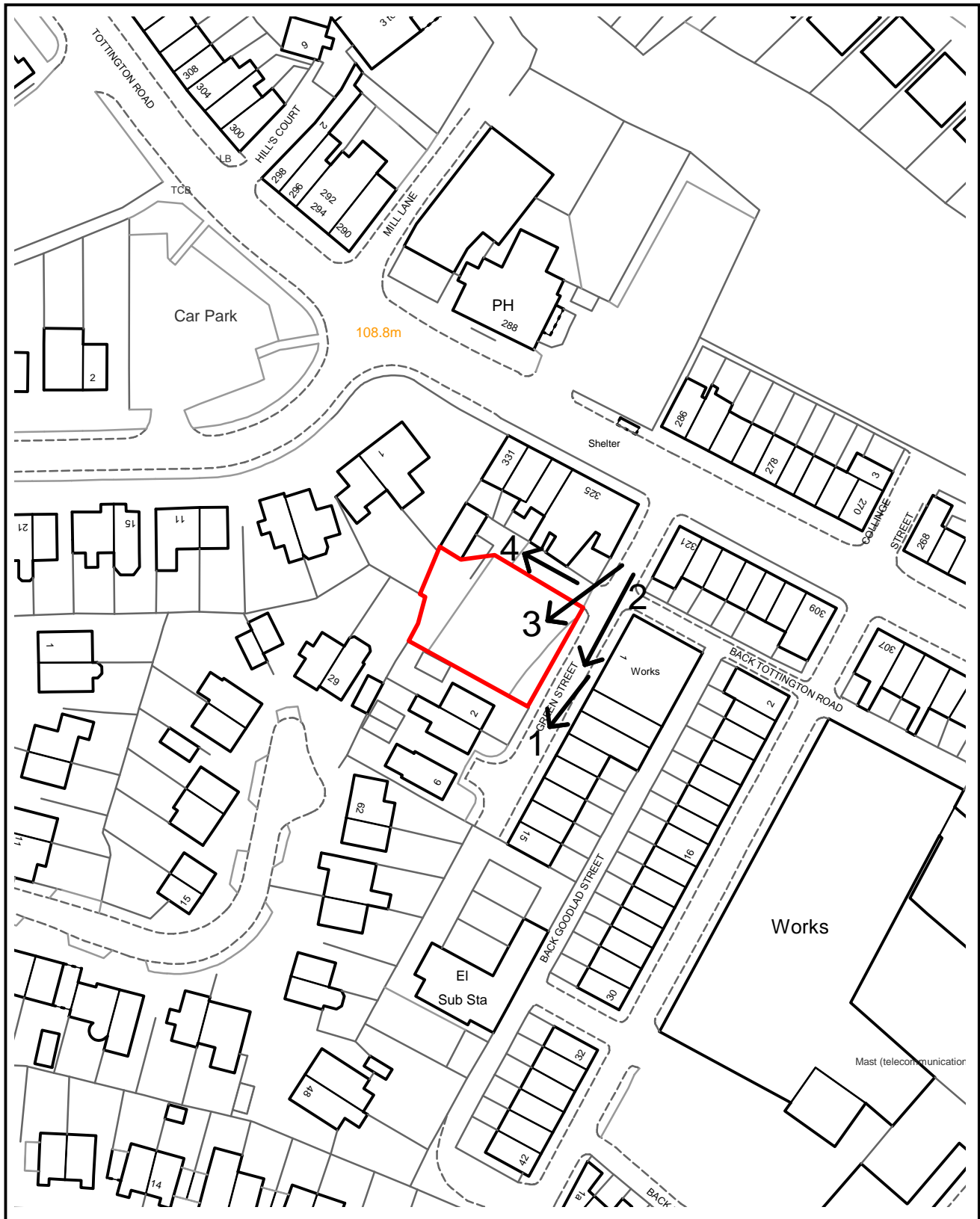
- Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

For further information on the application please contact **Helen Leach** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65293

**ADDRESS: Land at North Street
Bury**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

Photo 1



Photo 2

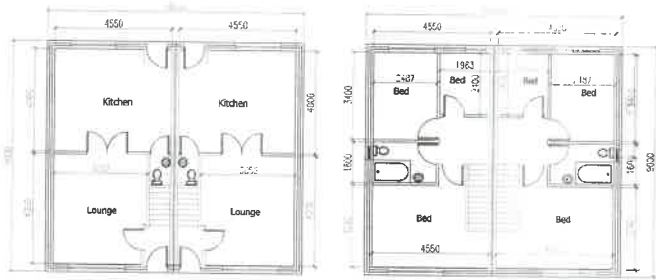


Photo 3



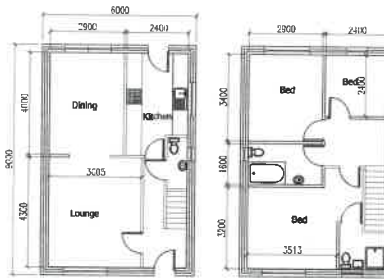
Photo 4





PROPOSED GROUND FLOOR

PROPOSED FIRST FLOOR



PROPOSED GROUND FLOOR

PROPOSED FIRST FLOOR



BLOCK PLAN (1:500)



LOCATION PLAN (1:1250)
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Ordnance Survey 0100031673



PROPOSED FRONT ELEVATION



PROPOSED REAR ELEVATION



PROPOSED GABLE ELEVATION (SEMI)



PROPOSED GABLE ELEVATION (SEMI)



PROPOSED GABLE ELEVATION (DETACHED)



PROPOSED GABLE ELEVATION (DETACHED)



PROPOSED FRONT STREETSCENE



TOPOGRAPHICAL PLAN (1:200)

Date	Rev	Description	Drawn
10/08	A	amended dimensions and top notes	RA
22/09/20	B	heights reduced / formula not back menu	RA

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The whole of the works to comply with Planning Approval & Conditions and current Building Regulations. No work to commence without Planning & Building Regulations approval.
The client must ensure the project complies with the Construction Design and Management Regulations 2015
The Client should ensure consent from any landowner or interested party is obtained, as well as compliance with Party Wall Act 1996

Notes

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LAND AT GREEN ST
TOTTINGHAM
BURY
BL8 1TF

EXISTING & PROPOSED PLANS

RA
FEBRUARY 2020

1:100 @ A1 / 1:200 @ A3
RAD/2115/20/1/RevA

GRASSED AREA

Figure 9

TARMAC

PAVING

[B][B][B]

BINS

[illegible]

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LAND AT GREEN ST
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EXISTING & PROPOSED PLANS

RA

FEBRUARY 2020

1:100 @ A1 / 1:200 @ A3

RAD/2115/20/2/RevA

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Ward: Radcliffe - East

Item 05

Applicant: Bourne Group Limited

Location: Radcliffe Metrolink Car Park, Spring Lane, Radcliffe, Manchester, M26 2ST

Proposal: The erection of an additional deck and ramp to form a second floor to car park, providing an additional 115 no. spaces; landscaping scheme and lighting

Application Ref: 65354/Full

Target Date: 20/08/2020

Recommendation: Approve with Conditions

Description

The application relates to the car park which serves Radcliffe Metrolink Station. The site is approximately 0.91 hectares in area located to the east of Radcliffe Town centre. Opposite to the north is Spring Street School (former Coney Green School site). The Metrolink line runs the length of the eastern boundary on top of an embankment and to the west is the boundary with 2 storey residential dwellings on Howarth Street and Barlow Street and industrial units which are set at a lower level to the site, separated by a concrete post and wire fence and some intermittent tree planting. A pedestrian route from the southern part of the car park leads to Church Street West. The site is accessed off Spring Lane which runs along the northern frontage of the site.

The existing Metrolink car park comprises of two levels of parking - the main car park providing 222 No Standard bays and 22 No. disabled bays (244 No in total) and circulation routes and a first floor deck for 120 No. spaces, equating to 364 total provision. The first floor tier is accessed internally from the main car park via a ramp and there are stairways to the north and south of the deck providing pedestrian accesses routes.

The access into the site is from the western side of Spring Lane via a single lane which circumnavigates the car park and leads to the access out of the site at the eastern side of Spring Lane, where 2 lanes provide a left and right hand turn out onto the highway. There is a pedestrian access into the site from Spring Street with a continuous footway around the perimeter of the site to the station entrance which is via an underpass at the southern end of the car park.

There are 5 cycle store lockers and 5 Sheffield cycle stands located close to the station platform.

The application proposes to erect a 2nd floor parking deck to provide additional parking for the station and users of the Metrolink. With the re-configuration of the existing car park, there would be a net gain of an additional 115 No. spaces. The proposed deck would be located on top of the existing 1st floor deck, essentially mirroring the 1st floor arrangement which would provide a two-way ramp and pedestrian stairwells.

Minor re-configurations of the existing layout would be required to facilitate the new access ramps and to provide a safe pedestrian route round the site.

The total number of spaces provided on completion would be 479 spaces.

The new deck would comprise a lightweight steel frame with low horizontal safety barriers similar to those at the first floor level and finished in a dark grey asphalt or tarmac surface. There would be a ramp and two sets of pedestrian stairways built to facilitate access to the

new deck, the same as the existing arrangement.

The application also proposes to incorporate lighting and emergency lighting within the new deck, together additional CCTV security cameras.

The scheme includes landscaping which would be planted along the western boundary within the existing foliage and to the east next to the embankment.

The development would be built out in a phased approach. The applicant has submitted a phasing plan, with the works falling into 3 phases - Enabling works, (sectional closures of the existing car park), main works (full closure of the existing car park), finishing works (full closure) followed by completion.

The Radcliffe Metrolink station for travel would remain open throughout the construction period.

For information, a planning application has also been submitted by TfGM to erect a 2nd floor parking deck at the existing Metrolink car park in Whitefield - planning reference 65465. It is anticipated this will be delivered in conjunction with the expansion at Radcliffe.

The Applicant states that replacement parking to compensate for the loss of parking whilst the works are underway is being investigated. It is anticipated that an application for alternative temporary parking arrangements is to be submitted in due course. It is likely that a joint replacement scheme is to be submitted for both the Whitefield and Radcliffe sites whilst works are progressed. The provision of the replacement temporary parking would be secured by condition and to be provided prior to the commencement of works at the Metrolink.

TfGM states that the proposed development at Radcliffe and Whitefield are the first in a number of expansions being explored for other Park and Ride Schemes (P&R Schemes) in the Greater Manchester Area for the future.

Relevant Planning History

56462 - Erection of deck to form first floor to car park to provide an additional 109 spaces - Approved 21/8/2013

To clarify numbers of existing spaces, whilst there are currently 120 No. spaces at first floor, there was a net gain of 109 in total as the development for the 1st floor deck resulted in the loss of the spaces at the 'ground floor level' by the provision the ramp and re-configuration of circulation space to facilitate the additional 1st floor deck.

Publicity

Letters sent on 26/5/20 to 93 properties. A full list of those notified are available on the public file.

Site notice posted 28/5/20.

Press advert in the Bury Times on 28/5/20.

A total of 58 representations received.

48 objections and 1 petition with 13 signatures.

5 in support

4 comments

A full list of those who have made representations can be found on the public file.

Objections -

- Another level will just attract more out of town commuters. Perhaps if commuters had to pay they wouldn't be so keen to stop off at Radcliffe on their way to work.
- There are already too many problems being caused to the surrounding residents, not least the car park for the health centre. It's absolutely ridiculous that people with appointments struggle to get a space. Start charging & pay inspectors to make sure that

people parking around the Metrolink are actually using it!!!

- Object to the lack of extra cycling provision, lack of secure cycle storage, and lack of protected cycling infrastructure through the car park.
- The station is close to National Cycle Route 6. It is ludicrous that cycling has not been given more importance. This is a car-centric plan that should be denied
- Strongly dispute that there are 0 properties affected.
- The car park is located in a densely populated area of Radcliffe and will have a long term effect on the people of Radcliffe's health and well-being of people living in the surrounding condensed housing
- Increase in traffic to the locality.
- Radcliffe already have to deal with the overflow of car parking from Bury and surrounding areas, so whilst other towns are benefiting from a less polluted air to breathe, I feel Radcliffe has taken on enough to support the share of pollution and the Council should be looking elsewhere.
- Why don't you use this money to build a car park at Bury metro link or other stations?
- The development does not serve the people of Radcliffe and is essentially a park and ride for the City.
- There has been no consultation with local elected members and the application should form part of the wider SRF for Radcliffe
- The development of sites such as ELPM and Spring Lane school need to be considered in this application.
- Increase in air pollution.
- Transport Statement - Despite the Crime Impact Statement (CIS) recognising that the current site of the Radcliffe Riverside/Radcliffe Temporary Leisure Centre potentially being the site of a new high school, the transport statement does not take this into account in their current observations. The following points have been omitted from the transport statements road safety plan and road safety figure 2.1 - Frequency of school buses, volume of drop off's from parents, increased number of school pupils using the main road. These should be taken into consideration with regards to hundreds more cars accessing the Metrolink site on a daily basis.
- The Transport Statement doesn't take into consideration the volume of traffic and people using this section of road when Radcliffe last had a high school on the site prior to 2014 therefore the current assessment is surely flawed.
- The CIS does reflect the broader concern of how busy the area will be when a high school is built.
- There is a poster at the station advertising clean air- ironic if this project is passed.
- There will be another 400 new houses built in the area - cause more traffic and gridlock.
- See what clean air do we have?
- There is an eyesore of a car park straight across from the Bury town Hall - what a poor looking place - parking for the tram should have been made when Six Ton Housing and the Premier Inn built.
- The application states Radcliffe is chosen due to the proximity of the A56 and M60 and is a prominent transport hub for travel to Manchester and Bury - in fact, Radcliffe car park is the furthest away. Other metro stops are closer to the A56 and M60. What study was used to come to this conclusion?
- There would be close to 60% of all Metrolink parking at Radcliffe - how can TfGM justify this?
- The promises to protect residents parking and stop parking outside houses have not yet been implemented and there will be a further surplus of cars outside local residents' houses
- Noticeable that during lockdown air pollution has been much reduced.
- The height of the extended car park will be close to the height of the nearby houses increasing level of air and noise pollution at bedroom level.
- The existing structure overlooks both floors of my house and an additional level will be a further invasion of my privacy.
- Ongoing anti social behaviour and car crime in the residential area.
- I would support this application to ease parking issues if there was already the parking permit scheme arranged in the local streets before this work was to commence.

Representative of Radcliffe First Party - List of questions and points seeking clarification:

- Radcliffe already has a large car park and more Metrolink spaces than the other stations - Considering that many residents in Radcliffe do not use the Metrolink parking how can TfGM justify this? Has TfGM carried out a customer survey to identify the original journey start points for commuters using Radcliffe Metrolink station? If so, can you provide the data
- Has Bury Council insisted that the applicant Transport for Greater Manchester perform a series of measurements of traffic emission pollution levels on roads connecting to the existing Metrolink parking?
- How can it be justified that such a big car park design to serve a large number of commuters will be located in what Transport for Greater Manchester confirms is a residential area?
- What evidence has the applicant provided that other alternative sites were considered? Locating the largest car park on the Metrolink system in a residential area is not something that I would have thought was an ideal solution.
- A further survey is required to establish when traffic travelling to the car park would arrive - it is likely that there would be congestion on Spring Lane with negative impacts on residents.
- Residential housing borders the site and is at a lower level than the existing car park. Impossible from the plan to evaluate the likely impact on residents due to loss of light by being overshadowed by the structure. Require a cross sectional drawing.

Further representative of Radcliffe First Party - still waiting for further information but given the evidence currently provided, object on the following grounds -

- Would provide close to 60% of Metrolink parking
- Has TfGM collected any data on journey start points?
- Has TfGM measured traffic pollution levels on Spring Lane? Evidence from Water Street in Radcliffe (which is the nearest fixed monitoring station) show that the monitored levels regularly exceed permitted legal levels
- Radcliffe Metrolink is actually the furthest away from the A56 and a convenient motorway junction of all the Metrolink stations in Bury MBC, why therefore does TfGM claim that it provides a convenient location for access to these roads. Can TfGM provide evidence that other sites have been considered (especially Bury) and explain the reasons why they are not considered to be suitable locations?
- The Transport Statement created on behalf of the Milson Group Ltd is incomplete
- No evidence was contained in the documentation provided that the impact of potential overshadowing and resultant loss of light for residents of Barlow Street and Howarth Street had been considered.

The following should be considered:

- Reference to the GMSF and Radcliffe Strategic Regeneration Framework (SRF).
- Air pollution

Objection Cllr Mike Smith

- Write to you as a resident, Metrolink user and Local Councillor to voice my objection to the proposal. I would also like more information on the following points in order to be able to make an informed assessment regarding this plan.
- The application should be in abeyance until it can be measured against the strategic plan for the town.
- Does not take into account the outline planning for 400 houses and Academy school for 600 pupils.
- Adverse effects of the application would significantly outweigh the benefits with regards to traffic impacts and deterioration of the outdated infrastructure in the area
- Pollution and poor air quality, capacity of park and ride facilities, traffic congestion at peak periods - all issues.
- Radcliffe furthest away from A56 and M60 than Bury, Whitefield or Prestwich - was any other site considered?
- The parking accumulation survey was undertaken on Friday and Saturday 22nd and 23rd June 2018. These figures are two years out of date and do not take into account the current conditions and usage of the site. Inaccurate figures at a weekend.

- Existing Spaces/Accumulated Vehicles applied to the extra 120 spaces.
- Nowhere in this document is a rationale as to where these figures are applied to the 120 increased spaces. Would like some clarity on the math behind this figure please.
- At the very least I would expect to see a trip generation survey for the expected increase as a result of this development
- Each report in these documents is prepared in support of the application. Is it Bury Council planning department policy to rely on supporting documents only? Is there no facility for an independent report to be prepared to give an impartial view on the effect such a development will have on the residents of this area which is already an AQMA?
- In terms of the consultation process, the impression given is that I was somehow consulted on this plan, a brief communication by TfGM does not constitute stakeholder engagement.

Comments -

- What evidence has been provided that other alternative sites have been considered - Radcliffe is in fact located furthest away from the two commuter routes - A56 and M60. Locating the Metrolink near residential properties not an ideal solution.
- Added pollution from outside traffic.
- Without developing the roads this will cause more problems than solving issues.
- How temporary will the replacement parking be?

Support -

- Great idea, less cars on the street.
- Need this as I struggle to park to use the tram and am forced to park on the surrounding streets
- Working full time extra spaces would give me a chance to drop children off at childcare park and be in work within 50 min without cutting working hours down. Same for home time and without the need to look for spaces that will not affect home owners. Support this - without children I used to walk this is not a option now.
- We need more parking at Radcliffe, its full at 7:30 in the morning and people object to parking in the side streets, this is badly needed.

The Applicant has provided a response to the representations made above, summarised as follows -

- Air quality, pollution, and residents' well-being - The Transport Statement that has been submitted as part of the Planning Application for this scheme has concluded that the introduction of the proposed additional car park deck will not have a material impact on the operation of the highway network in this location. Results from the accumulation survey undertaken on Friday 22nd June 2018 and Saturday 23rd June 2018 were used to assess the existing usage of the station car park between 7.00am-19:00pm on both days. Air Quality Assessment - a detailed survey was conducted in February 2020, which was desktop based and concludes that "Based on the assessment results, air quality is not considered a constraint to planning consent for the proposed development." A separate cycle scheme is being developed for Radcliffe Park & Ride (P&R), where cycle parking will be upgraded in line with TfGM's wider cycle parking strategy. It is also anticipated that the lower car park usage currently being experienced, will enable construction to proceed with less disruption to users and local residents. In addition, we are currently assessing the potential requirement to construct a temporary car park to offset the loss of parking
- Noise, traffic and congestion - The Noise Impact Assessment at Radcliffe took place on Thursday 13th March 2020 between 16:05 and 20:05 and Friday 14th March 2020 between 06:10 and 10:10. The timing of the survey was determined to align to both the morning and evening peak. The survey concluded there would be an insignificant increase to the existing noise levels. The LPA's Pollution Control Officer has raised no concerns regarding noise disturbances.
- Why is it proposed to expand Radcliffe P&R rather than other P&R at Metrolink stops? - The selection of Radcliffe P&R facility as a site to expand, has been reviewed with several factors in mind, including the wider Greater Manchester (GM) strategies and future development over the coming years. The Business Case completed for the

Metrolink Capacity Improvement Programme (MCIP), which includes Radcliffe P&R, considers its strategic alignment with Transport for the North's vision to transform the region's economy, which is set out in 'The Northern Powerhouse: One Agenda, One Economy, One North' (2015) which recognises the importance of local connectivity. The Business case also considered the Greater Manchester Spatial Framework (GMSF). For Radcliffe this means sustainable travel, therefore reducing congestion, better access to employment areas, support regeneration of town centres, travel for future new housing. The proposed scheme at Radcliffe P&R is to be delivered in conjunction with the expansion at Whitefield P&R and not in isolation, to offer a combined increase in spaces for the area.

- What about cycle facilities, will these be improved? - A separate cycle scheme is being developed for Radcliffe Park & Ride and to upgrade where cycle parking provision in line with TfGM's wider cycle parking strategy. Plans for the car park extension will facilitate the delivery of the future cycling enhancements. Due to recent coronavirus, there is an increased use of sustainable travel modes such as cycling, and we are looking to continue the promotion of sustainable travel when patronage returns to normal levels. There are separate aspirations to connect the cycle parking to an established cycle route between the 'Banana Path' off Spring Lane, and Church Street West, by installing a cycle path along the west boundary of the Radcliffe Park & Ride site.
- There are already parking issues for residents on nearby streets, won't this scheme make it worse? - A residential parking scheme, is being promoted by Bury Council, was due to commence in May 2020 but due to Covid-19 restrictions this has meant it has been delayed but will still be delivered within similar timescales to the proposed Park & Ride expansion. The lining and signing works for this scheme have been ordered and once those have been introduced on site, an operative date for the scheme will be set. The expansions of the Radcliffe and Whitefield Park & Ride facilities look to absorb those Metrolink users currently parking on the surrounding streets and combined with the Residents Parking Scheme this will significantly reduce the problem. Also investigating the opportunity to provide a temporary car parking facility with Bury Council in order to reduce disruption during the works. TfGM is currently looking into how it manages its Park & Ride facilities across Greater Manchester, and this includes ways to ensure that users of the Park & Ride sites are Metrolink travellers only.
- Customers who are using the Radcliffe P&R facility are not Radcliffe locals and are travelling from further afield - In 2017, a survey was conducted of customers travelling to the Radcliffe Metrolink stop to inform the feasibility of this scheme - 21% of customers had driven less than 1 mile to the Park & Ride facility, 57% travelled 1 - 3 miles, 19% travelled 3 - 5 miles, and 3% travelled over 5 miles. The means that 78% (the majority) of users were driving less than 3 miles to use the Park & Ride facility. Bury Council also conducted a series of parking 'beat' surveys around the immediate streets to Radcliffe P&R. Capacity needs to be increased to accommodate population growth and also not cause detriment to the environment. Improvements also need to consider north and south bound travel and counteract overspill parking to nearby streets.
- What impact will this development have on residents in neighbouring streets due to loss of light by the structure? - Design features including light spill protection measures are included in the scheme. Cross-section drawing has been submitted to show the relationship of the structure to the nearest houses. Screening from vegetation has been considered.
- Have other future developments - new high school and ELPM site - been taken into consideration when developing this scheme at Radcliffe? - With the GM strategies in mind, the proposed Radcliffe Park & Ride expansion will serve as a first step in the wider development of Radcliffe - there are several transport interventions in Radcliffe, which are referenced in the Radcliffe Strategic Regeneration Framework (SRF). The expansion of the Radcliffe Park & Ride facility is the first scheme to be delivered in this wider plan. As a key stakeholder, TfGM are working with Bury Council to ensure that transport improvements form an integral part of the SRF by referring to a wider package of potential interventions such as cycling, walking and Metrolink stop improvements
- Security of the site - The current P&R has been awarded the park mark accreditation,

indicating a low rate of crime and the adoption of appropriate security measures. . The proposed design will be constructed with similar security features.

The full response by the Applicant is available to view on the public file.

Those who had made representations to the application have been notified of the Applicant's above response on 3/7/20.

Further objections following the applicant's response to representations received.

- TFGM response is disappointing and dismissive of objections and based very much on the business case alone. Little or no regard for what is good for the town of Radcliffe.

Air quality/pollution -

- Disagree and how can a 2 year old report be valid and relevant?
- No thought to new high school and how this will impact on traffic and pollution in the area.
- Better decision to develop at Bury.
- Their reference to new future cycle ways - all very nice words but with absolutely no substance and no plan.
- TfGM appear to have more concern about pollution and disruption during the construction of the proposed extra level than they do have about afterwards. TfGM seem to be convinced that a benefit of this proposal is that it can be done during this national pandemic. This argument is irrelevant.
- Disappointing that the EHO looks first at the construction phase and not the operational phase.
- Questionable the air quality assessment was desk top based and the analysis was only over 2 days and include a Saturday. Due to lack of physical data, it can be concluded the claims are unsafe.

Noise traffic and congestion -

- How can a 2 day survey inform the correct decision, one being on a Saturday and in June, a quieter time in the school calendar?
- What is known and acknowledged in reports by the applicant is that Spring Lane and other roads joining spring lane do experience high levels of congestion during the morning and evening rush hour. To claim any validity for a survey a number of sets of data should be collected over several fixed time periods spaced out over a number of weeks/months for morning and evening rush hour periods.
- How can 115 + cars not increase noise and pollution?
- No clear data is presented

Why expansion at Radcliffe? -

- The applicant response is a HOW rather than a WHY.
- The only reason Radcliffe has a shortage is because commuters are having to travel further than their local Metrolink to park at Radcliffe.
- The LPA must not be rushed into making any decisions - we are still in a pandemic situation.
- The proposal of a P&R does not necessarily have to be in Radcliffe, because the structure is already there and TfGM can just "Add on to it", that may be the cheapest and easiest option, but it will not stop people parking on the streets.
- There are still consultations on the Radcliffe SRF and GMSF with no conclusion at present. Waiting for a final decision would be advisable.
- The response from TfGM very much focuses on the business cases for the region and on Greater Manchester strategies and quotes documents dating back to 2015. It makes little comment about the community of Radcliffe and the benefit for Radcliffe.
- No benefit economically for Radcliffe due to the car park's out of town location.
- Increasing the catchment area must actually increase the volume of traffic coming in to the small town of Radcliffe
- It is stated that the GMSF requires principal town centres such as BURY to have improved access from a wider catchment' - So, in your own words this expansion of a car park MUST be done at Bury
- Don't understand the reference to a new residents housing area of 'Elton Parkland'. I

don't know where that is.

Parking issues for Radcliffe residents -

- Don't understand how the Parking Permits are linked to TfGM, they are provided by the council. The permits are not specifically issued to stop Metrolink commuters. They are to stop ANY parking.
- The current policy of not charging for parking is a flawed policy
- Cheaper for somebody who lives further away to drive to Radcliffe, park for free and then pay a small amount to travel by tram to Bury to work. How can that be a good justifiable policy?
- Not all streets are covered by the parking scheme - it will displace the problems elsewhere.

Customers who are using the Radcliffe P&R are not Radcliffe locals -

- The response refers to a survey carried out in 2017, again another dated document. The document then goes on to include data from a survey done at the Whitefield P&R, another example of the scant regard that TfGM are giving to the people of Radcliffe.
- A huge amount of any parking at the Radcliffe station should be devoted to Radcliffe residents
- Distinct lack of clear methodology for this data

Have other future developments been taken into consideration? -

- Fail to see how improving a car park for Commuters will improve the development of Radcliffe as a town.
- The document contains more repetition of nice words that TfGM have consulted and/or they are working with BMBC on the SRF and the new high school and on their aspirations of cycling routes, finding finance etc.
- Would like to see TfGM do a survey of those arriving and alighting the tram at the Metrolink station
- The disclosure by TfGM that they are already procuring new trams should also be included in the modelling, the likely increase in passenger numbers will add more traffic due to people being dropped off, taxis to and from etc
- Inadequate landscaping.
- Inadequate CCTV
- Increase in crime - out of date
- Covid Sars-19 has greatly impacted on the level of transport required and is likely to require in the near future. Therefore, planning permission would be premature.
- Considerable new housing is a contentious issue locally in any event.
- A temporary car park should have been sourced during this consultation. There are no guarantees that one will be in place.
- In summary, full thorough surveys need to be completed before a decision is made on the location of any P&R facilities.
- The opportunity to invest in what the customer would actually want, has been missed.
- There should be no changes now until such time that consultation on the Radcliffe SRF is concluded and the final developments are agreed

Support - Great idea

Response to further representations received.

The Applicant's response to the validity of the date of the traffic survey carried out in 2018 - The traffic counts used in the Transport Statement were of cars entering and leaving the car parks and not on the public highway network.

Any other matters not already addressed in the report will be responded to in the Supplementary Agenda.

Statutory/Non-Statutory Consultations

Traffic Section - No objection in principle. Conditions to be finalised in the Supplementary Agenda.

Borough Engineer - Drainage Section - No response received.

United Utilities (Water and waste) - No objection subject to a condition for the submission of a drainage scheme.

Environmental Health - Contaminated Land - No objection subject to conditions
Environmental Health - Pollution Control - No comments to make
Greater Manchester Police - designforsecurity - Any response to be reported in the Supplementary Agenda.
EDS Head of Parks and Countryside - No response received.
Greater Manchester Ecology Unit - No objection subject to conditions and informatives.

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN6/4	Wildlife Links and Corridors
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
HT2/4	Car Parking and New Development
HT2/5	Public Car Parks
HT2/6	Replacement Car Parking
HT3	Public Transport
HT3/4	Schemes to Assist Metrolink
HT4	New Development
HT5/1	Access For Those with Special Needs
SPD11	Parking Standards in Bury
EN1/5	Crime Prevention
EN7	Pollution Control
HT2	Highway Network
HT6/1	Pedestrian and Cyclist Movement
EN8/2	Woodland and Tree Planting

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policies - At the heart of the NPPF is a presumption in favour of sustainable development and states the three objectives to achieving sustainable development which are:

- economic objective - to help build a strong, responsive and competitive economy , including identifying and coordinating the provision of infrastructure;
- social objective - to support a strong, vibrant and healthy communities....by fostering well-designed and safe built environments with accessible services;
- environmental objectives - contribute to protecting and enhancing the natural, built and historic environment, including making effective use of land including moving to a low carbon economy. Planning policies and decisions should play an active role in guiding development towards sustainable solutions.

Section 9 of the NPPF - Promoting sustainable transport seeks to ensure that appropriate opportunities to promote sustainable transport modes can be - or have been - taken up, given the type of development and its location; safe and suitable access to the site can be achieved for all users and any significant impacts from development on the transport

network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

At paragraph 109, development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Development that would generate significant amounts of movement should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed.

UDP Policy HT1 - A Balanced Transportation Strategy seeks to ensure a co-ordinated approach to the formulation of policies and proposals for the alteration, upgrading or improvement of the transport network. This includes implementing measures to increase attractiveness of public transport as a viable alternative to private travel; facilitating better interchange between different modes of transport; and improving the accessibility of public transport to those travellers whose mobility is impaired.

At policy HT2 - Highway Network seeks, amongst other things, to improve the operation and attractiveness of the public transport network, improve the flow of traffic and improve or protect the environment.

Policy HT3/4 - Schemes to Assist Metrolink seeks to support the provision of new or improved stations and car parks which will increase the available park and ride facilities on the Metrolink.

In considering new development, Policy HT4 - New development encourages proposals which will support the principles of sustainable development, assist the implementation of a balanced transportation strategy and minimise the environmental impact of traffic.

Principle and Need - The Applicant states that the proposals to expand the Radcliffe Park and Ride (P&R) Facility has been evaluated with regards to a number of factors, including the wider Greater Manchester Spatial Framework (GMSF) and Radcliffe Strategic Regeneration Framework (SRF) and future developments, and to address the on-street parking problems experienced by local residents due to the shortfall in capacity at the existing Metrolink car park.

The Metrolink Capacity Improvement Programme (MCIP) Business Case recognised that improvements to public transport and sustainable modes of travel must be enhanced, whilst also seeking to reduce air pollution and help deliver regeneration and economic growth to areas.

The Business Case, with specific reference to Radcliffe P&R, recognised that improvements at the site would provide a sustainable and attractive alternative to driving, thereby reducing congestion and carbon emissions, enable better access to employment and local areas, support regeneration in Radcliffe and the wider Borough, improve access to the wider catchment area and provide improved sustainable transport connectivity for new developments, particularly housing growth for the area.

The Applicant has highlighted Radcliffe as one of the stops for park and ride improvements as part of the Association of Greater Manchester Authorities (AGMA) Scheme, which recognised the shortfall of car parking at Radcliffe particularly on weekdays. Bearing in mind the 'bigger' picture aspirations of the GMSF and SRF, the expansion of the P&R facility at Radcliffe Metrolink would support long-term objectives for sustainable development, economic growth and at the local level would improve connectivity, alleviate pressure on the roads and address the local on street parking problems.

It must be noted that the GMSF as a development plan would not have weight in the planning decision making process and is not material at this time until the plan nears

adoption, and the SRF would only be a material planning consideration upon adoption. Both of these documents at present are for reference but not material at this time. What is clear is that the Council must prepare a local plan and following the requirements of National Policy seek to uphold an agenda for growth which would encourage the development of sustainable development for transport, housing and employment.

In addition, the Metrolink Improvement Programme includes the expansion at Whitefield's P&R for an additional deck at the station. Delivered in conjunction with Radcliffe, there would be a combined increase in spaces in the area.

The shortfall in capacity of the Radcliffe car park has resulted in overspill parking by users of the Metrolink on the surrounding streets, causing problems for the immediate residents that live in the locality and it is intended that the opportunity to expand the P&R facility would alleviate such pressures on the nearby streets.

In terms of the justification for the expansion at the Radcliffe P&R, the Applicant has sought to demonstrate that proposals have been developed and modelled appropriately taking into account the broader growth strategies and aspirations of a growth agenda as well as considering the benefits the scheme would bring to ease the immediate parking pressures in the immediate locality from commuter parking in relation to current demand. By providing the additional deck, the proposed development would continue to contribute to sustainable development in allowing greater use of public transport instead of encouraging longer vehicle movements between Bury and Manchester and beyond.

No one solution alone of sustainable travel would be a magic bullet, but overall on a growth agenda there must be a package of different options available. This Metrolink Scheme would provide one contribution and therefore as a principle must be acceptable. It is also noted that the resident parking scheme is in the process of being implemented.

In considering whether the development should be in Radcliffe or elsewhere, it is acceptable as a matter of principle and it is the details as to whether the scheme relates appropriately to its surroundings, works in relation to traffic flows, is considered in relation to air quality for the area where it would be located and is considered in the wider scheme of approaches concerning sustainable travel options.

It is therefore considered the proposed development would comply with the principles of sustainable development and the NPPF and UDP Policies HT3/4 and HT4.

Design and layout - The proposed 2nd floor parking deck would be located directly on top of the existing 1st floor deck. In terms of the current arrangements on site, the existing car park layout would not fundamentally change as a result of the development. There would be some re-configuration of the circulation area around the main parking area and the 1st floor deck resulting in the removal of a couple of spaces to facilitate the additional ramps and access around the car park, but the re-provision of 120 new spaces at the 2nd floor would absorb this loss.

The existing access and egress from/to Spring Street would operate as currently exists.

The development proposes additional tree planting which would be located along the eastern embankment to the tram line and to the western boundary of the site, providing additional screening to the houses beyond.

In terms of design, the proposed deck would be modelled on the design of the existing deck in terms of its appearance and materials, proposing a steel frame and horizontal barriers around the periphery of the deck. The surfacing would be finished in a dark grey asphalt or tarmac, as is standard for such multi-decked parking structures and the same used for the 1st floor deck.

It is therefore considered that by adding an additional deck to the existing structure, the

requirement to address the parking capacity issues on the site would make best use of the potential land available. The car park facility would continue to operate within a safe and secure environment for Metrolink users.

The design would be appropriate and functional for the purposes it would serve and emulate the existing structure.

As such, the proposed development is considered to be acceptable and would comply with UDP Policies EN1/2, HT3/4, HT6/1 and HT4.

Impact on residential amenity - The nearest properties to the proposed development would be the terraced houses on Howarth Street and Barlow Street, to the west. There is a difference in levels between the car park and the land to the west, with the houses set approximately 2.7m lower than the site.

The existing decked car park is approximately 4.5m high to the top part of the rail which runs along the sides of the car park. The additional deck would increase the height of the structure to approximately 7.5m.

The row of houses on Howarth Street are perpendicular to the site, with only the side gable of No 45 facing the site. There would be a distance of 24m from the side gable of no 45 to the proposed deck and there is tree planting along this part of the boundary. Given the difference in levels, the distance between the houses and the site and that there would not be a direct interface between the site and the houses on Howarth Street, it is considered there would not be an overbearing relationship or a detrimental impact on this property or this row of houses.

The rear elevations of the houses on Barlow Street face the site and have habitable room windows directly opposite the existing car park and the 1st floor deck. The houses, set at a lower level already have some views of the car park with the first floor of the dwellings in line with the existing 1st floor deck. The proposed 2nd tier would be slightly higher than the roof of these houses.

There would be a minimum distance of approximately 32m from the decked car park to the rear elevations of Barlow Street. There is existing intervening landscaping with tree planting up to 12m high along part of the site boundary and proposed landscaping would fill in some of the gaps which would further screen the site. In addition, there are some industrial units which are located directly behind Nos 16-20 Barlow Street which obscure views of the car park.

The proposed 2nd floor deck would be erected in a material of a relatively lightweight appearance with the use of a steel frame and horizontal barriers, mirroring the existing form. The pedestrian access stairs to the decks are located at the opposite corners of the structure to the houses on Barlow Street and as such this would not add additional bulk or visual massing when viewed from the houses. Orientated to the west of the site and over 30m away, the properties would not experience significant or adverse periods of overshadowing from the proposed extension.

Given the distance to the houses on Barlow Street, the intervening industrial units and the existing and proposed landscaping along the western boundary, it is considered that the addition of the 2nd floor deck would not introduce an overbearing or dominant form when viewed from the houses and therefore not have a detrimental impact on the amenity of these occupiers.

It is therefore considered that the physical relationship of the 2nd deck to the nearby houses would be acceptable and the proposed development would comply with UDP Policies EN1/2.

Noise - A Noise Impact Survey has been carried out, the timings of which took account of

both the morning and evening peak usages. The report has been assessed by the Council's Pollution Control Section. The Report concluded that there was likely to be an increase of between 0 and 0.6 in the existing ambient noise levels at the nearest noise sensitive receptor. This would indicate there would be no significant effects to the existing noise in the area and the Pollution Control Section are satisfied with this conclusion.

There is already a degree of noise which is generated by the Metrolink line itself, nearby industrial buildings as well as the existing car park. Given the relatively minimal increase in noise levels that would be generated by the development, it is considered there would not be a significant harm caused to nearby residential priorities or the area in general.

Lighting - The application proposes lighting similar to the current arrangement on the 1st floor deck, which consists of 4m high column mounted luminaires located around the periphery and within the parking areas of the new deck (as shown on the 3'D' visualisation plan). The lighting columns would be of a slimline type, facing inwards towards the car park and the lamp would be a 'top hat' design which would direct light downwards, the same as the existing columns. The lights would be controlled by a combination of daylight sensors and timers and a condition would ensure that the lights would only be on at the early parts of the day, to serve the first commuters and switch off half an hour after the last tram would leave the Metrolink stop, providing both security and safety to the station and commuters whilst minimising impacts from light pollution to those living in the immediate locality.

Lighting would also be fitted under the deck on a sensor module and emergency lighting provided to driving lanes, parking bays and to clearly define pedestrian walkways to provide a safer environment.

Located over 30m away from the nearest property, it is considered that given this separation distance, the directional orientation of the lighting, and control of the timings of luminance by condition, it is considered that the proposals would not cause any more harm or have an adverse impact on residential amenity than the existing arrangements on site.

In terms of light spillage from car headlights, the development proposes low level barriers around the periphery of the deck, and as are currently in situ for the existing 1st floor deck, which would shield light from vehicle headlights using the car park. It is therefore considered there would not be significantly more harm or impact to the residents to the west than the existing situation.

Air Quality/Air Pollution - The Environmental Health Section have been consulted on the proposed development.

The site is located within an Air Quality Management Area. Due to the nature of the development for 115 additional spaces, it is considered possible that the development would create an increase of more than 100 AADT (annual average daily traffic). Therefore, an air quality assessment has been carried out which has concluded, based on the modelled predicted concentrations, the impact on air quality of the redevelopment was considered negligible and therefore, based on the IAQM and EPUK Guidance, 'not significant'.

The above assessment is acceptable to this Section.

Given the requirement to decrease NO₂ across Bury, as required by Defra in the 'UK Plan for Tackling Roadside NO₂', published in July 2017, it is important that the proposed development incorporates good design principles and best practice measures as outlined in IAQM and EPUK Guidance, to ensure emissions are minimised.

The section therefore welcome proposals to install mitigation measures such as electric vehicle charging points and cycle facilities to encourage the use of low emission and low carbon transport option.

There are currently no electric vehicle charge points at the site. However, the information

submitted with the application suggests that 6 no. electric vehicle (EV) charging points are proposed in the future. Given the requirement to reduce emissions and improve air quality in the Borough, and in light of good design and best practice measures it is reasonable to apply a condition that details and a scheme for the provision of EV charging points/bays be submitted for approval.

The current COVID-19 crisis has also seen a significant increase in participation in cycling, and the EH Section are keen to see this continue. Radcliffe Metrolink Station is located close to National Cycle Network Route 6. The existing site currently has 5 cycle storage lockers and 5 Sheffield stands located adjacent to the site access. However, the Section would welcome improved, secure cycle parking at the station, to encourage the uptake of the cycling and multi-modal travel.

The Radcliffe Metrolink P&R site is included within the cycle and pedestrian route investment scheme which is being delivered collaboratively by Bury Council & Transport for Greater Manchester (TfGM) with Department for Transport funding. As such the proposed provision of cycle upgrades and storage facilities will be undertaken under a separate application, which includes for a dedicated cycle lane at Radcliffe and improved storage facilities.

The aspiration of TfGM is to ensure that their plans for the car park extension will facilitate the delivery of future cycling enhancements. TfGM continue to promote the increase in use of sustainable travel modes such as cycling and the promotion of sustainable development.

It is therefore considered that at this time, it would not be reasonable to put in place permanent cycle storage facilities given the future plans. It would, however, seem sensible for a temporary solution to be put in place to facilitate the extra demand for cycle parking which could arise from the proposed development and this would be informed by an appropriate condition.

It is therefore considered that the proposed development would not cause adverse or significant impact on residential amenity from nuisances of noise, light pollution, or air quality pollution and as such the development would comply with EN1/2, EN7, EN7/1 and EN7/2.

Highways issues

Traffic

The applicant has conducted a survey (in 2017) of customers travelling to and using the Radcliffe Metrolink car park and it was established that 21% of customers drove for less than 1 mile, 57% between 1-3 miles, 19% travelled 3-5 miles and 3% travelled over 5 miles, resulting in 78% of users driving less than 3 miles away to use the P&R facility.

It can therefore be concluded that the majority of the existing car parking provision at the Metrolink serves local users in the area and the proposed development would continue to do so. The number of commuters from further afield are not likely to change.

A Transport Statement has been prepared by SCP which examines the consequences of the proposed expansion on the current highway and transportation systems. It has been estimated that approximately 58 additional vehicles would enter the site during the morning peak and 27 additional vehicles which would depart at the evening peak times. Other movements would likely take place out of peak periods. This would equate to one vehicle movement per minute at AM peak and one vehicle movement per 2 minutes at the PM peak. It can reasonably be concluded that the proposed additional parking provision would not be of such a scale which would significantly impact on the local highway network.

The increase in trips would likely be predominantly commuter trips which already take place on the highway and it is expected that with the expansion of the P&R to provide more parking for the Metrolink, local commuters who are having to travel to further afield to reach their destinations due to the lack of nearby parking facilities would instead be able to park

at Radcliffe station and thereby reduce the number of cars on the roads.

The Transport Statement considers that the proposed development would not have a significantly material impact on the operation of the highway network. The LPA's Highway section have raised no objection to the proposed development in principle, subject to conditions.

As such, it is considered that the proposed development would be acceptable and would comply with UDP Policies HT2, HT3/4 and HT4.

Access

There are currently two access points into the site which are located at either end of Spring Lane. The one to the west is a single lane access into the site, which circumvents the car park and leads to the ramp and 1st floor deck. The route follows the eastern boundary of the site to the two lane exit, which has a separate right and left turn out onto Spring Lane.

The existing access would remain unchanged, and it is considered that given the provision already in place, the scale of the development would not warrant any alterations to the access points.

Parking

The existing car park currently provides a total of 364 parking spaces. The existing provision comprises 244 spaces (inc 22 disabled spaces) at level 0 and 120 at level 1.

With the re-configuration which would be required to facilitate the new accesses, circulation space and ramps, the scheme would provide:

- level 0 - 240 spaces (inc 22 disabled),
- level 1 - 117 spaces
- level 2 - 120 spaces (new deck),

equating to an additional 115 and resulting in a total of 479 spaces.

SPD11 - Parking Standards in Bury does not advise on maximum standards for P&R facilities. For tram stations, it is stated that individual consideration be given to proposals where appropriate and practical.

TfGM have identified that the existing parking facilities at Radcliffe are regularly at capacity, resulting in users parking on nearby residential streets. The proposed development will aide in mitigating this problem as well as providing a more attractive and sustainable option to car travel.

The additional parking would be provided at an existing facility. It would not require any more land use than already is utilised and therefore appropriate in terms of its expansion, locality and sustainability.

It is therefore considered that the proposed development, in terms of the provision of the additional spaces would not conflict with UDP Policies HT2/4, SPD11 and the principles of the NPPF.

Replacement parking - The development would be carried out in phases, resulting in the loss of some spaces whilst enabling works are carried out and full closure of the car park following commencement of the main works. In order to maintain the availability of most of the existing parking spaces for as long a period as possible, the preparation works to the existing structure would be undertaken whilst the car park remains partly open and in use.

The Applicant is conscious that on commencement of the construction works, users of the Metrolink P&R would need to be displaced elsewhere, and they are currently investigating the provision of a temporary parking facility in order to reduce disruption to local residents from parking on the nearby streets. It is anticipated that a planning application is shortly due for submission to deal with this. Notwithstanding this, a condition would be

recommended requiring the submission of a temporary replacement parking scheme together with a timetable for implementation and re-instatement.

With a mitigation scheme in place, that would appropriately compensate for the loss of number of parking spaces throughout each phasing of the development, it is considered there would be no displacement of cars from the Metrolink users onto the nearby residential streets and as such this would be acceptable.

Security/anti-social behaviour - The proposed development would include a series of mitigation measures designed to prevent anti-social behaviour. These would include installation of lighting, solid fire resistant bins, enclosing voids between stairs, provision of additional CCTV and securing the site out of hours.

The layout of the site also provides for good open circulation and well defined routes.

The current P&R has been awarded the Park Mark accreditation, indicating a low rate of crime and with the adoption of appropriate additional security measures, it is proposed the development would continue to be in compliance with such accreditation.

The applicant states that the car park is also patrolled on an ad-hoc basis to deter 'non-users' from using the car park and TfGM also work in partnership with GMP Neighbourhood Teams to share local information and knowledge and take part in joint initiatives to tackle anti-social behaviour.

The Applicant has submitted a Crime Impact Statement and have engaged with the Design for Security team at Greater Manchester Police. It is understood that in principle, GMP have no objection to the proposed development. Any response shall be reported in the Supplementary Agenda.

It is therefore considered that there would not be any adverse security implications or rise in anti-social behaviour as a result of the proposed development and as such the development would comply with UDP Policy EN1/5.

Ecology

GMEU have been consulted on the application.

Summary - There are unlikely to be any significant ecological issues associated with this development. Risks associated with bats, nesting birds and invasive species can be resolved via informative.

Bats - The building design and the nature of the development makes the risk of bats being present and/or disturbed very low. The existing structure has no cladding the only theoretical bat roosting habitat being the existing decking which is to be retained. GMEU recommend an informative is applied to any permission to remind the applicant that under the Habitat Regulation it is an offence to disturb, harm or kill bats. If a bat is found all work should cease immediately and a suitably licensed bat worker employed to assess how best to safeguard the bat(s). Natural England should also be informed.

Nesting Birds - Existing landscaping is only semi-mature and to be retained. The bird nesting potential is low. There is also a low risk of birds nesting within the existing car park. GMEU recommend an informative that, under the Wildlife and Countryside Act 1981 as amended it is an offence to remove, damage, or destroy the nest of a wild bird, while the nest is in use or being built. Planning consent does not provide a defence against prosecution under this act. If a birds nest is suspected work should cease immediately and a suitably experienced ecologist employed to assess how best to safeguard the nest(s).

Invasive Species - Rail corridors are high risk for species such as japanese knotweed. However the development is significantly buffered from the corridor by the embankment below the line, which is managed. There is also no proposed impact on adjacent soft landscaping. GMEU are therefore satisfied that the risks of an offence are very low and

recommend an informative is applied to any permission to advise the applicant of their responsibilities under the Wildlife & Countryside Act 1981, as amended.

Contributing to and Enhancing the Natural Environment - Section 170 of the NPPF 2019 states that the planning system should contribute to and enhance the natural and local environment. The development is restricted to hard standing and the existing structure. No negative impacts are likely. Native tree planting is proposed that would result in net gain. GMEU have no objection to the landscape proposals being conditioned.

Response to objectors - Any matters not already addressed in the report will be responded to in the Supplementary Agenda.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered -
Plans
Location plan 2460-MA-01-ZZ-DR-A-PL01 rev 2
Proposed site and deck plans 2460-MA-01-ZZ-DR-A-PL03 rev 3
Proposed elevations 2460-EL03
Proposed site cross section 2460-MA-01-XX-SE-A-SE01 rev1
'3D' visualisation plan 2460-MA-01-ZZ-DR-A-VS01/VS02 rev 3 (entrance and exit)
Cycle provision plan 2460-MG-ZZ-DR-PL04
Proposed lighting and emergency lighting
MCIP003-BRN-BRL-RADPR-DR-E-LIG-0014 and 0015 levels 1 and 2
Re-built stair 1 - MICP003-BRN-BRL-RADPR-DY-Y-STR-0011 p2
Existing elevation 2460-MA-01-ZZ-DR-A-EL01 rev 2
Existing site and deck plans 2460-MA-01-ZZ-DR-A-PL02 rev 2
Tree survey by tba 4465.02
Tree protection plan by tba 4465.03
Landscape proposal by tba 4465.04

Reports

Noise Impact Assessment ref AC108130-1RO dated March 2020
Air Quality Assessment ref AQ108137 dated Feb 2020
Tree Survey report by tba Feb 2020 ref PD/4465/TSR/Feb20
Arboricultural Impact assessment and method Statement by tba Feb 2020
Transport Statement by SCP Feb 20 ref MC/200062/TS/0
Lighting Design Philosophy MCIP003-BRN-BRL-RADPR-RP-E-ZZ-009

and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

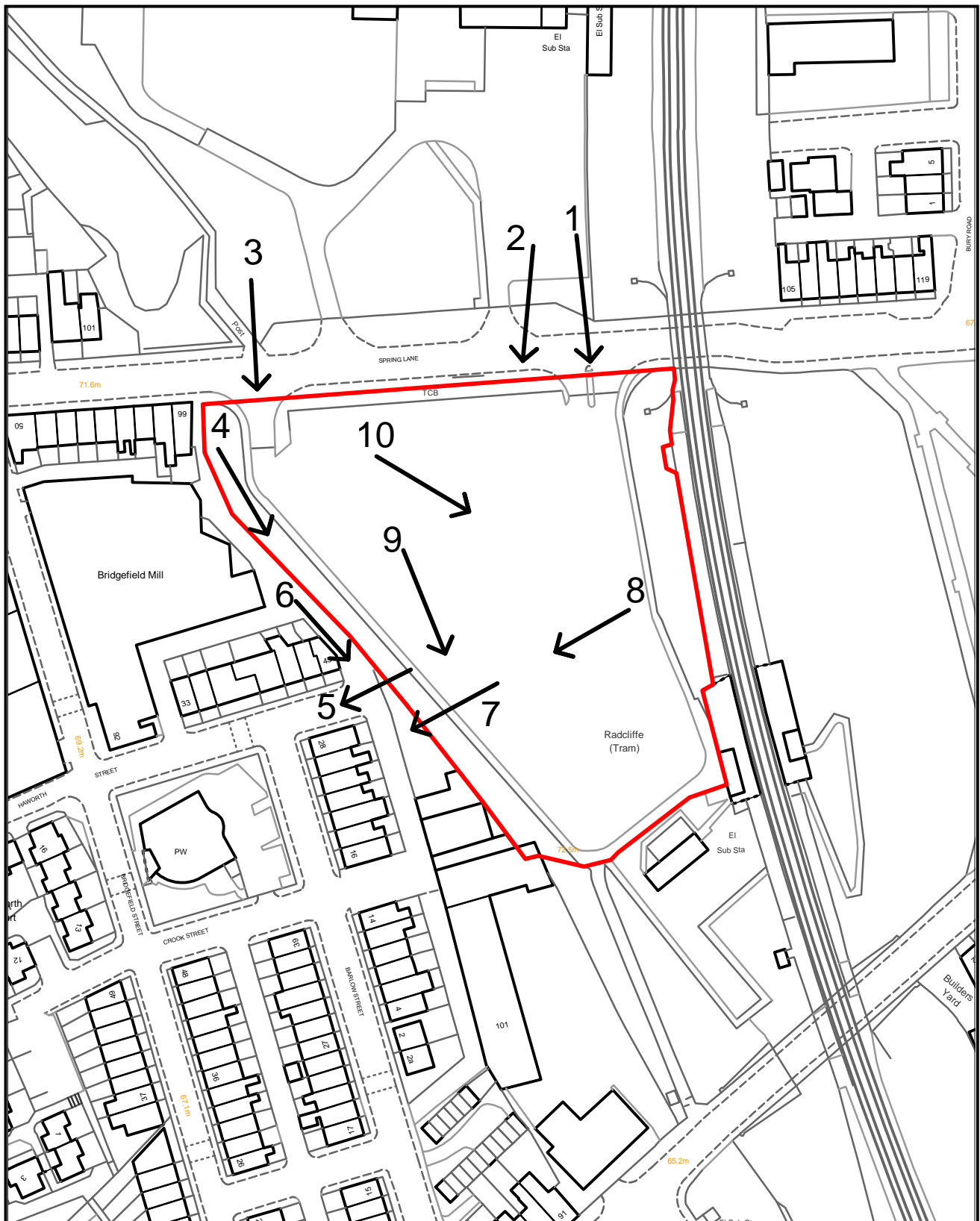
3. Prior to the commencement of any development, a drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority.
The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards. In the event of surface water draining to the public surface water sewer, the pass forward flow rate to the public sewer must be restricted to 6 l/s.
Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented prior to first use of the development hereby approved and thereafter maintained.
Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
4. Following commencement of the development hereby approved, details and a scheme for the provision of EV Charging Points/Charging Parking Bays shall be submitted to and approved by the Local Planning Authority. The approved details/scheme only shall thereafter be implemented prior to the first use of the development hereby approved and thereafter maintained.
Reason. Information not submitted at application stage and in accordance with the principles of the NPPF, to encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life.
5. Following commencement of the development hereby approved, details of a temporary cycle storage provision and scheme shall be submitted to and approved by the Local Planning Authority. The approved temporary provision shall thereafter be implemented and made available for use prior to the first use of the development hereby approved.
Reason. To secure satisfactory cycle facilities on site and in accordance with Unitary Development Policies HT6/1 - Pedestrians and Cycle Movement, HT3/4 - Scheme to Assist Metrolink and HT4 - New Development.
6. The development hereby approved shall be carried out in accordance with the submitted landscape proposals by tba landscape architects plan ref 4465.04. The approved scheme shall be implemented not later than 12 months from the first use of the development hereby approved or within the first available tree planting season, and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.
7. The development hereby approved shall provide for 3 Schwegler bat boxes to be installed on retained mature trees which shall be provided prior to the first use of the development hereby approved and thereafter maintained.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 –

Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

8. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
9. The development hereby approved shall be constructed in accordance with the design and materials used for the existing first floor deck, and as proposed on the '3D' Visualisation plans 2460-MA-01-ZZ-DR-A-VS01/VS02 Rev 3.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.
10. The lighting columns to the upper deck hereby approved shall be switched off half an hour after the last Metrolink service to the Radcliffe Metrolink station and remain off until half an hour before the first service to the Radcliffe Metrolink station daily.
Reason. To protect the amenity of the nearby residential occupiers pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and EN7 - Pollution Control.
11. No development shall commence unless and until details of a scheme for temporary alternative provision for parking has been submitted to and approved by the Local Planning Authority. The scheme shall include a timetable for implementation, details of any works required to provide the temporary car park and any re-instatement works following it ceasing operation. The approved scheme only shall thereafter be implemented and to the approved timetable.
Reason. To ensure there is adequate and accessible replacement car parking provision for users of the Metrolink whilst works are carried out and completed at the Metrolink car park, pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design, HT2/6 - Replacement Car Parking and HT3/4 - Schemes to Assist Metrolink.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65354

**ADDRESS: Radcliffe Metrolink Car Park
Spring Lane**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6



Photo 7



Photo 8



Photo 9



Photo 10



Location Plan 1250

1 : 1250

Drawing Information:
All levels and dimensions must be checked onsite by the contractor prior to commencement of works. Any variations must be reported to Millson Group Ltd. All information herein is the copyright of Millson Group Ltd. Copying in full or part is forbidden without written permission from Millson Group Ltd.



Mill
age 118

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Location Plan 500

1 : 500

Client:  Transport for Greater Manchester

Drawing: Location Plan

Drawing Number: 2460 - MA - 01 - ZZ - DR - A - PL01

Scale:	Size:	Date:
As indicated	A3	15/01/20
Drawn:	Checked:	Rev:
SD	LJ	2
Status:	Planning	

 Millson GROUP

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2460 - Radcliffe P&R Extension









PLANTING SCHEDULE

Tree	Abbreviation	Species	Height	Girth	Spread	Specimen	Plant Size	Number of Plants
ACE	Asar cam	Asar cam	350-425cm	12-14cm	H	Standard 3x B		3
BP	Betula p	Betula p	350-425cm	12-14cm	H	Standard 3x B		4
SA	Sorbus a	Sorbus a	200-250cm			Feather 2x B		2
Total: 9								

Number	Abbreviation	Species	Specimen	Densit	Vol	Plant Contribution
91	ORAV	Orav	Trans: 1-1.1m ²			20%
137	ORMON	Ormon	Trans: 1-1.1m ²			30%
46	ILEA	Ilex aquifolium	Container	1m ²		10%
91	PRUSP	Prunus s. rosa	Trans: 1-1.1m ²			20%
24	ROSAR	Rosa arvensis	Trans: 1-1.1m ²			5%
24	ROSCA	Rosa canina	Trans: 1-1.1m ²			5%
46	VRSCP	Viburnum s. rubus	Trans: 1-1.1m ²			10%
Total: 458						

No alterations to be made from this drawing. Date: 01/02/2020
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tba
Landscape Architecture
Arboriculture

Trevor Bridge Associates Ltd
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Lancs OL8 7FW
Tel: 0161-604 2090
Email: info@trevorbridge.co.uk

Project: RADCLIFFE PARK & RIDE
BOURNE PARKING
Landscape Proposal

Date: FEB'20
Scale: 1:250
Sheet: @ A1
Rev: 4465.04

By: CHS
Dwg: DIG

Ward: Ramsbottom + Tottington - Tottington

Item 06

Applicant: 2019 Ltd

Location: Land off Claybank Drive, (off Victoria Street), Tottington, Bury

Proposal: Variation of condition no. 2 (approved plans) of planning permission 63275 to amend house on Plot 1 (north) by raising roof eaves/ridge by 600mm, addition of pitched roof dormer to front elevation, additional windows to attic space and removal of hipped end to roof on west elevation.

Application Ref: 65459/Full

Target Date: 30/06/2020

Recommendation: Approve with Conditions

Description

The application relates to a construction site (0.28ha) and a recently approved application for three detached houses. To the east is a row of two storey terraced houses on Victoria Street whilst to the north and west are detached houses on Claybank Drive. The rectangular site is not within the Green Belt. There is a Public Right of Way running from Claybank Drive, down the northern boundary to Turton Road.

This proposal relates the dwelling to be constructed on plot 1, the northernmost plot, nearest to the access. It is proposed to create additional accommodation in the roof by raising the eaves and ridge by 600mm, forming a gable end on the west elevation and adding a pitched roof dormer on the front/north elevation and windows on the front and rear gables.

Relevant Planning History

63275 - Erection of 3 dwellings - Approved 14/11/18.

Publicity

Site notice posted and the following neighbours were notified by letter dated 19/05/2020. Nos.2-14 Victoria Street, 67 Turton Road, 33, 37, 39 and 41 Claybank Drive.

Objections received from 4, 6, 8 and 10 Victoria Street and 67 Turton Road.

- The pitched roof dormer to the front elevation will introduce a serious overlooking issue to the houses on Victoria Street which have a West Facing Elevation.
- The increase in height, with the already increased ground levels would reduction daylight available to houses on Victoria Street.
- More upset to residents for what appears to be so small a gain for the developers.
- Councillors/Committee should visit the site.

Those making representations have been notified of the Planning Control Committee.

Statutory/Non-Statutory Consultations

N/A

Pre-start Conditions - N/A

Unitary Development Plan and Policies

H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN3	Archaeology

EN5/1	New Development and Flood Risk
EN7/2	Noise Pollution
EN7/1	Atmospheric Pollution
EN7/5	Waste Water Management
EN8/2	Woodland and Tree Planting
HT2/4	Car Parking and New Development
H1/2	Further Housing Development
EN6/3	Features of Ecological Value
EN1/2	Townscape and Built Design
H2/6	Garden and Backland Development
EN6	Conservation of the Natural Environment
HT6/2	Pedestrian/Vehicular Conflict
HT6/1	Pedestrian and Cyclist Movement
NPPF	National Planning Policy Framework
H2/3	Extensions and Alterations
SPD6	Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Background - The proposal to construct the three detached dwellings on the site was approved by Planning Control Committee in November 2018 following a site visit. Development has commenced on the site and the house on plot three has been effectively completed. The foundations for the house on plot one, which is subject to this application, have been laid.

Visual Amenity - The increase in the eaves and ridge of 600mm and addition of windows on the front and rear gables from that originally approved is not so significant as to have a serious impact on the overall design and appearance of the house within the site.

The creation of a gable end on the west elevation and the addition of the pitched roof dormer on the front/north elevation would be more significant but again, this would not have a serious and detrimental impact on the overall design and appearance of the house and character of surrounding area. The proposed amendment is therefore acceptable in terms of visual amenity and complies with UDP Policies EN1/2 Townscape and Built Design and H2/3 Extensions and Alterations and associated guidance in SPD6 Alterations and Extensions.

Residential amenity - Given the separation distance of 23m between the side of the dwelling and houses on Victoria Terrace, the increase in the height of 600mm, even with the site being elevated in relation to Victoria Terrace, would not be so significant as to cause undue harm to the residential amenity of the occupiers of these properties.

The new gable end would be adjacent to the boundary with No.37 Claybank Drive and face the back of the existing garage to this property. No.37 Claybank has a kitchen window facing across the boundary but this would overlook the roof of the single storey outrigger on the rear on the new house and thus would not be seriously affected by the gable end.

With regard to the additional windows and pitched roof dormer, there is no direct 'window to window' issues arising. The separation distance to the nearest property to the north is approximately 12m away and this is in accordance with adopted guidance and considered

to be acceptable in terms of residential amenity. The separation distance at the rear, towards Plot 2 is 20m and this is considered to be acceptable in terms of residential amenity and compliant with UDP Policy H2/3 Extensions and Alterations.

Parking - The amended scheme would increase accommodation within the roofspace although this would not be so significant as to have an impact on the parking requirement for the property which, with a double garage and driveway, would have space within the site for at least three cars. The proposed alterations are considered acceptable and compliant with UDP policy and guidance.

Objections - The issues raised by the objectors have been addressed in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to revised plans A1826-BR-SP01A, SS02, P1-EL01, P1-GF01, P1-FF01 and P1-SF01. The development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.
6. Following the provisions of Condition 5 of the original application 63275, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the

development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

7. The landscaping scheme, approved under reference 53275 shall be implemented not later than 6 months from the date the first dwelling is first occupied; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within three years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 Townscape and Built Design and EN1/3 Landscape Provision of the Bury Unitary Development Plan.

8. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the LPA.

Reason. In order to protect nesting birds pursuant to UDP Policy EN6 Conservation of the Natural Environment and the NPPF.

9. The scheme relating to the treatment of invasive species Japanese knotweed and Himalayan Balsam shall be implemented in accordance with the approved method statement, approved under application reference 63275.

Reason - To prevent the spread of Japanese knotweed and Himalayan balsam, which are invasive species. Without it, avoidable damage could be caused to the nature conservation value of the site contrary to national planning policy as set out in National Planning Policy Framework paragraph 109, which requires the planning system to aim to conserve and enhance the natural and local environment by minimising impacts on biodiversity and providing net gains in biodiversity where possible and pursuant to Unitary Development Plan Policy EN6 Conservation of the Natural Environment.

10. The car parking indicated on approved plan reference A1826-PS-SP01 Revision A under application reference 63275 shall be surfaced and made available for use to the written satisfaction of the Local Planning Authority prior to the development hereby approved being occupied and thereafter maintained at all times.

Reason. To ensure adequate off-street car parking provision in the interests of road safety pursuant to UDP Policy HT2/4 Car Parking and New Development.

11. There shall be no direct means of vehicular access between the site and Victoria Street.

Reason. To ensure good highway design in the interests of road safety pursuant to the NPPF and UDP Policies listed.

12. Details of surface water drainage shall be implemented in accordance with details approved under application reference 63275 and in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards and include a timescale for implementation. In the event of surface water draining to the public surface water sewer, the pass forward flow rate to the public sewer must be restricted to 5l/s.

Reason: To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies within the NPPF and UDP Policies H2/2 The Layout of New Residential Development, EN5/1 New Development and Flood Risk, EN7/3 Water Pollution and EN7/5 Waste Water

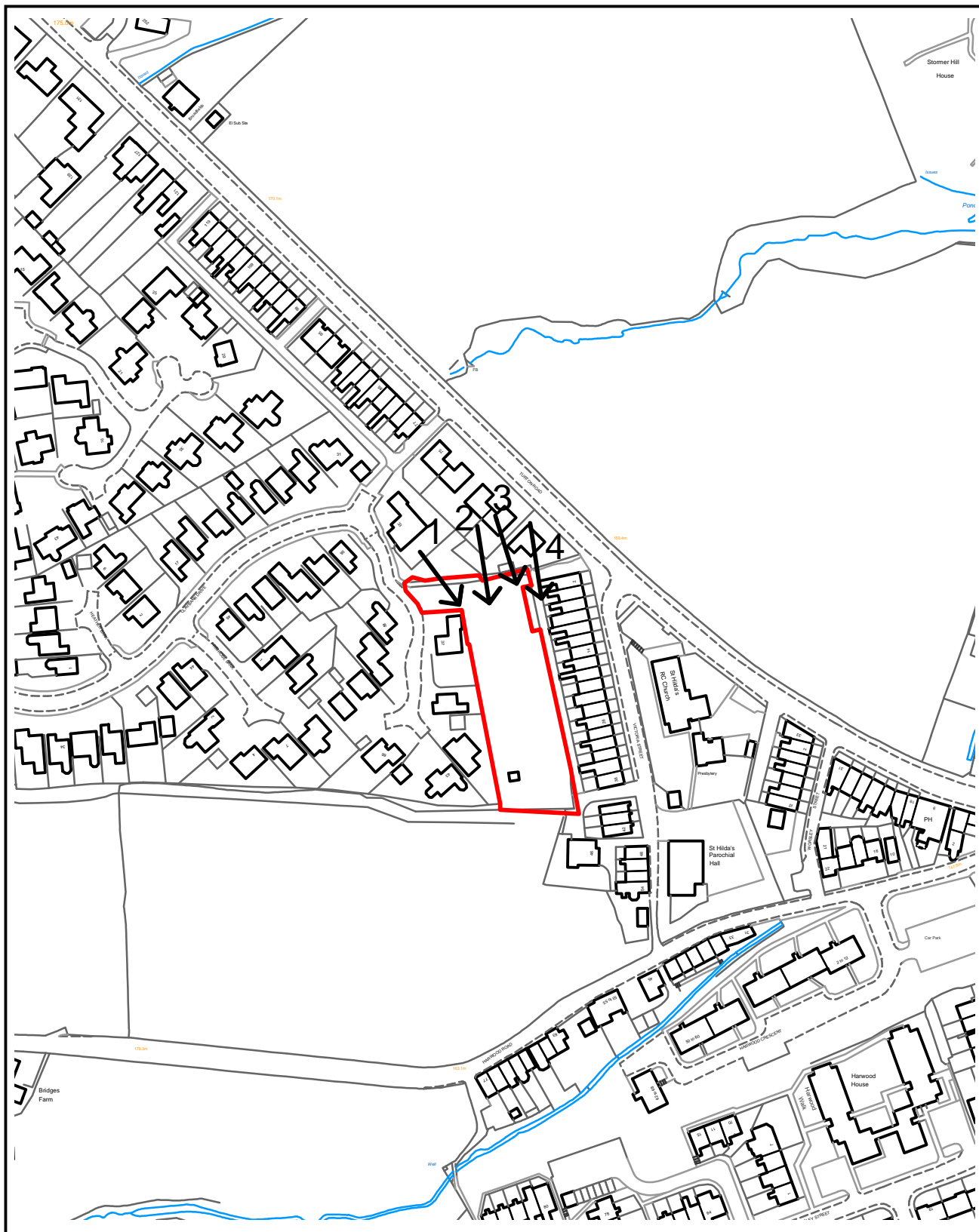
management.

13. Foul and surface water shall be drained on separate systems. The approved scheme only shall be implemented.
Reason: To secure proper drainage and to manage the risk of flooding and pollution pursuant to the NPPF and UDP Policies H2/2 The Layout of New Residential Development, EN5/1 New Development and Flood Risk, EN7/3 Water Pollution and EN7/5 Waste Water management.
14. The details of the following highway management points, approved under application reference 63275, shall be implemented prior to first occupation and to the satisfaction of the Local Planning Authority:
- Formation of the vehicular access to the site in the form of a service strip crossing consistent with the arrangements at other properties on Claybank Drive served from this section of accessway, to a specification to be agreed and incorporating the demarcation of the limits of the adopted highway across the site frontage;
 - Retention/improvement of, and connection onto, Public Footpath No. 78a, to be used as part of the pedestrian access to the proposed dwellings, to a scope, specification and width to be agreed and incorporating the low level lighting proposed, located clear of the route of the Public Right of Way;
 - Visibility splays of 2.4 x 25m at the junction of the site access with Claybank Drive in accordance with the standards in Manual for Streets, with no obstructions above the height of 0.6m within them;
 - Bin storage/collection and access arrangements from the adopted highway to be agreed with the Council's Waste Management team;
 - Tree planting and landscaping clear of the adopted highway, taking into consideration confirmed root zones;
 - The proposed new retaining wall adjacent to the unadopted access to the rear of properties fronting Victoria Street, proposed to ensure that errant vehicles are restrained within the site;
 - All associated highway and highway drainage remedial works;
 - The visibility splays shall be implemented to the satisfaction of the Local Planning Authority before the development is first occupied and subsequently maintained free of obstruction above the height of 0.6m.
- Reason. To ensure good highway design, maintain the integrity of the adopted highway and ensure the intervisibility of the users of the site and the adjacent highways in the interests of highway safety.
15. The 'Construction Traffic Management Plan' (CTMP), approved under application reference 63275, shall be adhered to throughout the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.
Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 Townscape and Built Design and HT6/2 Pedestrian/Vehicular Conflict.
16. The road shall be maintained in accordance with the approved management and maintenance details approved under application reference 63275.
Reason. To ensure that the estate roads serving the development are maintained to an acceptable standard in the interest of residential / highway safety.

17. The access road referred to in the previous condition shall be constructed in accordance with the approved details.
Reason. In the interests of highway safety pursuant to UDP Policy H2/2 The Layout of New Residential Development.
18. The vehicular and pedestrian access arrangements indicated on approved plan reference A1826-PS-SP01 Revision A shall be implemented to the satisfaction of the Local Planning Authority before the development is first occupied.
Reason. To ensure good highway design in the interests of road safety pursuant to UDP Policy H2/2.
19. The turning facilities indicated on approved plan reference A1826-PS-SP01 Revision A shall be provided before the dwellings are first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.
Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to UDP Policy H2/2.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65459

**ADDRESS: Land off Claybank Drive
Tottington**



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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65459

Photo 1



Photo 2



Photo 3



Photo 4







NOTES

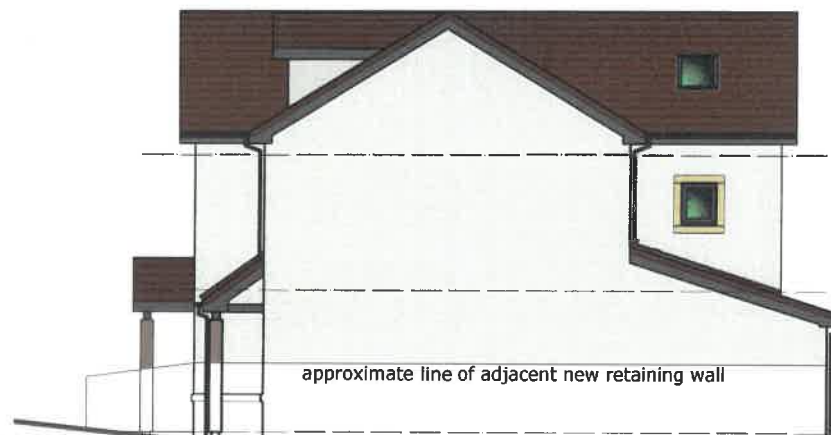
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- Contractors to check all dimensions on site prior to commencement of works.
- This drawing is to be read in conjunction with all relevant consultants' and/or specialists' drawings/documents and any discrepancies or variations are to be notified to the architect before affected work commences.

REVISIONS

-

NOTE:

- denotes outline of previous consented house types



approximate line of adjacent new retaining wall



Lancashire Office;
18 Pickering Close
Bury
Lancashire
BL8 1UE
Tel: 079555 69385
Email: chris.shiels@aa-d.co.uk

Proposed housing development:
PLOT ONE

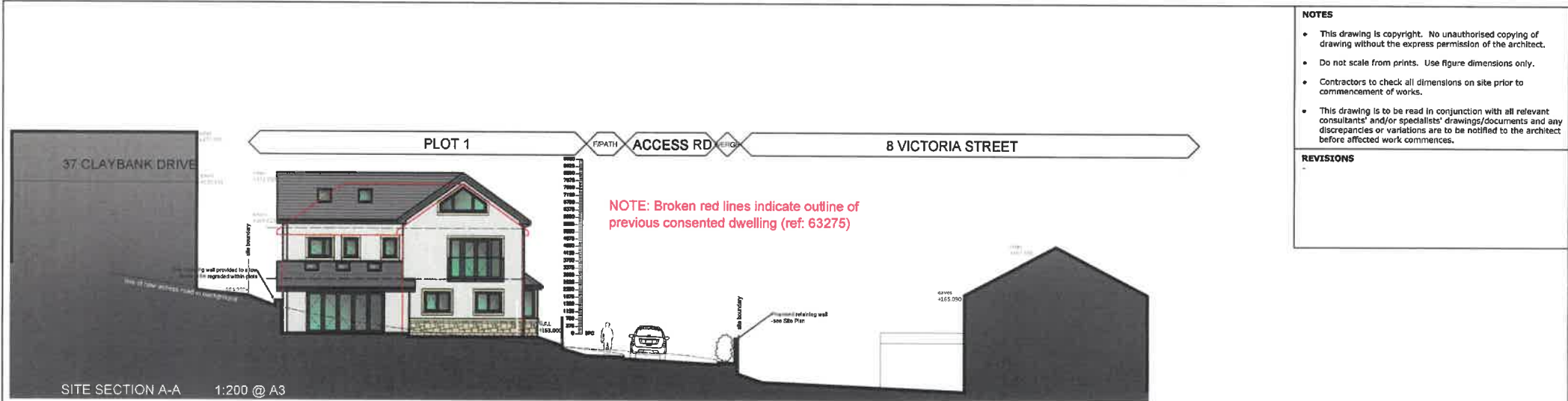
2019 Ltd.

ELEVATIONS

A1826-BR-P1-EL01

Scale 1:50 @ A3

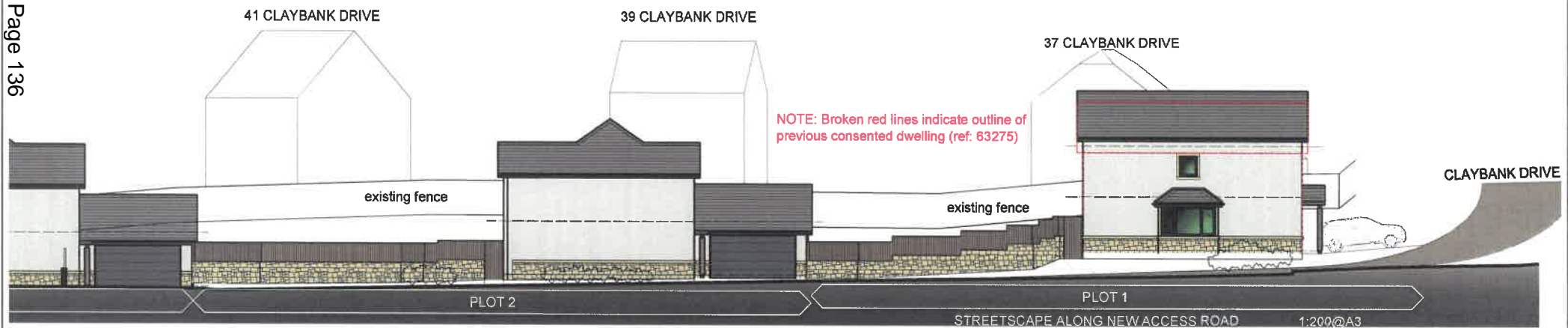
0 1 2 3 4 5 6 7 8 9 10 2.5m



NOTES

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REVISIONS



Lancashire Office:
18 Pickering Close
Bury
Lancashire
BL9 1UE
Tel: 079555 69385
Email: chris.shiels@aa-d.co.uk

Proposed housing development:
Claybank Drive, Tottington, Bury

2019 Ltd.

PROPOSED SITE SECTIONS

A1826-BR-SS02

Scale 1:100 @ A1

0 1 2 3 4 5m

Ward: Whitefield + Unsworth - Pilkington Park

Item 07

Applicant: Bourne Group Limited

Location: Whitefield Metrolink, Stanley Road, Whitefield, Manchester, M45 8AB

Proposal: Reconfiguration of the bus turning head and the erection of an additional deck and ramp to form a second floor to car park, providing in total an additional 123 no. spaces; landscaping scheme and lightning

Application Ref: 65465/Full

Target Date: 20/08/2020

Recommendation: Approve with Conditions

Description

The application relates to the car park which serves Whitefield Metrolink Station. The site is approximately 0.9 hectares in area located within the town centre. To the northern boundary is Whitefield Park which is separated from the site by dense tree planting and landscaping and to the east are industrial/commercial units which form the Park 17 industrial estate. The nearest houses are to the south on Nuttall Avenue which are separated from the car park by the Metrolink line which runs along this boundary in a north/south direction. To the north of the site is the access into the Metrolink station.

The northern part of the site provides a bus turning area and bus stops for local services with the southern part of the site utilised by the Metrolink car park which comprises two levels of parking - the ground floor level providing 102 No Standard bays and 13 No disabled bays (115 in total) and a first floor deck directly above for 94 spaces, equating to 209 total provision. The access into the car park is at the southerly point of the turning circle and follows an anti-clockwise direction around the car park. Pedestrian stairways are located at either end of the deck.

The access into the Metrolink site is off Stanley Road, opposite Morrison's supermarket and close to the 4-way junction with Bury New Road and Church Lane. The pedestrian entrance to the Metrolink platform is either via the car park or from the main road just outside of the site.

There are 5 Sheffield cycle stands located on the western side of the car park close to the disabled parking spaces.

The application proposes to erect a 2nd floor parking deck to provide additional parking for the station and uses of the Metrolink. There would be a net gain of 123 spaces provided.

The proposed deck would be located on top of the existing 1st floor deck, essentially mirroring the 1st floor arrangement which would provide a two way ramp and pedestrian stairwells. The layout of the existing car park would be re-configured to facilitate the new circulation route, ramps and additional spaces required for the new deck.

The development also proposes alterations to the drop off area, bus turning area and bus stop in the northern section of the site. A turning area would still be provided to enable a circular route but it is now proposed to also provide additional parking spaces in this part of the site. There would be a row of 8 spaces (4 drop off and 4 permanent) located centrally in an 'island', around which cars would circumnavigate, 10 spaces along the northern boundary to replace the existing drop off bay, 8 spaces to the southern boundary and 6 bays to the front of the main car park and 1st floor deck. These works would require some tree removal along the boundaries.

The direction of the one way traffic system into the main car park and decked floors would be re-configured to flow in a clockwise with the access into the main car park re-located to the current exit position and likewise the new exit formed at the existing entrance.

With the re-configuration of the existing car park and the additional 2nd deck, there would be a net gain of 123 No. spaces.

The total number of spaces provided on completion would be 332 spaces.

The new deck would comprise a lightweight steel frame with low horizontal safety barriers similar to those at the first floor level and finished in a dark grey asphalt or tarmac surface. There would be a ramp and two sets of pedestrian stairways built to facilitate access to the new deck, the same as the existing arrangement.

The application also proposes to incorporate lighting columns and emergency lighting within the new deck, together additional CCTV security cameras.

The scheme includes replacement planting along the north and southern boundary in the region where trees are proposed for removal.

The development would be built out in a phased approach. The applicant has submitted a phasing plan, with the works falling into 3 phases - Enabling works, (sectional closures of the existing car park), main works (full closure of the existing car park) finishing works (full closure) followed by completion.

The Whitefield Metrolink station would remain open throughout the construction period.

For information, a planning application has also been submitted by TfGM to erect a 2nd floor parking deck at the existing Metrolink car park in Radcliffe - planning reference 65354. It is anticipated this will be delivered in conjunction with the expansion at Whitefield.

The Applicant states that replacement parking to compensate for the loss of parking whilst the works are underway is being investigated. It is anticipated that an application for alternative temporary parking arrangements is to be submitted in due course. It is likely that a joint replacement scheme is to be submitted for both the Whitefield and Radcliffe sites whilst works are progressed. The provision of the replacement parking would be secured by condition and to be provided prior to the commencement of works at the Metrolink.

TfGM states that the proposed development at Radcliffe and Whitefield are the first in a number of expansions being explored for other Park and Ride Schemes (P&R Schemes) in the Greater Manchester Area for the future.

Relevant Planning History

56461 - Erection of deck to form first floor to car park to provide an additional 83 spaces - Approved 25/6/2013.

To clarify numbers of existing spaces, whilst there are currently 94 No. spaces at first floor, there was a net gain of 83 in total as the development for the 1st floor deck resulted in the loss of the spaces at the 'ground floor level' by the provision the ramp and re-configuration of circulation space to facilitate the additional 1st floor deck.

Publicity

Letters sent on 26/5/20 to 137 properties. A full list of those notified are available to view on the public file.

Site notice posted 28/5/20

Press advert 28/5/20.

A total of 4 representations received.

4 objections

Objections -

- Deeply concerned about the addition of another level being added.
- Angry and disappointed that the story was in the Radcliffe Times 3 months ago before lockdown but the residents were never given the chance to air their views and ask any questions as we have only been informed now.
- Would be unsightly, overbearing for the space and an eyesore basically a multi storey car park, causing 124 more cars to congest the small access road at a busy junction, pollution from extra cars and added traffic noise .
- Light pollution from lit area.
- Demographics of Besses , Radcliffe and Prestwich would mean that people making use of this extra parking would be driving less than 1 mile to use because of proximity to other stations -would it not be better to invest in secure bike stores at this location for people to cycle locally down to station .
- More CCTV is a joke - they do not monitor it now.
- Should be adding more cycle bays instead of 124 parking bays next to a children's playground.
- As a staunch tree lover with strong environmental concerns, note the removal of some trees - these are beautiful mature trees - why is this necessary? Does it justify chopping trees down?
- It is understood that the trees to be removed are not protected or within Whitefield Park.

The Applicant has provided a response to the representations made above, summarised as follows -

- Air quality, pollution, and residents' well-being - The Transport Statement that has been submitted as part of the Planning Application for this scheme has concluded that the introduction of the proposed additional car park deck will not have a material impact on the operation of the highway network in this location. Results from the accumulation survey undertaken on Friday 22nd June 2018 and Saturday 23rd June 2018 were used to assess the existing usage of the station car park between 7.00am-19:00pm on both days. Air Quality Assessment - a detailed survey was conducted in February 2020, which was desktop based and concludes that "Based on the assessment results, air quality is not considered a constraint to planning consent for the proposed development." A separate cycle scheme is being developed for Radcliffe Park & Ride (P&R) , where cycle parking will be upgraded in line with TfGM's wider cycle parking strategy. It is also anticipated that the lower car park usage currently being experienced, will enable construction to proceed with less disruption to users and local residents. In addition, we are currently assessing the potential requirement to construct a temporary car park to offset the loss of parking.
- Noise, traffic and congestion - The Noise Impact Assessment at Radcliffe took place on Thursday 13th March 2020 between 16:05 and 20:05 and Friday 14th March 2020 between 06:10 and 10:10. The timing of the survey was determined to align to both the morning and evening peak. The survey concluded there would be an insignificant increase to the existing noise levels. The LPA's Pollution Control Officer has raised no concerns regarding noise disturbances.
- Why is it proposed to expand Whitefield P&R rather than other P&R at Metrolink stops?
- The selection of Whitefield P&R facility as a site to expand, has been reviewed with several factors in mind, including the wider Greater Manchester (GM) strategies and future development over the coming years. The Business Case completed for the Metrolink Capacity Improvement Programme (MCIP), which includes Whitefield P&R, considers its strategic alignment with Transport for the North's vision to transform the region's economy, which is set out in 'The Northern Powerhouse: One Agenda, One Economy, One North' (2015) which recognises the importance of local connectivity. The Business case also considered the Greater Manchester Spatial Framework (GMSF). For Whitefield this means sustainable travel, therefore reducing congestion, better access to employment areas, support regeneration of town centres and travel for future new housing. The proposed scheme at Whitefield P&R is to be delivered in conjunction with the expansion at Radcliffe P&R and not in isolation, to offer a combined increase in spaces for the area.

- What about cycle facilities, will these be improved? A separate cycle scheme is being developed for Radcliffe Park & Ride and to upgrade where cycle parking provision in line with TfGM's wider cycle parking strategy. Plans for the car park extension will facilitate the delivery of the future cycling enhancements.
- What impact will this development have on residents in neighbouring streets? Please provide a cross section drawing to indicate this has been taken into account - Feedback on the site has been considered and incorporated into the proposed design prior to the submission of planning. This includes light spill protection measures to shield neighbouring houses from car headlights. A cross section drawing has been submitted to show how the structure will stand in relation to the houses in closest proximity (Nuttall Avenue) and with regards to landscaping.

The full response is available to view on the public file.

Those who had made representations to the application have been notified of the Applicant's response on 3/7/20.

No further comments received.

Statutory/Non-Statutory Consultations

Traffic Section - No objection in principle. Conditions to be finalised in the Supplementary agenda.

Borough Engineer - Drainage Section - No response received.

United Utilities (Water and waste) - No objection subject to a condition for the submission of a drainage scheme.

Environmental Health - Contaminated Land - No objection subject to conditions

Environmental Health - Pollution Control - No comments to make

Greater Manchester Police - designforsecurity - Response to be reported in the Supplementary Agenda

EDS Head of Parks and Countryside - No response received.

Greater Manchester Ecology Unit - Recommend that if any mature trees are to be removed that they are assessed for bat roosting potential prior to determination.

Conservation Officer - No objection.

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN6/4	Wildlife Links and Corridors
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
HT2/4	Car Parking and New Development
HT2/5	Public Car Parks
HT2/6	Replacement Car Parking
HT3	Public Transport
HT3/4	Schemes to Assist Metrolink
HT4	New Development
HT5/1	Access For Those with Special Needs
SPD11	Parking Standards in Bury
EN1/5	Crime Prevention
EN7	Pollution Control
HT2	Highway Network
HT6/1	Pedestrian and Cyclist Movement
EN8/2	Woodland and Tree Planting

EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EC2/1	Employment Generating Areas
HT3/1	Schemes to Assist Bus Movement
HT3/2	Bus Services
HT3/3	Design of Roads for Bus Routes

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policies - At the heart of the NPPF is a presumption in favour of sustainable development and states the three objectives to achieving sustainable development which are:

- economic objective - to help build a strong, responsive and competitive economy , including identifying and coordinating the provision of infrastructure;
- social objective - to support a strong, vibrant and healthy communities....by fostering well-designed and safe built environments with accessible services;
- environmental objectives - contribute to protecting and enhancing the natural, built and historic environment, including making effective use of land including moving to a low carbon economy. Planning policies and decisions should play an active role in guiding development towards sustainable solutions.

Section 9 of the NPPF - Promoting sustainable transport seeks to ensure that appropriate opportunities to promote sustainable transport modes can be - or have been - taken up, given the type of development and its location; safe and suitable access to the site can be achieved for all users and any significant impacts from development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

At paragraph 109, development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Development that would generate significant amounts of movement should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed.

UDP Policy HT1 - A Balanced Transportation Strategy seeks to ensure a co-ordinated approach to the formulation of policies and proposals for the alteration, upgrading or improvement of the transport network. This includes implementing measures to increase attractiveness of public transport as a viable alternative to private travel; facilitating better interchange between different modes of transport; and improving the accessibility of public transport to those travellers whose mobility is impaired.

At policy HT2 - Highway Network seeks, amongst other things, to improve the operation and attractiveness of the public transport network, improve the flow of traffic and improve or protect the environment.

Policy HT3/1 - Schemes to Assist Bus Movement seeks to promote and support measures to assist bus movement, including measures for highway improvements or traffic management schemes designed to assist bus movement, provision of lay-by's, bus

turnarounds or other infrastructure designed to make bus operation more efficient and attractive to users.

Policy HT3/2 - Bus services - Seeks to ensure that adequate bus services and infrastructure are provided to meet the needs of those travelling.

HT3/3 - Design for Roads for Bus Routes seeks to ensure that where appropriate the road layout in all new developments incorporates appropriate roads to a sufficient standard of design to accommodate bus services.

Policy HT3/4 - Schemes to Assist Metrolink seeks to support the provision of new or improved stations and car parks which will increase the available park and ride facilities on the Metrolink.

In considering new development, Policy HT4 - New development encourages proposals which will support the principles of sustainable development, assist the implementation of a balanced transportation strategy and minimise the environmental impact of traffic.

Principle and Need - The Applicant states that the proposals to expand the Whitefield Park and Ride (P&R) Facility has been evaluated with regards to a number of factors, including the wider Greater Manchester Spatial Framework (GMSF) and future developments over the coming years.

The Metrolink Capacity Improvement Programme (MCIP) Business Case recognised that improvements to public transport and sustainable modes of travel must be enhanced, whilst also seeking to reduce air pollution and help deliver regeneration and economic growth to areas. With specific reference to Whitefield, improvements would provide sustainable and attractive alternatives to driving, enable better access to employment and local areas, support the regeneration of the town centre and provide improved links to new development and new housing in the Borough.

Whitefield was also highlighted as one of the stops for park and ride improvements as part of the Association of Greater Manchester Authorities (AGMA) Scheme, which recognised the shortfall of car parking at Whitefield particularly on weekdays. Bearing in mind the 'bigger' picture aspirations of growth, the expansion of the P&R facility at Whitefield Metrolink would support long-term objectives for sustainable development, and ease congestion on key routes whilst bettering air quality along these corridors.

It must be noted that the GMSF as a development plan would not have any significant weight in the planning decision making process and is not material at this time until the plan nears adoption. What is clear is that the Council must prepare a local plan and following the requirements of National Policy seek to uphold an agenda for growth which would encourage the development of sustainable development for transport, housing and employment.

In addition, the Metrolink Improvement Programme includes the expansion at Radcliffe's P&R for an additional deck at the station. Delivered in conjunction with Whitefield, there would be a combined increase in spaces in the area.

The shortfall in capacity would alleviate the pressures and congestion on the local and wider routes between Bury and Manchester in particular and would also aid in relieving the pressure on overspill parking to the local surrounding streets.

In terms of the justification for the expansion at the Whitefield P&R, the Applicant has sought to demonstrate that proposals have been developed and modelled appropriately taking into account the broader growth strategies and aspirations of a growth agenda as well as considering the benefits the scheme would bring to ease the immediate parking pressures in the immediate locality from commuter parking in relation to current demand. By providing the additional deck, the proposed development would continue to contribute to

sustainable development in allowing greater use of public transport instead of encouraging longer vehicle movements between Bury and Manchester and beyond.

No one solution alone of sustainable travel would be a magic bullet, but overall on a growth agenda there must be a package of different options available. This Metrolink Scheme would provide one contribution and therefore as a principle must be acceptable.

In considering whether the development should be in Whitefield or elsewhere, it is acceptable as a matter of principle and it is the details as to whether the scheme relates appropriately to its surroundings, works in relation to traffic flows, is considered in relation to air quality for the area where it would be located and is considered in the wider scheme of approaches concerning sustainable travel options.

It is therefore considered the proposed development would comply with the principles of sustainable development and the NPPF and UDP Policies HT3/4 and HT4.

Heritage - Part of the application site borders part of the north east boundary of the Whitefield All Saints Conservation Area.

Paragraph 189 of the NPPF requires that in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary.

Paragraph 190 of the NPPF requires Local planning authorities to identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

Section 72 of The Planning (Listed Buildings and Conservation Areas) Act 1990 requires local planning authorities to give special attention to the desirability of preserving or enhancing the character or appearance of the conservation area.

Identified harm to a designated heritage asset or its setting, in this case the setting of the All Saints Conservation area, even when identified as less than substantial harm cannot preserve or enhance the character and appearance of a conservation area as a requirement of Section 72 of The Planning (Listed Buildings and Conservation Areas) Act 1990 without clear and convincing justification and public benefit.

The proposed second floor car parking deck and reconfiguration of the bus turning area would have no more significant impact on the setting of the conservation area than the existing car deck and turning area and no more prominent comparative to Morrison's supermarket and the lattice telecoms tower adjacent the north east boundary of the Metrolink site or within the visual context of the site. The interrelationship of these structures is such as to not represent a cumulative negative impact on the setting of the conservation area. The proposed second deck and turning reconfiguration would not interrupt views or vistas either in or out of the conservation area and would not be viewed within the setting of it.

Any trees or shrubs lost as a consequence of the development being carried out should be replaced and the perimeter rail for the proposed deck should be of the open mesh type in order to reduce the impact on the visual amenity of the occupiers of Nuttall Avenue.

It is therefore considered that the proposed development would not represent harm to the

character and appearance of the conservation area and therefore would preserve it and as such would comply with the principles of the NPPF and Section 72 of The Planning (Listed Buildings and Conservation Areas) Act 1990.

Design and layout - The proposed 2nd floor deck would be located directly on top of the existing 1st floor deck, maximising the use of the site to provide the additional parking spaces. This would require some re-configuration of level 0 and level 1 to provide the necessary circulation routes and ramps, but the general arrangement would remain unchanged.

There would, however be a change to the direction vehicles currently enter and exit the car park. The existing exit into level 0 would become the entrance, and likewise the existing entrance would thereafter become the new exit. This would enable traffic to manoeuvre straight into the car park rather than navigating part of the turning area, resulting in a more free flowing and safer route into and out of the car park.

The proposal to re-configure the drop off and turning areas would not fundamentally alter the space for vehicles to manoeuvre round the site but would better utilise this area of the site by re-locating the bus stop and providing parking bays where there are currently none.

It is proposed to provide 10 spaces along the northern boundary in place of the existing drop off zone. There is substantial tree planting along this area, some of which are identified as mature and in relatively good health. Two mature trees would be removed but these have been identified with defects and have low or moderate future growth potential. The majority of the trees would be retained and it is proposed to provide replacement native and heavy standard tree planting in this vicinity and with tree protection measures incorporated to protect the remaining trees (see Tree Section below).

The existing 'island' which consists of a raised tarmac strip would be replaced to provide 8 spaces (4 for drop offs only and 4 parking bays) would still allow for traffic to circulate the site in a clockwise direction. It is also proposed to provide 6 additional parking bays along the front of the car park and a row of 8 spaces in place of the existing bus shelter, which would be replaced by a bus stop located just to the north. There would also be some tree loss which would be compensated for by replants. The existing bus service schedules would continue to operate as it does currently.

The existing access and egress onto Stanley Road from and to the site would remain unchanged.

In terms of design, the proposed deck would be modelled on the design of the existing deck in terms of its appearance and materials, proposing a steel frame and horizontal barriers around the periphery of the deck. The surfacing would be finished in a dark grey asphalt or tarmac, as is standard for such multi-decked parking structures and the same used for the 1st floor deck.

It is therefore considered that by adding an additional deck to the existing structure, the requirement to address the parking capacity issues on the site would make best use of the potential land available whilst re-configuration of the turning areas would improve the circulation around the car park. The car park facility would continue to operate within a safe and secure environment for Metrolink users.

The design would be appropriate and functional for the purposes it would serve and emulate the existing structure.

As such, the proposed development is considered to be acceptable and would comply with UDP Policies EN1/2, HT3/1, HT3/2, HT3/3, HT3/4, HT6/1 and HT4.

Impact on residential amenity - The nearest properties to the proposed development would be the 2 storey semi-detached properties which are located to the west of the site on

Nuttall Avenue. The rear of the houses face the site.

The existing decked car park is approximately 4.5m high to the top part of the rail which runs along the sides of the car park. The additional deck would increase the height of the structure to approximately 7.5m.

There would be a minimum distance of 30m from the structure to the houses on Nuttall Avenue at the closest point. There is fairly dense and mature tree planting between the car park and the tram line, which already obscures much of the existing structure from views. Whilst the development would add more height, with these intervening features additional tree planting and given the separation of 30m, it is considered that visual impact and outlook from the houses would not be detrimentally affected by the proposed development.

The proposed 2nd floor deck would be erected in a material of a relatively lightweight appearance with the use of a steel frame and horizontal barriers, mirroring the existing form and structure. There would be one set of pedestrian access stairs to the decks which would be located on the corner opposite to the houses on Nuttall Avenue but this would not add significant bulk to what already exists. In terms of overshadowing, these houses are orientated to the south of the site and loss of light would not be significantly harmful or adverse, particularly given the distance away of the proposed structure and the intervening features.

Given the distance away of the houses on Nuttall Avenue, the intervening existing and proposed landscaping along the southern boundary and the Merolink tramline itself, it is considered that the addition of the 2nd floor deck would not introduce an overbearing or dominant feature when viewed from the houses and therefore not have a detrimental impact on the amenity of these occupiers.

It is therefore considered that the physical relationship of the 2nd deck to the nearby houses would be acceptable and the proposed development would comply with UDP Policies EN1/2.

The issues of impacts from noise, lighting, pollution and traffic are covered in the specific sections below.

Noise - A Noise Impact Assessment has been carried out, taking into account the morning and evening peaks. The Report has been reviewed and assessed by the LPA's Pollution Control Section. The Report concluded that there would be a likely increase of between 0 and 0.1 to the existing ambient noise levels at the nearest noise sensitive receptor which would indicate that there would be no or no significant effects to the existing noise climate in the area. The Pollution Control Section are satisfied with this conclusion.

There is already a degree of noise which is generated by the Metrolink tram which runs between the car park and the houses on Nuttall Avenue. Given the relatively minimal increase in noise which would be generated by the proposed development, it is considered there would not be significant harm caused to local residents in terms of additional noise generation.

Lighting - The application proposes lighting similar to the current arrangement on the 1st floor deck, which consists of 4m high column mounted luminaires located around the periphery and within the parking areas of the new deck. The lighting columns would be of a slimline type and the lamp would be a 'flower pot' design which would direct light downwards, the same as the existing columns. The lights would be controlled by a combination of daylight sensors and timers and a condition would ensure that the lights would only be on at the early parts of the day, to serve the first commuters, and switched off half an hour after the last tram would leave the Metrolink stop, providing both security and safety to the station and commuters whilst minimising impacts from light pollution to those living in the immediate locality.

Lighting would also be fitted under the deck on a sensor module and emergency lighting provided to driving lanes, parking bays and to clearly define pedestrian walkways to provide a safer environment.

In terms of light spillage from car headlights, the development proposes low level barriers around the periphery of the deck, and as are currently in situ for the existing 1st floor deck, which would shield light from vehicle headlights using the car park. It is therefore considered there would not be significantly more harm or impact to the residents to the south than the existing situation.

Located over 30m away from the nearest property, it is considered that given this separation distance, the directional orientation of the lighting, and control of the timings of luminance by condition, it is considered that the proposals would not cause any more harm or have an adverse impact on residential amenity than the existing arrangements on site.

Air Quality/Air Pollution - The Environmental Health Section have been consulted on the proposed development.

The site is located within an Air Quality Management Area. Due to the nature of the development, it is considered possible that the development would create an increase of more than 100 AADT (annual average daily traffic). Therefore, an air quality assessment has been carried out which has concluded, based on the modelled predicted concentrations, the impact on air quality of the redevelopment was considered negligible and therefore, based on the IAQM and EPUK Guidance, 'not significant'.

The above assessment is acceptable to this Section.

Given the requirement to decrease NO₂ across Bury, as required by Defra in the 'UK Plan for Tackling Roadside NO₂', published in July 2017, it is important that the proposed development incorporates good design principles and best practice measures as outlined in IAQM and EPUK Guidance, to ensure emissions are minimised.

The section therefore welcome proposals to install mitigation measures such as electric vehicle charging points and cycle facilities to encourage the use of low emission and low carbon transport option.

There are currently no electric vehicle charge points at the site. However, the information submitted with the application suggests that 6 no. electric vehicle (EV) charging points are proposed in the future. Given the requirement to reduce emissions and improve air quality in the Borough, and in light of good design and best practice measures it is reasonable to apply a condition that details and a scheme for the provision of EV charging points/bays be submitted for approval.

The current COVID-19 crisis has also seen a significant increase in participation in cycling, which is encouraged to continue. The existing site currently has Sheffield stands, located adjacent to the site access. However, the Section would welcome improved, secure cycle parking at the station, to encourage the uptake of the cycling and multi-modal travel. It is understood that the Sheffield stands will be relocated adjacent to the ticket office for improved security.

The Whitefield Metrolink P&R site is included within the cycle and pedestrian route investment scheme which is being delivered collaboratively by Bury Council & Transport for Greater Manchester (TfGM) with Department for Transport funding. As such the proposed provision of cycle upgrades and storage facilities will be undertaken under a separate application.

The aspiration of TfGM is to ensure that their plans for the car park extension will facilitate the delivery of future cycling enhancements. TfGM continue to promote the increase in use of sustainable travel modes such as cycling and the promotion of sustainable development.

It is therefore considered that at this time, it would not be reasonable to put in place permanent cycle storage facilities given the future plans. It would, however, seem sensible for a temporary solution to be put in place to facilitate the extra demand for cycle parking which could arise from the proposed development and this would be informed by an appropriate condition.

Highways issues

Traffic -

A Transport Statement has been prepared by SCP which examines the consequences of the proposed expansion on the current highway and transportation systems. A parking accumulation survey was undertaken to assess the usage of the car park. The survey was carried out from 7am to 7pm and all parking spaces were surveyed in 15 minute segments.

The additional 123 parking spaces has the potential to increase vehicle trips to the site given the car park occupancy reaches capacity. A peak hour trip rate for arrivals during the AM peak and departures during the PM peak has been derived from the on-site survey.

It has been estimated that 56 additional vehicles would arrive at the morning peak times and as the survey demonstrated that evening trips are more staggered, departures of the site would result in an additional 49 vehicles. Other movements would likely take place out of peak periods. In terms of volume of traffic, this would equate to 1 vehicle movement per minute at the AM peak and 1 vehicle every 1-2 minutes at the evening time.

The increase in trips would likely be predominantly commuter trips which already take place on the highway and it is expected that with the expansion of the P&R to provide more parking for the Metrolink, local commuters who are having to travel to further afield to reach their destinations due to the lack of nearby parking facilities would instead be able to park at Whitefield station and thereby reduce the number of cars on the roads.

The Transport Statement considers that the proposed development would not have a significantly material impact on the operation of the highway network. The LPA's Highway section have raised no objection to the proposed development in principle, subject to conditions.

As such, it is considered that the proposed development would be acceptable and would comply with UDP Policies HT2, HT3/4 and HT4.

Access

There is currently one access in and out of the site off Stanley Road. This is close to the crossroads and major junction which connects to Bury New Road, and the exit out of the site, especially when travelling northbound, can make it difficult to manoeuvre out of the site onto the highway network.

That said, the site access is generous in its width and there is room for cars to wait at the junction with Stanley Lane and for cars to turn into the site. There is also an ample length of road before the car park is reached and which can facilitate a number of cars waiting to exit the site.

The existing access would remain unchanged and it is considered that given the scale of the development and additional numbers of cars which would be leaving the site at any one time, and particularly at peak periods, the existing access would be acceptable.

Parking

The existing car park currently provides a total of 209 spaces, comprising 115 (inc 13 disabled spaces) at level 0 and 94 at level 1.

With the re-configuration of the site and the additional parking proposed, the scheme would provide:

- level 0 - 118 spaces (inc 13 disabled)

- level 1 - 88 spaces
 - level 2 - 94 spaces (new spaces)
 - bus turning area - 32 spaces
- resulting in the total of 332spaces for the site.

SPD11 - Parking Standards in Bury does not advise on maximum standards for P&R facilities. For tram stations, it is stated that individual consideration be given to proposals where appropriate and practical.

TfGM have identified that the car park at Whitefield is used to capacity, and there are incidents of overspill parking to surrounding streets, plus additional traffic on commuter routes as people have to travel further to their destinations.

The proposed development would aide in mitigating this problem as well as providing a more attractive and sustainable option to car travel.

The additional parking would be provided at an existing facility. It would not require any more land use than already is utilised and therefore appropriate in terms of its expansion, locality and sustainability.

It is therefore considered that the proposed development, in terms of the provision of the additional spaces would be acceptable and comply with UDP Policies HT2/4, SPD11 and the principles of the NPPF.

Replacement parking - The development would be carried out in phases, resulting in the loss of some spaces whilst enabling works are carried out and full closure of the car park following commencement of the main works. In order to maintain the availability of most of the existing parking spaces for as long a period as possible, the preparation works to the existing frame would be undertaken whilst the car park remains open and in use.

The applicant is conscious that on commencement of the construction works, users of the Metrolink P&R would need to be displaced elsewhere, and they are currently investigating the provision of a temporary parking facility in order to reduce disruption to local residents from parking on the nearby streets. It is anticipated that a planning application is shortly due for submission. Notwithstanding this, a condition would be recommended requiring the submission of a replacement parking scheme together with a timetable for implementation.

With a mitigation scheme in place, which would appropriately compensate for the loss of number of parking spaces throughout each phasing of the development, it is considered there would be no displacement of cars from current users of the Metrolink onto the nearby residential streets and as such the development would be acceptable.

Security/anti-social behaviour - The proposed development would include a series of mitigation measures designed to prevent anti-social behaviour. These would include installation of lighting, provision of additional CCTV and securing the site out of hours.

The P&R is currently Park Mark accredited, and the Safer Parking Scheme is an initiative in partnership with the association of Chief Police Officers aiming to reduce fear and crime in parking facilities. These requirements mean that the parking operator has put in place measures to deter crime and anti-social behaviour. TfGM have liaised with Park Mark to ensure that the proposed scheme would continue to comply with the current accreditation based on the provision of security measures such as CCTV and lighting and TfGM's commitment to the reduction of crime on the Metrolink lines.

The applicant states that the car park is also patrolled on an ad-hoc basis to deter 'non-users' from using the car park and TfGM also work in partnership with GMP Neighbourhood Teams to share local information and knowledge and take part in joint initiatives to tackle anti-social behaviour.

Greater Manchester's dedicated Bus Watch scheme have special constables who have been tackling crime and anti-social behaviour on the bus network for over 2 years and who now also cover the Metrolink network. Since its inception, the initiative has reduced crime and anti-social behaviour across the region's bus network.

The applicant has submitted a Crime Impact Statement and have engaged with the Design for Security team at Greater Manchester Police. It is understood that in principle, GMP have no objection to the proposed development. The response will be reported in the Supplementary Agenda.

It is therefore considered that there would not be any adverse security implications or rise in anti-social behaviour as a result of the proposed development and as such the development would comply with UDP Policy EN1/5.

Ecology -

GMEU have been consulted on the application.

Summary - Potential ecological issues include bats, nesting birds and invasive species.

Bats - The existing multi-storey car park is very low risk for bats consisting of open metal supports with no cladding that could provide bat roosting opportunities. Whilst there may be opportunities within the decking this is to be retained, with disturbance limited to adding an additional level. GMEU are therefore satisfied the risk are extremely low and can be covered by an informative.

Trees are however to be removed. However, some of the poplars adjacent to this Metrolink are fully mature and therefore may have bat roosting potential. GMEU therefore recommend that if any mature trees are to be removed that they are assessed for bat roosting potential prior to determination.

An Ecology report has since been received which identified that none of the trees scheduled for removal support potential roost features indicating there would be no adverse impact on bats. Bats are however likely to forage in the vicinity and it is therefore recommended that three Schwegler bat boxes are installed on retained mature trees. This would be a condition of an approval.

Nesting Birds - Trees and shrubs, potential bird nesting habitat, will be lost. All British birds nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife & Countryside Act 1981, as amended. GMEU recommend a condition that no works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the LPA.

Invasive Species - Rail corridors are high risk for invasive species such as Japanese knotweed and Himalayan balsam, although some control of knotweed along the Manchester to Bury line has occurred in recent years. The development is also adjacent to a formal park, there is therefore a risk of rhododendron being present. GMEU recommend a survey which can be dealt with via condition.

Contributing to and Enhancing the Natural Environment - Section 170 of the NPPF 2019 states that the planning system should contribute to and enhance the natural and local environment. The main ecological impact is the loss of primarily non-native trees.

The majority of the trees requiring removal or pruning works are located along the northern boundary. There would also be some removal/pruning works to a group of trees to the south, where parking bays are also proposed. The Arboricultural Report assesses the trees to be removed as Category B - that is of moderate value.

The landscaping scheme proposes replacement tree planting to the north and replacement

tree planting is proposed adjacent to the Metrolink (a wildlife corridor) using native species. GMEU have no objection to the proposed species which are native or the level of planting proposed but would also recommend the provision of three bird boxes on retained trees as mitigation for loss of bird nesting habitat. The detail can be conditioned.

Response to objectors - The issues raised have been covered in the above report and by the Applicant's response to the representations received to the application.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered -
Plans
Location plan 2459-MA-01-ZZ-DR-A-PL01 rev 2
Proposed site and deck plans 2459-MA-01-ZZ-DR-A-PL03 rev 3
Proposed elevations 2459-MA-01-ZZ-DR-A--EL02 rev 3
Proposed site cross section 2459-MA-01-XX-SE-A-SE01 rev1
'3D' visualisation plan 2459-MA-01-ZZ-DR-A-VS01 rev 2/Vs02 rev 1/Vs03 rev 1
Cycle provision plan 2459-MA-01-ZZ-DR-A-PL04
Proposed lighting and emergency lighting
MCIP003-CLH-BRL-WFDRD-DR-E-LIG-0014 and 0015 rev P01
Extension to stair 1 - MICP003-BRN-BRL-WFDPR-DR-Y-STR-0100 P1
Existing elevation 2459-MA-01-ZZ-DR-A-EL01 rev 2
Existing site and deck plans 2459-MA-01-ZZ-DR-A-PL02 rev 2
Tree Protection Plan 4464.02 Rev A
Rhododendron Distribution plan by Dunelm ecology Figure 1
Landscape proposal by tba 4464.05 rev A

Reports

Noise Impact Assessment ref AC108131-1RO dated 19/3/20
Air Quality Assessment ref AQ108136 dated 27/3/20
Tree Survey report by tba Feb 2020 ref PD/4464/TSR/Feb20
Arboricultural Impact Assessment and Method Statement Revision A by tba
Revised April 2020
Ecological Assessment by Dunelm ecology dated June 2020
Landscape management plan Rev A by tba Revised April 2020
Transport Statement by SCP Feb 20 ref MC/200063/TS/0
Lighting Design Philosophy by Claddagh Electrical Ltd

and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of

design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence until a drainage scheme has been submitted to and approved in writing by the Local Planning Authority. The drainage scheme must include:
 - (i) An investigation of the hierarchy of drainage options in the National Planning Practice Guidance (or any subsequent amendment thereof). This investigation shall include evidence of an assessment of ground conditions and the potential for infiltration of surface water;
 - (ii) A restricted rate of discharge of surface water agreed with the local planning authority (if it is agreed that infiltration is discounted by the investigations); and
 - (iii) A timetable for its implementation.The approved scheme shall also be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards.

The development hereby permitted shall be carried out only in accordance with the approved drainage scheme and prior to the first use of the development hereby approved.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
4. Following commencement of the development hereby approved, details and a scheme for the provision of EV Charging Points/Charging Parking Bays shall be submitted to and approved by the Local Planning Authority. The approved details/scheme only shall thereafter be implemented prior to the first use of the development hereby approved and thereafter maintained.

Reason. Information not submitted at application stage and in accordance with the principles of the NPPF, to encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life.
5. Following commencement of the development hereby approved, details of a temporary cycle storage provision and scheme shall be submitted to and approved by the Local Planning Authority. The approved temporary provision shall thereafter be implemented and made available for use prior to the first use of the development hereby approved.

Reason. To secure satisfactory cycle facilities on site and in accordance with Unitary Development Policies HT6/1 - Pedestrians and Cycle Movement, HT3/4 - Scheme to Assist Metrolink and HT4 - New Development.
6. The development hereby approved shall be carried out in accordance with the submitted landscape proposals by TBA landscape architects plan ref 4465.05 Rev A. The approved scheme shall be implemented not later than 12 months from the first use of the development hereby approved or within the first available tree planting season, and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.
7. The development hereby approved shall be carried out in accordance with the recommendation in Section 4 of the Ecological Assessment dated June 202 by

Dunelm Ecology and to provide for 3 Schwegler bat boxes to be installed on retained mature trees which shall be provided prior to the first use of the development hereby approved and thereafter maintained.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

8. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
9. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
10. Prior to any development a survey for invasive plant species including japanese knotweed, rhododendron and himalayan balsam will occur and the finding supplied to and agreed in writing to the Local Planning Authority. If any invasive species are present a method statement detailing avoidance, control and eradication measures should also be supplied to and approved by the Local Planning Authority prior to any earthworks.
Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
11. The development hereby approved shall be constructed in accordance with the design and materials used for the existing first floor deck, and as proposed on the '3D' Visualisation plans 2460-MA-01-ZZ-DR-A-VS01 rev 2 /VS02 rev 1/Vs03 Rev 1.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.
12. The lighting columns to the upper deck hereby approved shall be switched off half an hour after the last Metrolink service to the Whitefield Metrolink station and remain off until half an hour before the first service to the Whitefield Metrolink station daily.
Reason. To protect the amenity of the nearby residential occupiers pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and EN7 - Pollution Control.
13. Notwithstanding the approved plan - Tree Protection Plan ref 4464.02A - the tree identified as 5T shall be retained and shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ

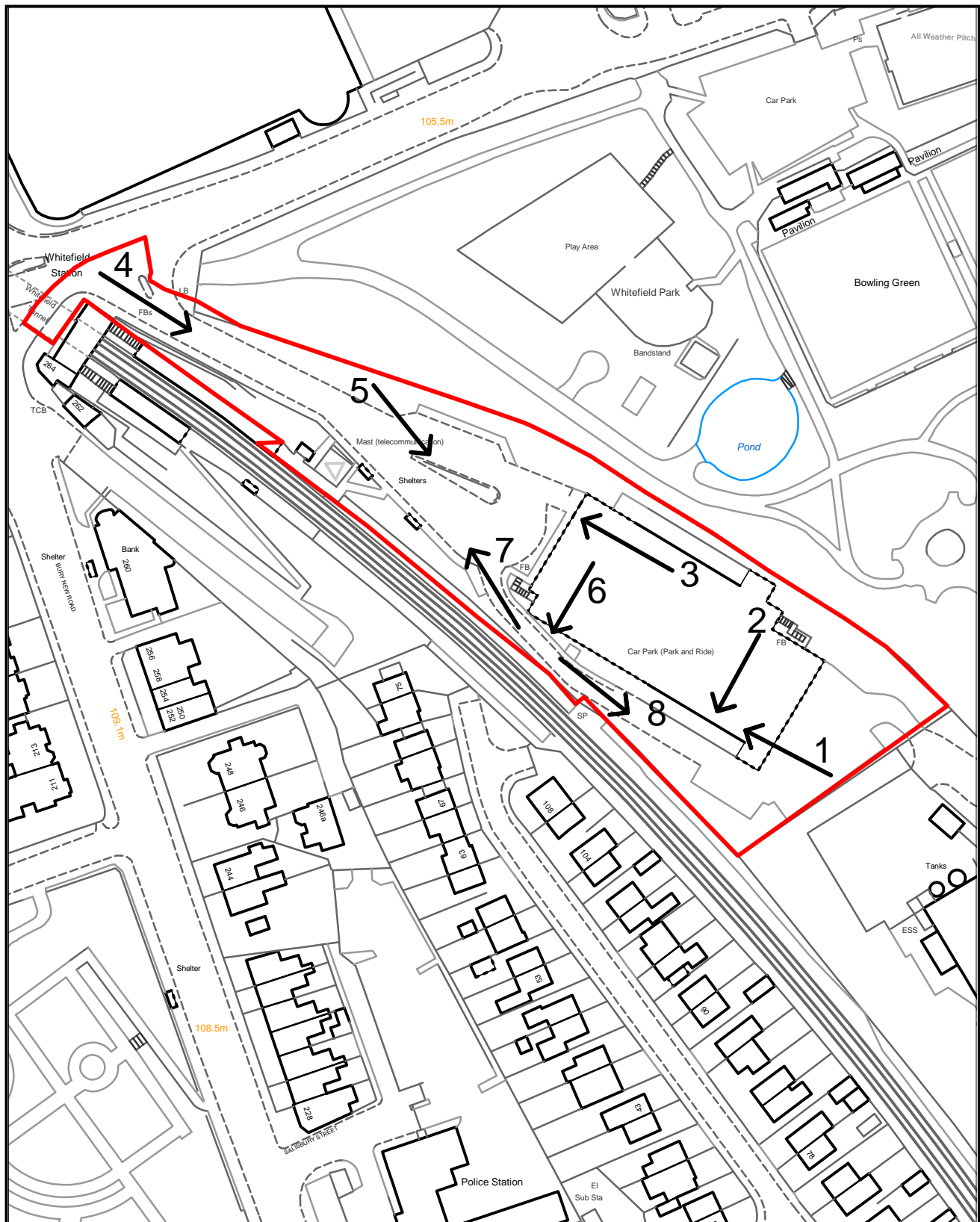
until the development has been completed.

Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

14. No development shall commence unless and until details of a scheme for temporary alternative provision for parking has been submitted to and approved by the Local Planning Authority. The scheme shall include a timetable for implementation, details of any works required to provide the temporary car park and any re-instatement works following it ceasing operation. The approved scheme only shall thereafter be implemented and to the approved timetable.
Reason. To ensure there is adequate and accessible replacement car parking provision for users of the Metrolink whilst works are carried out and completed at the Metrolink car park, pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design, HT2/6 - Replacement Car Parking and HT3/4 - Schemes to Assist Metrolink.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65465

**ADDRESS: Whitefield Metrolink
Stanley Road**

Planning, Environmental and Regulatory Services

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Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6



Photo 7



Photo 8



Location Plan 1250

1 : 1250

Drawing Information:

All drawings and dimensions must be checked against the master plan prior to construction of works. Any alterations must be referred to Millson Group Ltd. All information herein is the property of Millson Group Ltd. Copying or full or part is forbidden without written permission from Millson Group Ltd.



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Location Plan 500

1 : 500

Client:



Drawing:

Location Plan

Drawing Number:

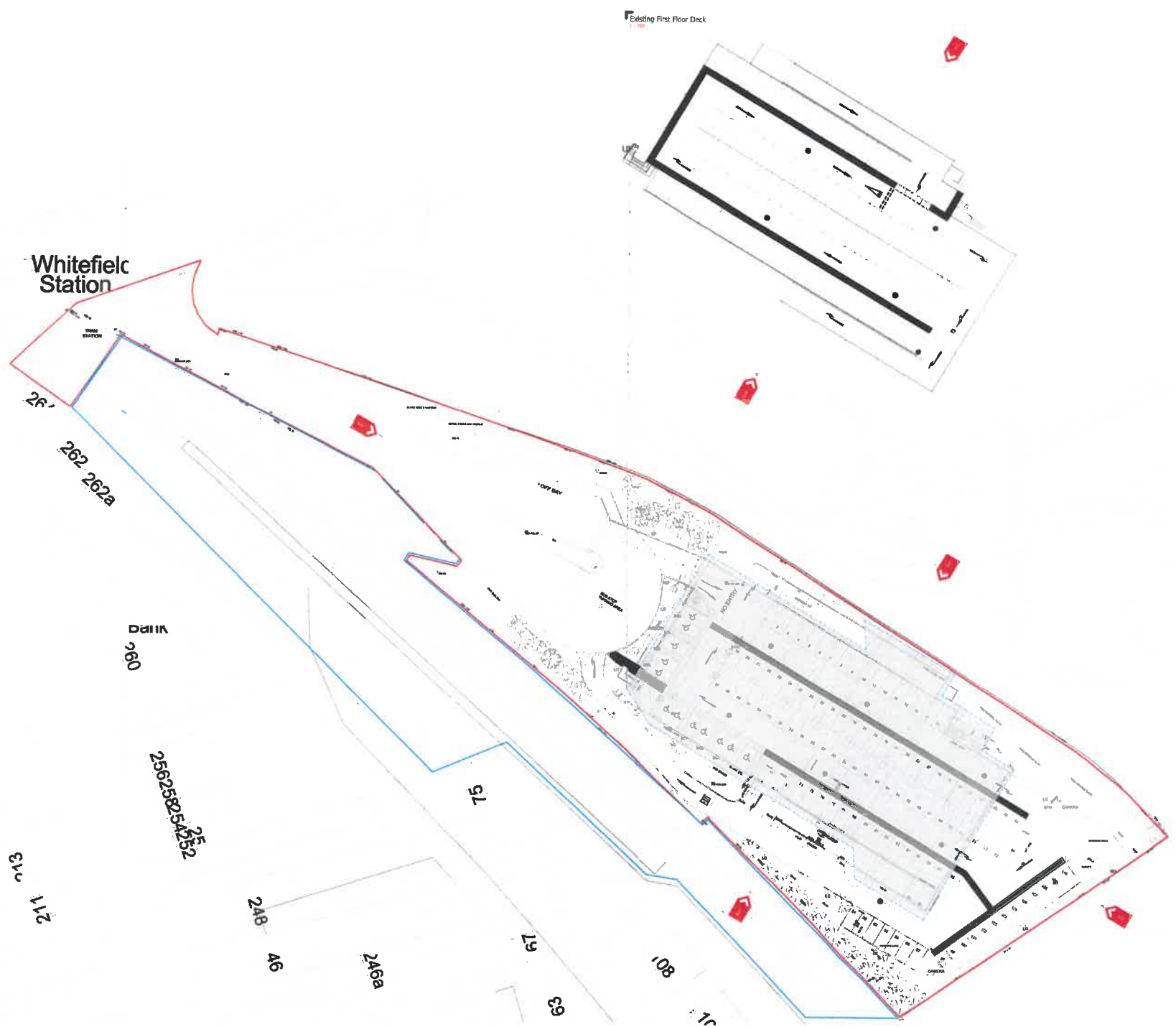
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Scale:	Size:	Date:
As indicated	A3	15/01/20
Drawn:	Checked:	Rev:
SD		2
Status:	Planning	



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2459 - Whitefield P&R Extension



Existing First Floor Deck
1:250

Project Information

Project Name: [Redacted]

Client: [Redacted]

Project Number: [Redacted]

Project Location: [Redacted]

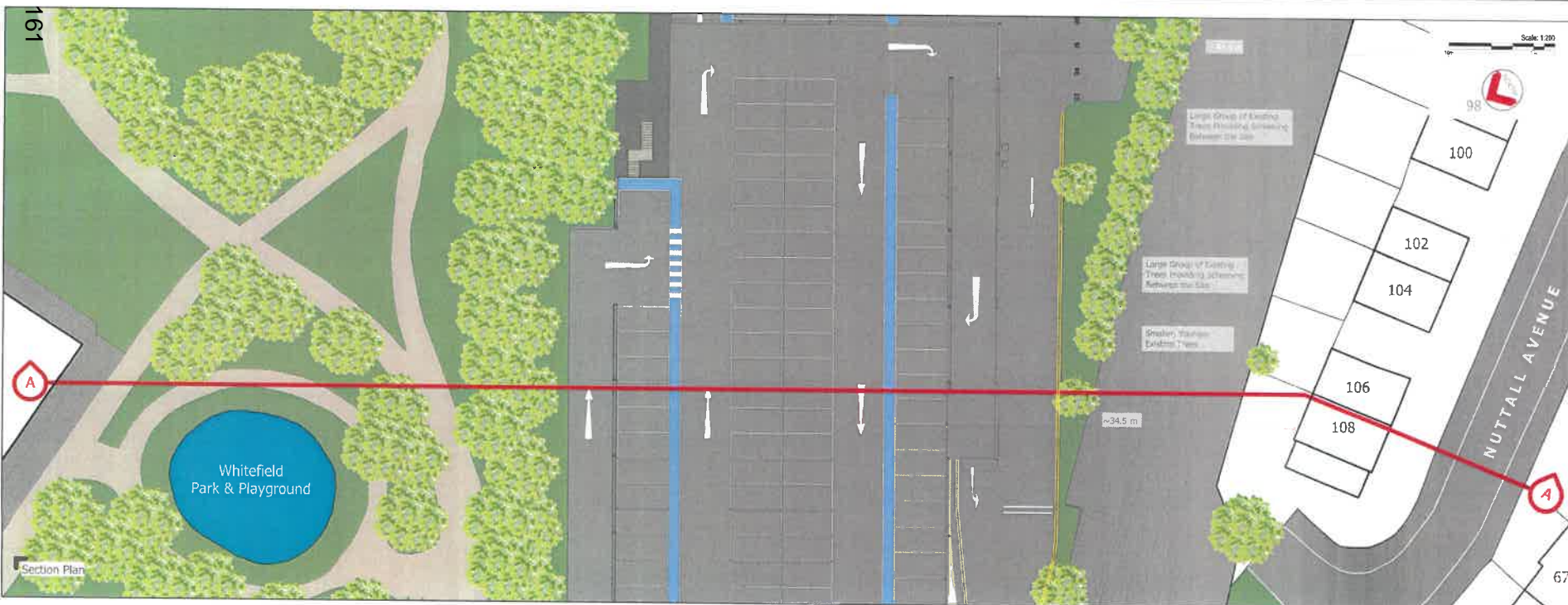
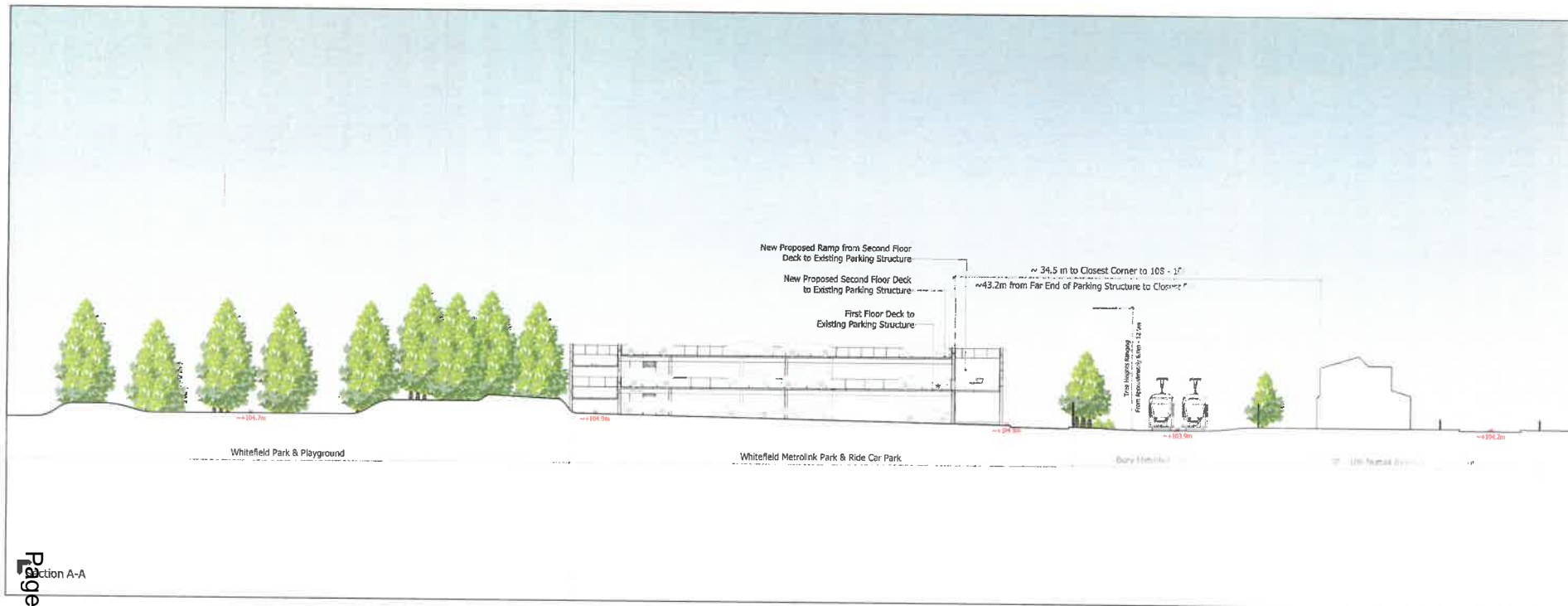
Project Status: [Redacted]



Milton Group

2

2023.000000000000



Millison Group
 24/0 - Whitefield Park & Ride Extension
 1:200 - Section A-A

Transport for Greater Manchester

Project: Whitefield Site Cross Section

Scale: 1:200

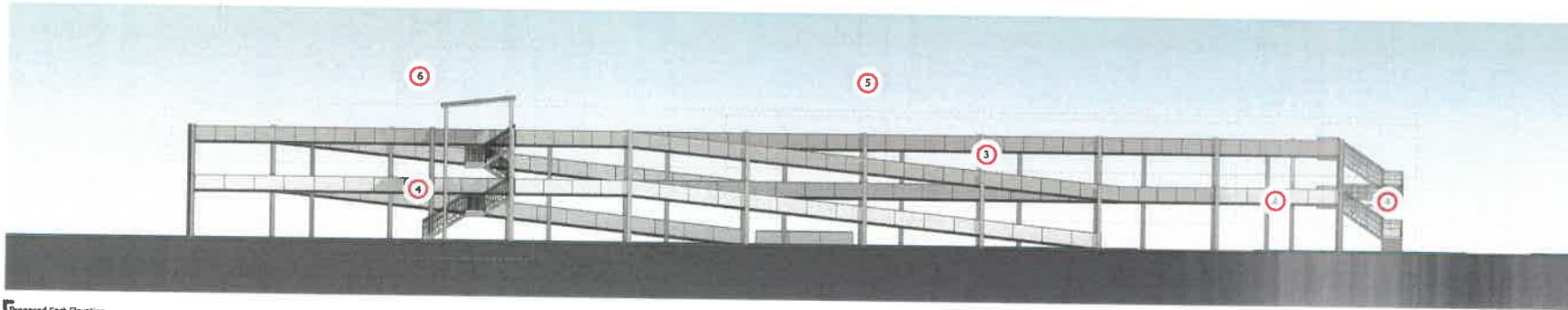
Date: 13/06/20

Sheet: 1

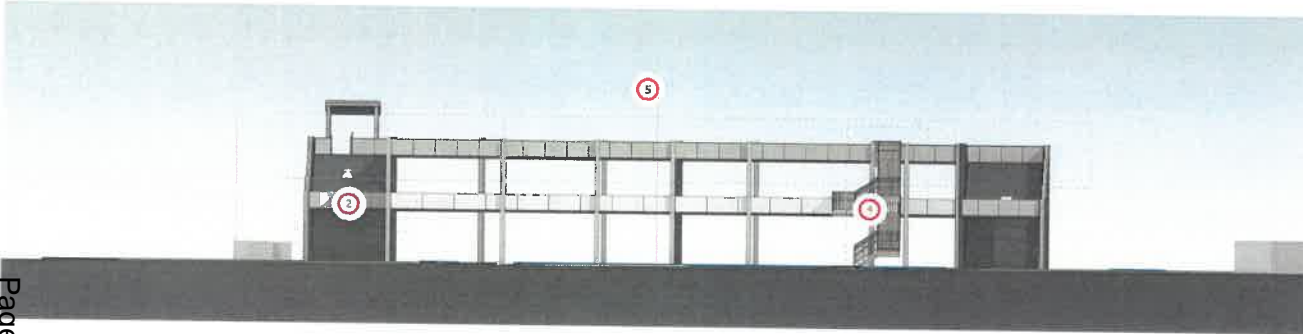
Millison GROUP

24/0 - Whitefield Park & Ride Extension

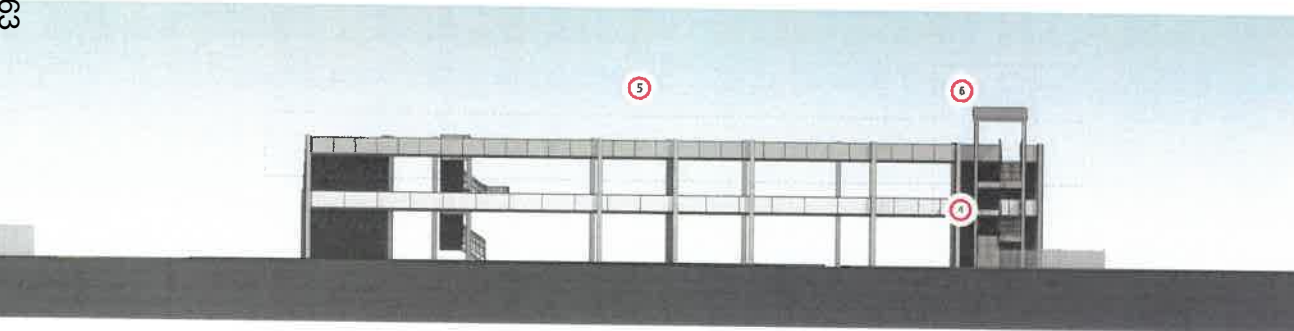




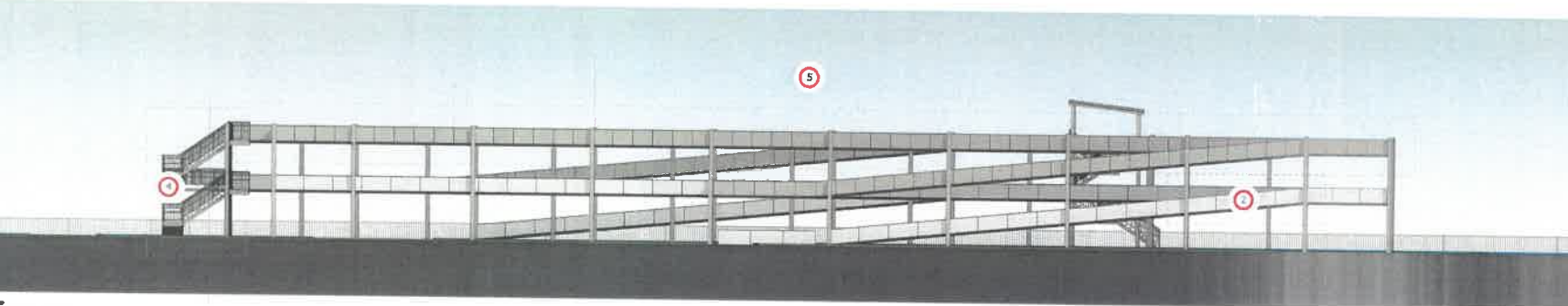
Proposed East Elevation
1 : 100



Proposed North Elevation
1 : 100



Proposed South Elevation
1 : 100

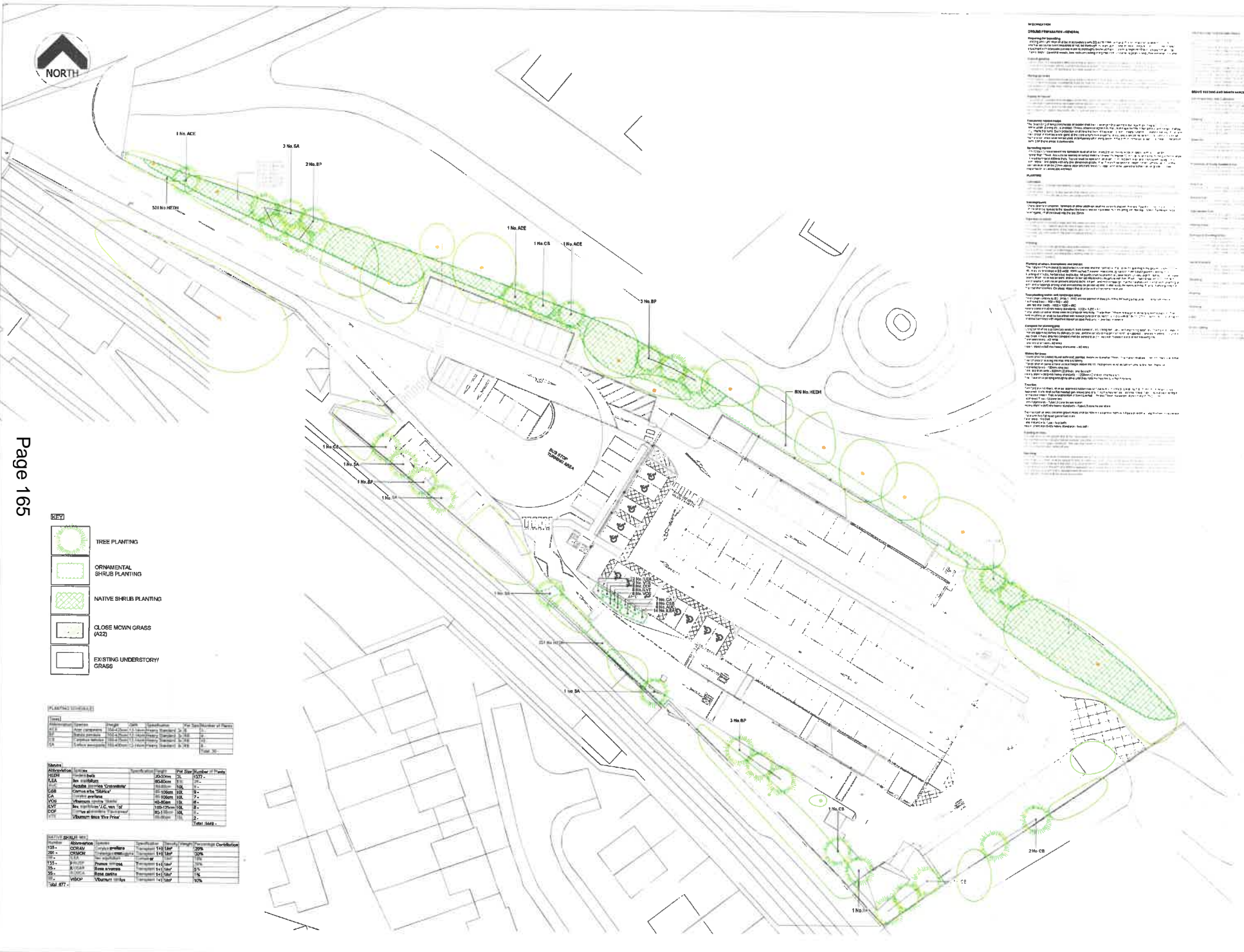


Proposed West Elevation
1 : 100

Project Information	
Project Name	San Jose Convention Center
Location	San Jose, Costa Rica
Client	San Jose Convention Center Authority
Architect	Millson Group
Engineer	Millson Group
Interior Designer	Millson Group
Exterior Designer	Millson Group
Structural Engineer	Millson Group
MEP Engineer	Millson Group
Cost Estimator	Millson Group
Construction Manager	Millson Group
General Contractor	Millson Group
Construction Start Date	2018
Construction End Date	2020
Project Status	Completed



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KEY

ORNAMENTAL SHRUB PLANTING

NATIVE SHRUB PLANTING

CLOSE MOWN GRASS (AG2)

EXISTING UNDERSTOREY GRASS

PLANTING SCHEDULE

Item No.	Species	Height	Color	Size	Per	Number of Plants
131	Acacia saligna	10m	Green	100mm	10	10
132	Acacia saligna	10m	Green	100mm	10	10
133	Acacia saligna	10m	Green	100mm	10	10
134	Acacia saligna	10m	Green	100mm	10	10
135	Acacia saligna	10m	Green	100mm	10	10
136	Acacia saligna	10m	Green	100mm	10	10
137	Acacia saligna	10m	Green	100mm	10	10
138	Acacia saligna	10m	Green	100mm	10	10
139	Acacia saligna	10m	Green	100mm	10	10
140	Acacia saligna	10m	Green	100mm	10	10
141	Acacia saligna	10m	Green	100mm	10	10
142	Acacia saligna	10m	Green	100mm	10	10
143	Acacia saligna	10m	Green	100mm	10	10
144	Acacia saligna	10m	Green	100mm	10	10
145	Acacia saligna	10m	Green	100mm	10	10
146	Acacia saligna	10m	Green	100mm	10	10
147	Acacia saligna	10m	Green	100mm	10	10
148	Acacia saligna	10m	Green	100mm	10	10
149	Acacia saligna	10m	Green	100mm	10	10
150	Acacia saligna	10m	Green	100mm	10	10
151	Acacia saligna	10m	Green	100mm	10	10
152	Acacia saligna	10m	Green	100mm	10	10
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154	Acacia saligna	10m	Green	100mm	10	10
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159	Acacia saligna	10m	Green	100mm	10	10
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185	Acacia saligna	10m	Green	100mm	10	10
186	Acacia saligna	10m	Green	100mm	10	10
187	Acacia saligna	1				

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Ward: Bury East - Redvales

Item 08

Applicant: Mr Philips

Location: Land adjacent to 23 Meadway, Bury, BL9 9TY

Proposal: Reserved matters for layout, scale, appearance and landscaping approval following grant of Outline approval ref. 61369 for 2 no. detached dwellings

Application Ref: 65469/Reserved matters

Target Date: 29/06/2020

Recommendation: Approve with Conditions

Description

The application site relates to a well-maintained piece of grassed open land which is situated within a cul de sac of a residential development comprising 5 detached properties. The eastern part of the site is identified as being located in the River Valley (UDP Policy OL5/2) and Wildlife Corridor (UDP Policy EN6/4) and shares the boundary with Roch Valley Greenway which is Protected Recreation under UDP Policy RT1/1.

To the east of the site is a raised bank along which is a line of trees and vegetation and beyond which is the River Roch. To the south and west, the site is partly bounded by trees and a row of leylandi. There is a public sewer which runs through the site from north to south. To the north is the boundary with No 23 Meadway and their detached garage and driveway with the other houses on the cul-de-sac located to the west of the application site in a horseshoe formation and comprise large detached properties with open frontages and gardens. The cul-de-sac is unadopted and is accessed via single lane which leads off the main part of Meadway which serves the other residential properties on this road.

Outline planning consent was granted 24/5/2017 for a residential development for 2 no. detached dwellings and included the means of access to the site.

This application seeks reserved matters consent for the details comprising layout, scale, appearance and landscaping.

The site, whilst broadly rectangular, has an irregular boundary to the north. That said, the proposed dwellings would be located more or less centrally within the site area rotated to front the cul de sac. Either side of the dwellings would be a detached double garage, set in an angled position and separated from the houses by a flagged footpath which would lead to the rear of the houses and the private garden space. The rear boundary would be defined by a 1.8m high timber boarded fence which would continue to run between the two properties, ending at the front elevation of the houses. The frontages would remain open and grassed.

It is proposed to remove 4 trees in total, 2 at the front and 2 at the rear with mitigation re-plants and landscaping forming part of the proposal.

Access to each property would be formed off Meadway onto driveways in front of the garages where there would be parking for two cars with space for visitors. It is also proposed to provide a passing place on the cul-de-sac formed from part of the southern boundary of the site. The passing place was established and approved in the outline planning consent.

The dwellings themselves would be 2 storey brick builds, relatively modest in design with

pitched roofs and symmetrically positioned windows and a piked roof projection at the front.

Internally, accommodation would comprise living space at ground floor and four bedrooms at the first floor level.

Relevant Planning History

01872/E - Pre-application enquiry for proposed no.2 detached houses & no.2 detached double garages - Enquiry completed 18/08/2016

60786 - Outline application for residential development of 2 no. detached dwellings with details of access (all other matters reserved) - Withdrawn by Applicant 15/02/2017

61369 - Outline application for 2 no. detached dwellings with detached double garages with details of access. - Approve with Conditions 24/05/2017

Publicity

Letters sent to 15 properties on 12/5/20 to Nos 3-21 (odds) and 38-48 (evens) Meadway.
Site notice posted 27/5/20

3 individual objections received from Nos 17, 44, 46 Meadway with the following issues:

- Increase in traffic that building 2 houses will create.
- The entrance is already narrow and this would cause an additional hazard.
- The amount of greenery at the access road restricts visibility
- Increase in cars will be hazardous to pedestrians.
- To build more than 5 houses would create a fire hazard along with many other dangers.
- Serious negative impact on residents at nos 15-21 Meadway and local people who regularly use the adjacent cycle path and wildlife corridor.
- Flood plain - The area was seriously flooded in December 2015 - any development could place any new properties at serious risk of flooding and the existing properties on Meadway.
- In addition, 2 further properties will put added pressure on the sewer system which also exacerbates the flooding issue.
- A Strategic Flood Risk assessment should be undertaken.
- A main sewer crosses the site and there would need to be at least a 5m easement either side pushing the houses further back than on the proposed plan, closer to the floodplain and land not in the applicants ownership. When the original 5 houses were built, the builder was refused permission to build on this land because of the main sewer pipeline.
- The Land Registry document states the easement should be left open and unbuilt on
- There is the potential impact of piling on sewers which could cause substantial damage.
- Due to issue with safety, 4 street lights were erected on this land in agreement with all 5 home owners at Nos 15-23 Meadway. Expert advice was sought regarding their positioning; changing this to accommodate a 'passing place' could compromise the safety benefits of the lighting.
- Cramming and garden grabbing - The development does not integrate with the area in terms of scale, density, layout and access. The area around the houses would be considerably less than the existing houses. Over development of the land.
- The design and appearance by virtue of size and scale is neither appropriate or sympathetic to the character of the street scene.
- Velux roof lights could cause privacy issues.
- Coal Authority - The area is a High Risk referral area - a mining report is recommended.
- Wildlife - additional land from the River Valley and Wildlife Corridor would be needed for the rear gardens and trees would need to be removed from this land which is not in the applicant's ownership. Serious impact on wildlife and natural habitat in an area where there is strong evidence of nesting wild birds and other wildlife.
- The driveway of one of the houses would lead directly to the blind spot. The proposed passing point would not alleviate this issue.
- Two additional houses would increase traffic capacity by 40%.
- There is the potential for each property to have 7 bedrooms if the roof space were used - more people and more cars.

- There is no pavement for safe pedestrian access.
- There is no on street parking which is why the houses were designed with large driveways for occupants and visitors.
- Government and Bury Council are tasked to improve cycle and walking to work in response to COVID 19 - this proposal would have an adverse effect on this ambition.

Letter received from a legal representation acting on behalf of Nos 15, 17, 19 and 21 Meadway which raises the following issues:

- Scale and appearance - occupy a significantly larger area - height and width especially - than the neighbouring properties; and are not in keeping in both scale and appearance, impact on the density and visual appearance of the cul de sac as a whole, detrimental effect on the outlook from neighbouring properties, The fabric and materials of the builds would not be in keeping, fences at the front - no other properties are allowed;
- Highways - the drive of the southerly property would reduce visibility for vehicles driving along the access to the neighbouring properties, parking of the cars on the driveway will restrict view to road users, 40% increase in traffic, environmental implications of traffic increase;
- Landscaping - the development sits on part of a flood plain of the River Roch - the site was severely flooded in Dec 2015 and further development could increase further serious floods;
- Capacity of Physical infrastructure - The plans would encroach over a sewer owned by UU. The neighbouring properties all required piling to a depth of 8m. Risk to the damage of the development on utilities.

Revised plans have been received seeking to address some of the concerns raised above and included amendments to the house types and floor plans, removal of rooms in the roof space and lowering of ridge height, detached garages, removal of front boundary fence, change to materials to brick.

Those who objected were notified of the revision by letter dated 22/6/20.
No further objections or comments have been received on the revised scheme.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection subject to conditions.

Borough Engineer - Drainage Section - No response received.

Environmental Health Contaminated Land - No objection subject to conditions.

Waste Management - No response received.

Environment Agency - No response received.

United Utilities (Water and Waste) - No objection. Commented previously on the outline approval - Drainage condition recommended on the outline consent.

The Coal Authority - No objection.

Greater Manchester Ecology Unit - No objection subject to conditions.

Public Rights of Way Officer - No issues

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/6	Garden and Backland Development
EN1/2	Townscape and Built Design
EN6/4	Wildlife Links and Corridors
EN5/1	New Development and Flood Risk

OL5/2	Development in River Valleys
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
RT1/1	Protection of Recreation Provision in the Urban Area
SPD11	Parking Standards in Bury
SPD6	Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Housing Policy Principle - The National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Bury's Strategic Housing Land Availability Assessment sets out the latest housing supply position, which is made up with sites that have an extant planning permission and sites that have potential to obtain planning permission in the future. This shows that there are a number of sites within the Borough with the potential to deliver a significant amount of housing. However, not all of these sites will contribute to the five year supply calculations as many sites will take longer than five years to come forward and be fully developed (e.g. some large sites could take up to ten years to be completed). As such, latest monitoring indicates that the Council is unable to demonstrate a five year supply of deliverable housing land and this needs to be treated as a material factor when determining applications for residential developments.

UDP Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

The site is within an urban and sustainable location and is adequately served by existing infrastructure. It would be located within an established residential development and would not conflict with the local environment in terms of the character and surrounding land uses. As such, the principle is in general accordance with national planning policy and would help to contribute to meeting local housing targets and would be in compliance with the NPPF and UDP Policies H1/2, H2/1 and H2/2.

The principle of a residential development for 2 dwellings has been established with the

grant of the outline consent, which also included details of access.

Environment Policy Principle - The proposal is partly within the River Valley (UDP Policy OL5/2) and Wildlife Corridor (UDP Policy EN6/4) and shares a boundary with Roch Valley Greenway which is Protected Recreation under policy RT1/1.

These issues were assessed and accepted under the Outline application. To summarise -

In terms of Policy OL5/2, it was considered the development

- would not divide the valley into sections or obstruct access through the valley.
- would potentially meet the criterion related to limited infilling in an established settlement.

Policy EN6/4 states that where development is accepted, it should contribute to their effectiveness through design, landscaping and siting and mitigation. Only a small part of the wildlife corridor would be affected by the development. The outline permission included a condition that details of boundary treatment and landscaping including measures to mitigate for any loss of habitats be submitted at reserved matters stage. A landscaping scheme including boundary treatment has been submitted with this application and GMEU have advised that the proposed mitigation would be acceptable (see Ecology section below).

As such, it is considered that the proposed development would not have a detrimental impact on the River Valley or Wildlife Corridor and that a suitably worded condition would ensure that appropriate boundary treatment and mitigation measures would be provided.

As Protected Recreation it was considered that the site did not fulfill a recreational function and as such considered that the development would not conflict with this policy.

Layout and siting - The existing houses laid out are in sizeable plots and the density of existing built development to available land area is low. The development would utilise an unused and available piece of land which currently serves no particular function.

The proposed layout would closely reflect the existing arrangements in the cul de sac by proposing to set the houses back into the site, a minimum of 11m and retaining open frontages and lawned gardens. The siting of the proposed dwellings would also provide the easement required by United Utilities of 3m between the footprint of the builds and the public sewer which runs across the length of the site.

The new dwellings would be set adjacent to each other with a detached double garage located at slight angles at either side accessed by a double driveway to provide parking for the occupiers and visitors.

Around the properties would be a paved footpath and private amenity space and garden areas at the rear, a minimum of 8m depth, which would be separated by a fence running between the two properties to the rear boundary. This fence would also run along the eastern boundary of the site to the land beyond.

The plot is a considerable size and the scale and siting of the proposed dwellings and associated garages, parking and amenity areas would be suitably accommodated within the available area, whilst also paying due regard to the character and layout of the existing houses by replicating the spacious open frontages.

It is therefore considered that the proposed layout would be in keeping with the composition and configuration of the existing houses which typifies the cul de sac and as such would comply with UDP Policies H2/1, H2/2, H2/6 and EN1/2.

Scale, design and appearance - Revised plans have been received which have amended the design and scale of the scheme with regards to height, footprint, fenestration and

materials.

The scheme now proposes 'true' 2 storey dwellings, with accommodation provided over the ground and first floors. By separating the garage from the properties and reducing its height to single storey, the proposed development would better relate to the scale and massing of the existing houses on the cul de sac.

The properties would present a mix of modern and traditional elements, modest in design with a pitched roof and brick facades and incorporating detailing with a piked front projection either side of each dwelling and a smaller pitched roof over the first floor front window on the opposite side.

Windows would be symmetrically positioned with brick headers and a canopied porch would define the front entrance.

A schedule of details of the materials have been submitted and it is proposed to use a random red brick facade, soldier course brick heads, concrete roof tiles, grey fascias and soffits and charcoal grey block driveway paving.

The design and scale of the proposed dwellings would take its reference from the existing properties on the cul de sac, reflecting the general detailing and design rhythms which characterise the area and as such this approach is considered to be acceptable and in keeping with the established estate.

As such, the proposed development is considered to be acceptable and would comply with UDP Policies H2/1, H2/2 , H2/6 and EN1/2.

Impact on residential amenity - Supplementary Planning Document 6 is used as a guide to assess relationships and aspect standards between properties and new built development to ensure that suitable separation distances are maintained and that a new development would not cause undue harm to adjacent neighbours. A separation of 20m between habitable room windows is advised.

The houses on the cul de sac are relatively spaced out and have generous front lawns and wide driveways. The proposed dwellings would likewise be set back into the site and as such the distance to the nearest property would be more than 20m away, comfortably satisfying policy guidance and SPD6.

One of the concerns raised by residents relates to access and the additional traffic the development would generate and safety implications particularly as there is a sharp bend into the cul de sac. The access into the site already serves 5 dwellings and it is considered that the additional 2 properties would not introduce significantly more vehicles or traffic and additionally, it is unlikely that cars would be leaving and entering the site at the same time as the other properties, and like all residential areas trips would be staggered. In addition, the applicant proposes to provide a passing place on the approach to/from the access and as such the cul de sac arrangement would be bettered.

It is therefore considered that the proposed development would not have an adverse impact on the residential amenity of the existing occupiers or those future occupiers of the new houses and as such the development would comply with UDP Policies H1/2, H2/1, H2/2 and H2/6.

Access and parking - The principle of the access was established by the grant of the outline consent.

The cul de sac is unadopted but well maintained and tarmaced. It is a single width and short stretch of road and there is a sharp bend into the cul de sac from the main part of Meadway.

The application proposes to provide a passing place located at the right hand side of the bend on entering the cul de sac which would improve visibility into and out of the road and reduce conflict between car users and pedestrians emerging onto Meadway at this point. This would bring a benefit to both the future occupiers of the dwellings and the existing residents.

At the outline stage, it was considered that the development for 2 additional dwellings would not significantly add to the volume of traffic to the extent to cause highway safety concerns, and the works proposed to introduce a passing place would improve the existing access arrangements. This similarly remains the case for the reserved matters application.

Conditions were recommended by the Highway Section on the outline consent, including the submission of details for the passing place and a construction traffic management plan, and these would be required to be fulfilled as part of the approval of the development.

In terms of parking, the development proposes a double detached garage and driveways which would provide for 2 spaces minimum and up to 4 spaces maximum. SPD11 advises that for 4 bed properties, 3 parking spaces would be required and as such the proposed development would comply with this policy guidance.

Subject to the conditions on the outline approval, the Highway's Section have raised no objection to the application and as such, the proposal is considered to be acceptable and would comply with UDP Policies H2/2 - The Layout of New Residential Development, H2/6 - Garden and Backland Development, EN1/2 - Townscape and Built Design, HT2/4 - Car Parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.

Public Right of Way - There is a public footpath next to the access to the site which follows the River Roch. The proposed development would not encroach onto the footpath, but the Applicant should be aware that should the Right of Way be affected, an appropriate closure or diversion order would be required.

Air quality - The site is not located within an Air Quality Management Area. Due to the scale of the development it is considered unlikely that the development would create an increase of more than 500 AADT (annual average daily traffic). Therefore, in line with the EPUK Guidance, an air quality assessment will not be required.

However, Bury Council has been identified by DEFRA as an area requiring to significantly improve air quality. The Government report, 'UK Plan for Tackling Roadside NO2', published in July 2017, identified Bury Council as an area having one or more roads with concentrations of NO2 above statutory limits, according to more recent modelling results. Bury is therefore required to address these exceedances and we are currently engaging with Government on our proposed plans to reduce NO2 concentrations across the borough.

Due to this requirement and in line with the principles of Good Practice set out in the EPUK Guidance, the EH Section recommends a condition to be placed on any planning permission granted for the provision of an electric vehicle (EV) charging points for each residential unit. EV charge points shall be chosen from the Electric Vehicle Homecharge Scheme approved chargepoint model list.

Given the scale of the development, it is considered reasonable this be recommended as an informative to the applicant.

Ecology - GMEU have been consulted on the proposed development and comment as follows:

Summary - Ecological issues were resolved and conditioned as part of the outline permission. The only issue that requires additional information at reserved matters is ecological mitigation.

Protected Species - No protected species issues were identified at outline stage. Whilst the original survey is now no longer valid, it is very unlikely the site will have been colonised by any such species. No further information or measures are required.

Nesting Birds - This issue is covered by condition 7 of the outline permission. No further information or measure are required at this time.

Invasive species - Japanese knotweed was identified adjacent to the development site in 2017. This is likely still present and may have expanded its spread. Himalayan balsam may also now be present as it expands its range rapidly and occurs along the river Roch. GMEU recommend the condition applied at outline (condition 6) is updated to provide an updated invasive species survey prior to any earthworks and for any such species present a method statement detailing eradication and/or control and/or avoidance should be submitted to and approved in writing by the LPA.

Contributing to and Enhancing the Natural Environment - Section 170 of the NPPF 2018 states that the planning system should contribute to and enhance the natural and local environment.

Condition 13 of the outline application required details of boundary treatment and landscaping including measures to mitigate for any loss of habitats. Trees are indicated as being lost on the landscape plan provided and it is proposed to provide replants to mitigate for this loss. GMEU are satisfied with the provisions of the landscaping plan and recommend the proposals are conditioned.

Flooding - The Environment Agency (EA) were consulted on the Outline planning application and had no formal comments to make. They have been re-notified of this reserved matters application and no further comments have been received.

United Utilities have also been consulted on the Outline and the Reserved Matters applications and a condition was recommended on the Outline Consent requiring a sustainable drainage scheme to be submitted for approval.

The residents have raised the issue of a sewer pipe which runs through the site, requiring an easement either side. United Utilities have also raised this in their response and the applicant has been advised.

The proposed plans demonstrate that the sewer would run across the site, in a north/south direction to the west of the properties and that a 3m easement either side, free of built development, would be provided, and as advised by United Utilities.

For planning purposes, the location of the sewer would be a private matter for the applicant to address and for the relevant consents and permits to be in place from United Utilities for build over agreements or connections to public sewers.

As such, it is considered that the proposed development, with the pre-existing condition for the submission of a drainage scheme would be acceptable and comply with chapter 10 - meeting the challenge of climate change, flooding and coastal change of the NPPF and UDP Policy EN5/1 - New Development and Flood Risk.

Coal Authority - It is noted this is a reserved matters submission pursuant to the outline consent granted under reference 61369. The Coal Authority have commented previously and recommended that relevant conditions were imposed on the grant of planning permission.

Condition 16 of the approved outline application required the recommended intrusive site investigations to be carried out on site.

As the coal mining legacy risks on this site have no implications for the layout of the development, the Coal Authority can confirm there is no objection to the reserved matters submission.

Response to objectors -

- United Utilities consultation response states that 3m either side of the centre line of the sewer in accordance with the minimum distances specified in the current issue of Part H of the Building Regulations, will be required. The plans show that this would be provided. In any event, the Developer would require consent from UU to undergo works which would involve the sewer.
- Potential impacts from pilling are not a material planning consideration and a concern under Building Regulations.
- A Coal mining report was submitted with the outline approval and a condition was recommended and included on the consent for a scheme of intrusive site investigations to be undertaken.
- The Environment Agency were consulted on the outline application and raised no objection. The EA have been consulted on this reserved matters application and no response has been received, thereby concluding that there are no representations for the organisation to make.
- Land Registry documents and any title deeds relating to the site are private matters and not a material planning consideration.
- It is considered that the revised plans received are considered to satisfactorily address the issues of height, scale, materials and boundary treatment raised in the objections above.
- All other issues raised including access and parking are covered in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than the expiration of two years beginning with the date of this permission.
Reason. Required to be imposed pursuant to Section 51 of the Planning and Compulsory Purchase Act 2004
2. This decision relates to drawings - Location plan 16-047 1000 rev E; Proposed site layout 16-047 1103 rev G; Proposed boundary treatment 16-047 1107 rev A; Proposed landscaping layout 16-047 1108 rev D; Proposed streetscene 16-047 1109 rev E; Proposed house type elevations plot 1 16-047 1113; Proposed house type floor plans plot 1 16-047 1114; Proposed house type elevations plot 2 16-047 1115; Proposed house type floor plans plot 2 16-047 1116; New vehicle passing place layout and construction details C-0930-01 - and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Prior to any earthworks an updated invasive species survey will be provided and

for any such species present a method statement detailing eradication and/or control and/or avoidance should be submitted to and approved by the Local Planning Authority. The method statement should include a timetable for implementation.

The development shall thereafter be carried out in accordance with the approved method statement.

Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.

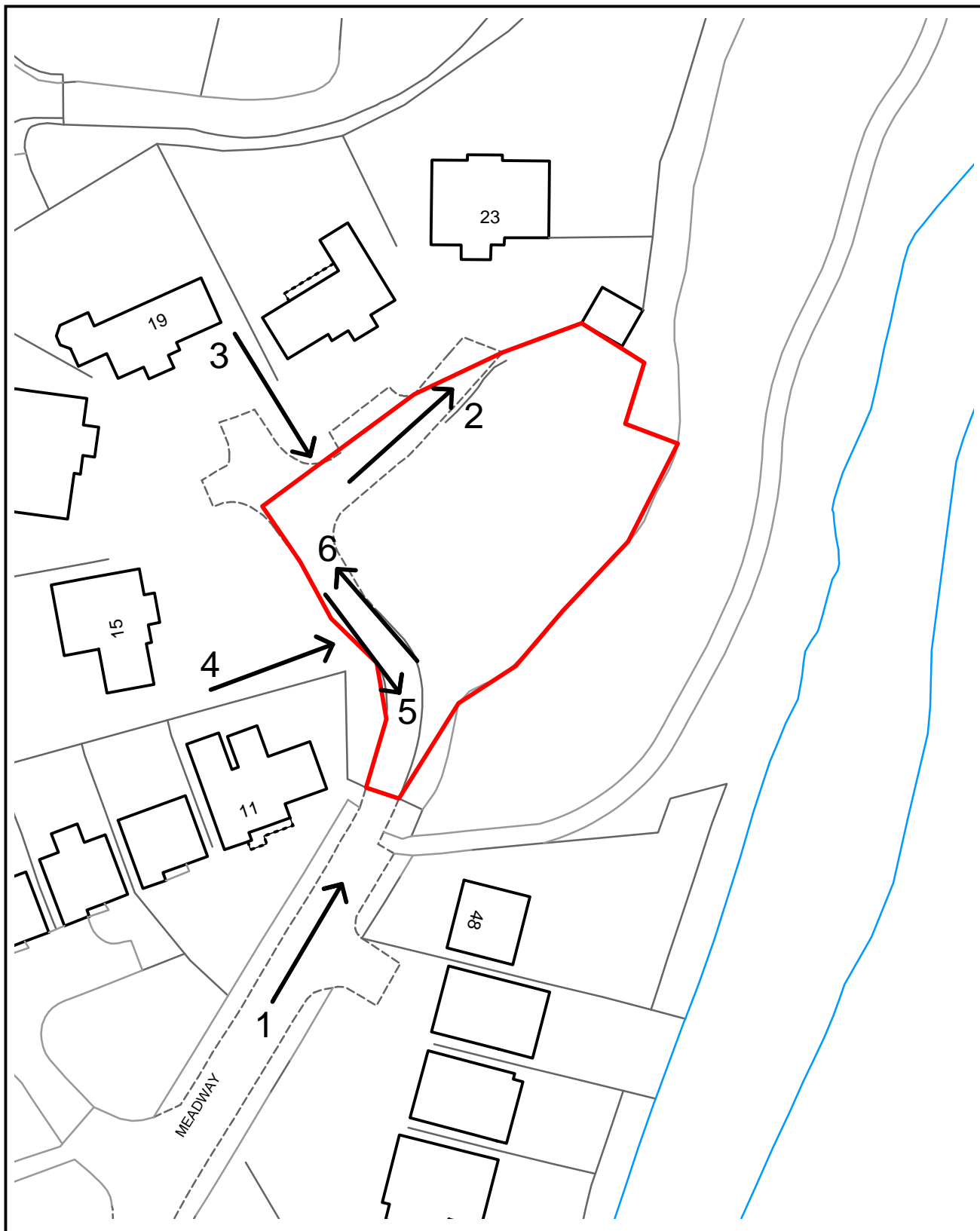
Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

4. The development hereby approved shall be carried out in accordance with the proposed landscaping layout plan ref 1108 Rev D. The approved scheme shall be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree planting season; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.
5. The proposed passing place, driveway positions and parking arrangements shown on the approved plans, along with any highway remedial works on Meadway required as a result of the construction of, and statutory undertakers connections to, the dwellings, shall be implemented to the written satisfaction of the Local Planning Authority prior to the dwellings hereby approved being first occupied and thereafter maintained.
Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design, maintain the integrity of the adopted highway, all in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, HT2/4 - Car Parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.
6. No development shall commence unless and until in-curtilage and refuse collection/bin storage arrangements for use on collection day have been agreed with Waste Management. The facilities subsequently approved shall be provided to the satisfaction of the Local Planning Authority before the development is first occupied and shall subsequently be maintained at all times.
Reason. Information not secured at application stage. To ensure adequate off-highway bin storage facilities are provided within the curtilage of the site in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and New Development
7. The development hereby approved shall be carried out in accordance with the submitted materials schedule - 16-047 1110C. Only the approved materials shall be used for the construction of the development.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policies H2/1 - The Form of New Residential Development, H2/6 - Garden and Backland Development and EN1/2 - Townscape and Built Design.

8. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application. Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65469

ADDRESS: Land adj 23 Meadway
Bury

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

Photo 1



Photo 2



Photo 3



Photo 4

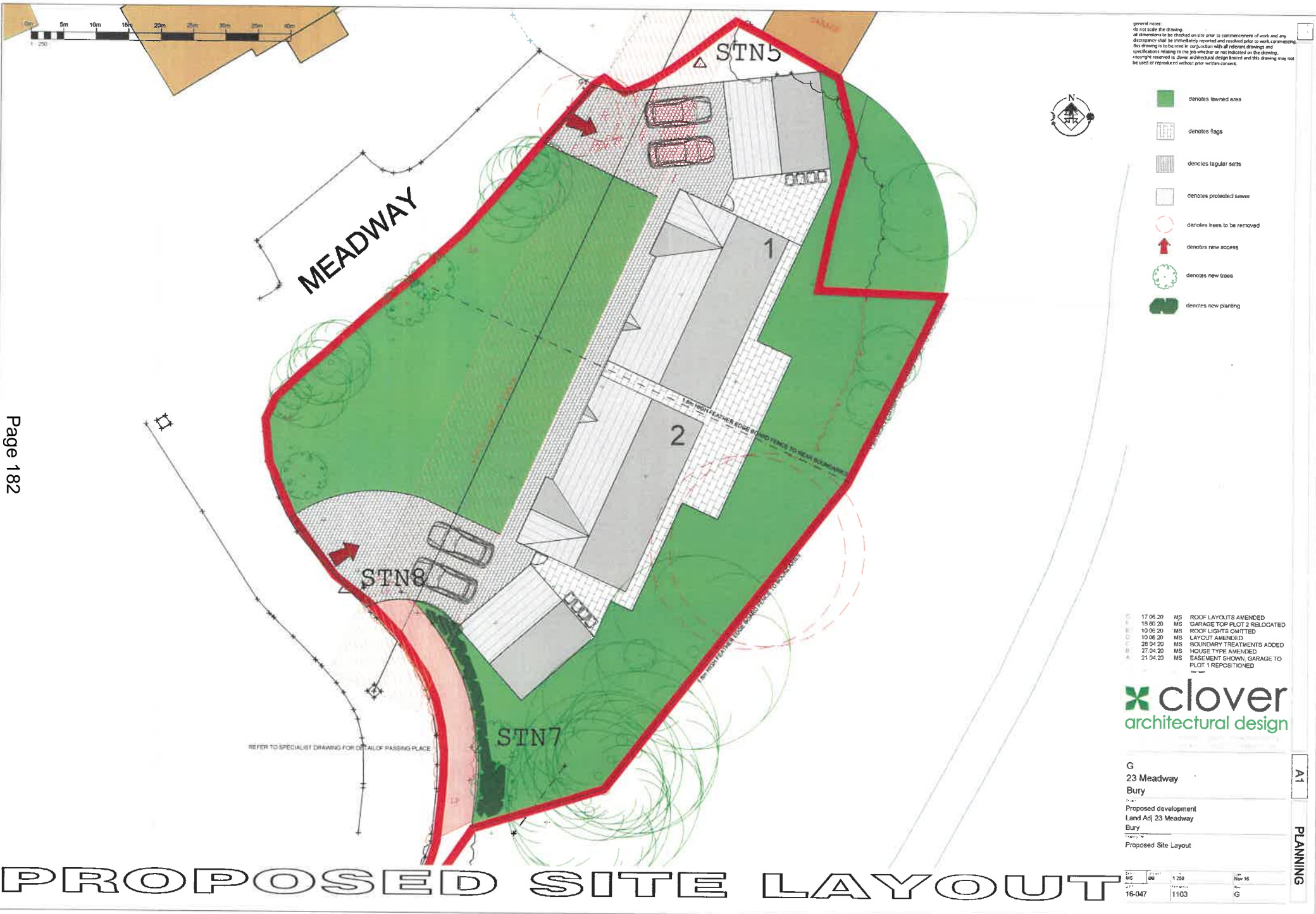


Photo 5

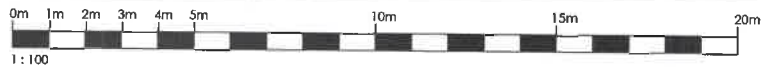


Photo 6









general notes:
do not scale the drawing.
all dimensions to be checked on site prior to commencement of work and any discrepancy shall be immediately reported and resolved prior to work commencing.
this drawing is to be read in conjunction with all relevant drawings and specifications relating to the job whether or not indicated on the drawing.
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PLOT 1

PLOT 2



STREET SCENE

E	17.06.20	MS	PIKE SHOWN ABOVE BEDROOM WINDOW.
D	16.06.20	MS	STONE SILLS AND HEADS OMITTED. SOLDIER COURSE HEADS ADDED.
C	16.06.20	MS	HEIGHT AND ROOF PITCH AMENDED
B	10.06.20	MS	SECOND FLOOR OMITTED
A	10.06.20	MS	UPDATED TO SUIT REVISED LAYOUT, SCALE AMENDED TO 1:100

 **clover**
architectural design

Client:

Mr Graham Phillips
23 Meadway
Bury

Project:

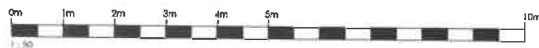
Proposed Housing Development
Land adjacent 23 Meadway
Bury

Document Title:

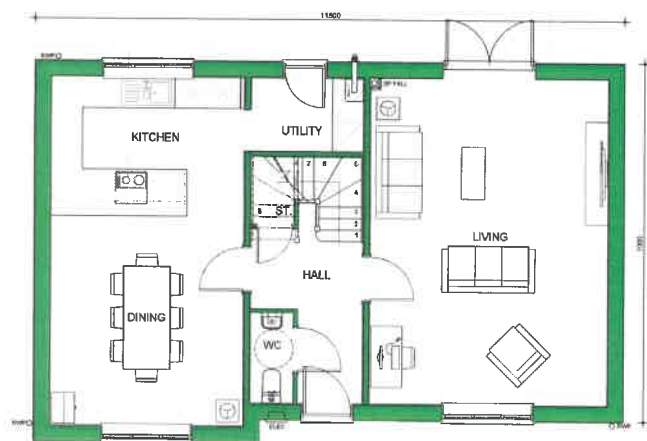
Proposed Street Scene

PROPOSED STREET SCENE

Design: MS	Check: 1:100	Date: June 20
Arch No: 16-047	Drawn by: 1109	Rev: E



general notes:
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GROUND FLOOR 71.4sq m (768 sq ft)



FIRST FLOOR 71.4sq m (768 sq ft)

PROPOSED HOUSE TYPE - PLOT 1

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Mr Graham Phillips
23 Meadway
Bury

Proposed Housing Development
Land adjacent 23 Meadway

Bury

Proposed House Type
Rear Plans

Plot 1

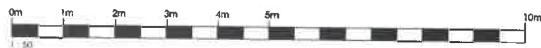
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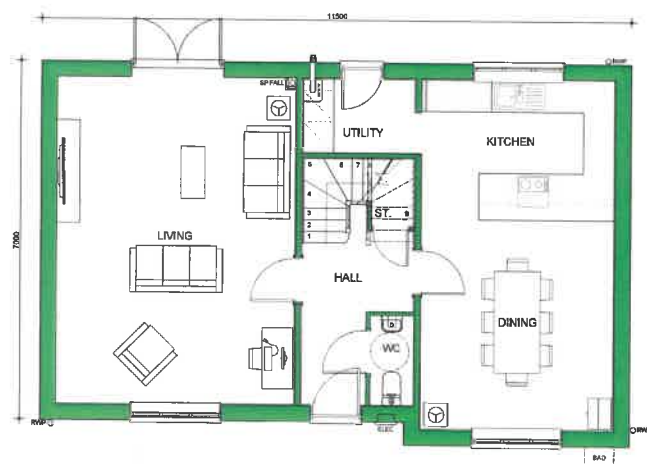
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A1

PLANNING



general notes:
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GROUND FLOOR 71.4sq m (768 sq ft)



FIRST FLOOR 71.4sq m (768 sq ft)

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architectural design

Mr Graham Phillips
23 Meadoway
Bury
Proposed Housing Development
Land adjacent 23 Meadoway
Bury
Proposed House Type
Floor Plans
PLOT 2

11-047 1114
11-047 1114

A1

PLANNING

PROPOSED HOUSE TYPE - PLOT 2

Ward: Radcliffe - North

Item 09

Applicant: 4pureheart

Location: 31 Brookfield Avenue, Radcliffe, Bolton, BL2 5QH

Proposal: Change of use from dwellinghouse (Class C3) to residential care home (Class C2) with additional parking.

Application Ref: 65569/Full

Target Date: 29/07/2020

Recommendation: Approve with Conditions

Description

The application site relates to a four bedroom, detached dormer bungalow at the end of a residential cul-de-sac, located on the edge of Ainsworth village. The immediate locality is residential in character with houses to the north, west and east on Brookfield Avenue and Newquay Avenue. To the south is open countryside. The site is within the Green Belt and is a Special Landscape Area.

The main street through the village is Church Street which is approximately 0.5km to the north and has a small section of shops and services and a park. The primary school in the village is Christ Church Ainsworth C of E School and there are also bus stops on Church Street with bus services to Bury, Radcliffe and Bolton.

The property, which faces south, towards the open countryside, comprises a lounge, kitchen, utility, bathroom and two bedrooms on the ground floor with two further bedrooms on the first floor. To the rear is a paved yard area and detached single garage with a driveway (approximately 17m) onto the turning head on Newquay Avenue. There is a small grassed side garden area between the side gable of the house and Newquay Avenue.

The application proposes a change of use from a dwellinghouse (residential use class C3) to a residential care home which falls within the residential use class C2. The property would accommodate a maximum of 3 children, aged between 7-17 years old and in full time education.

There would be 2 full time members of staff present at all times, operating a 24 hour shift pattern and also a home manager working a day shift (9am - 5pm). The two staff would work 2 shifts each, one during the day and one overnight shift. Shifts start at 7:45m and those staff arriving at that time would finish work the following day, leaving the premises at around 8.15am This means that there would be one 30 minute handover period per 24 hours.

A new area of hardstanding (approximately 6m by 5.5m) would be formed within the side garden to accommodate parking for one car with a space for turning. The hardstanding would be formed behind the existing boundary wall and proposed new planting and a small area of grass would be retained. Including the existing garage, the proposed development would provide 4 spaces in total - 2 in front of the garage and 1 on the proposed new hardstanding with space for turning.

The supporting statement sets out the background to the application - At present 4Pureheart have operated a number of children's care homes throughout the country over the last 20 years. They have one home in Bolton and have recently gained approval for a new home at 121 Lowercroft Road in Bury after the Local Planning Authority granted

planning permission under application reference 64816 in December 2019. The existing care homes are registered with Ofsted and also supervised by the Council's Children's Services Team.

It is the company's objective to extend childcare that is currently provided at their Bolton and Bury properties. The care home would accommodate up to three young people who require support from individuals who are experienced within the sector of children's residential care.

Visitors to the site would be infrequent, with occasional social workers visits pre-arranged, normally on a bi-monthly basis. There are no planned medical staff attendances to the home because the home would not cater for young people with a physical disability. Formal meetings with families generally take place away from the home.

Relevant Planning History

None relevant.

Publicity

The following 11 neighbours were notified by letter dated 09/06/2020.

27, 29, 33, 35, 38 Brookfield Avenue, 39 and 41 Newquay Avenue, 30 and 32 Harley Avenue, 37 and 39 Moorside.

Objections received from the occupiers of the following 44 properties.

1, 7, 19, 21, 25, 27, 29, 33, 35 and 37 Brookfield Avenue, 17, 18, 26, 28, 31, 32, 33, 35, 37, 39 and 41 Newquay Avenue, 14, 16, 22, 24, 26, 69A, 75 and 82 Church Street, 26 Harley Avenue, 36 and 38 Broomfield Close, 2 and 4 Kingsbridge Avenue, 37 Moorside, 1 Barnsdale Close, 7 Metcalfe Terrace, 1 Churchill Avenue, 11 Deansgate, 1A Devon Avenue, 1 Edgeworth Avenue, 6 Thurlestone Avenue, 16 Bradley Fold Road.

A petition has also been received with 6 names - Casalingo Restaurant, Post Office, Village Dental Practice, Village Store, Zugafun 45 Church Street and the Duke William Inn Well Street.

Ainsworth Community Association have also objected to the proposal.

The objector at 35 Brookfield Avenue has also submitted a letter from the local MP Christian Wakeford in support of his objection - that the development is unsuitable in this location which has high proportions of elderly and disabled. Children's homes need to be near youth facilities, schools and with good transport.

Objections are summarised below.

- This part of Ainsworth is a quiet area with a lot of elderly people and as such this care home is an unsuitable use in this location.
- The site is at the end of a cul-de-sac with very poor parking and access facilities.
- The site cannot accommodate all the proposed staff and visitor vehicles and this would lead to dangerous manoeuvres on the road and increased traffic on surrounding streets.
- The application has contradictory and misleading information on staff and traffic levels.
- The site location plan is totally misleading with regards to parking and proposed parking areas and indeed the size of the cars on the plan.
- With three staff with vehicles, changes of shift would create gridlock around the site.
- The road already has serious problems with on-street parking and this proposal would make the situation worse.
- The site has posts on the footway to prevent parking in front of the site.
- Access to Nos.33 and 35 is already restricted and this proposal would make the situation worse.
- The premises would be 'open' 24hours.
- Proposal would restrict waste collection even further.
- The company that would be running the care home has had poor Ofsted reports for

their existing care homes.

- Anomalies within application - applicant seems to have resigned as a director of 4Pureheart.
- There is a lack of transparency in the planning process which is skewed in favour of the applicant.
- The application has not been dealt with in accordance with proper procedure or with an impartial outlook.
- The proposal would create severe distress amongst residents.
- Concerns about children causing noise and disturbance, unruly behaviour, groups congregating outside the property.
- Problems with relatives visiting
- Danger of children absconding and anti-social behaviour/crime - drugs, thieving, joy riding, arson etc
- The three ponds near the site are a real risk to child safety in light of the incident last year when sensible teenager Shukri Abdi sadly lost her life by drowning in the River Irwell. Whilst I realise we need vulnerable children in a family surrounding this in my mind is the wrong location.
- There are insufficient facilities for children and teenagers in the area.
- Its disgraceful that the application is even being considered.
- Bury Council in considering this application has a conflict of interest in that they may use this facility.
- Company has a poor reputation and poor Ofsted rating.
- Loss of house values in the area.
- Approving this application would open floodgates for further changes of use in area.

One representation in support of the application has been received from the occupier of 70 Church Street who states:

There are lots of people already pre-judging these children and labelling them as 'unruly' and 'aggressive'. We should welcome them with love and offer kindness, care and compassion as this is what they are likely to need the most. The children may lack attachments and have low self worth which can result in challenging behaviours but surely if we as residents of Ainsworth can be supportive and nurturing we can actually help them in becoming positive members of our community. On that basis I am supporting the application.

Those making representations have been notified of the Planning Control Committee.

Statutory/Non-Statutory Consultations

Traffic Section - No objection subject to condition.

Environmental Health - No objection.

Children's Services - No objection. From Childrens services perspective 4Pureheart Heart Ltd is a provider that is fully regulated by Ofsted as an operator of Childrens homes. I do know of the provider and understand Bolton Council has used them. They most recently they came to my attention as they opened another home up at 121 Lowercroft in Bury. I do need to point out that we have no connection with the provider or company and have not asked for this home to be opened.

From our perspective we only use homes that are regulated by Ofsted as indeed Lowercroft is and I have no doubt this home will also be at some point. From a purely Childrens services perspective we have no objection to the home as far as the services provided by 4Pureheart Heart are concerned.

GM Police - No objection subject to conditions re security measures.

Pre-start Conditions - Not applicable.

Unitary Development Plan and Policies

OL1	Green Belt
EN9/1	Special Landscape Areas
CF3/1	Residential Care Homes and Nursing Homes
HT2/4	Car Parking and New Development

EN1/2	Townscape and Built Design
EN1/5	Crime Prevention
EN7/2	Noise Pollution
HT6/2	Pedestrian/Vehicular Conflict
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework
EN7	Pollution Control
H4/2	Special Needs Housing
CF3	Social Services
OL1/5	Mineral Extraction and Other Development in the Green Belt

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy and guidance - Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that: “where in making any determination under the planning Acts, regard is to be had to the Development Plan, the determination shall be made in accordance with the plan unless material consideration indicates otherwise”.

The Care Standards Act 2000 states that an establishment is a children's home if it provides care and accommodation wholly or mainly for children.

Over recent years central government has allowed children's care homes to be increasingly operated by private companies, albeit regulated by Ofsted and Local Authorities. Today the vast majority of children's care homes are privately run. As of March 2019, there were 2,304 children's homes of all types. Of these 418 are Local Authority run homes. Out of the 12 million children living in England just over 400,000 (3%) are in the social care system at any one time. More than 75,000 of these children are children in care. Across England, there are 152 LAs responsible for ensuring and overseeing the effective delivery of social care services for children. Ofsted regulates and inspects providers who offer placements for children in care.

The Children's Homes (England) Regulations 2015 and the accompanying guidance document Guide to the Children's Homes Regulations including the Quality Standards (April 2015) provides information and guidance for everyone providing residential child care.

Chapter 5 of the National Planning Policy Framework relating to the supply of homes and indicates that local planning authorities should endeavour to provide a sufficient supply of homes of different sizes, types and tenures.

Chapter 8 of the National Planning Policy Framework relates to promoting healthy and safe communities. Paragraph 91 states that planning policies and decisions should aim to achieve healthy, inclusive and safe places which:

- promote social interaction, including opportunities for meetings between people who might otherwise come into contact with each other.
- are safe and accessible, so that crime and disorder, and fear of crime, do not undermine the quality of life or community cohesion.
- enable and support healthy lifestyles.

Policy H4/2 Special Needs Housing states that the Council will encourage the provision of special needs housing. Such proposals would be assessed against criteria such as location

in relation to shops and services and should provide the necessary car parking and amenity space required for such uses.

CF3 Social services considers favourably proposals for the provision of new, and the improvement of, existing facilities including children and young people, encouraged to live in the local community rather than in large institutions. Support for new and improved services is supported providing that there is no conflict with existing residential amenity and the environment.

CF3/1 Residential Care Homes and Nursing Homes should be located in residential areas and will be permitted where they do not conflict with the amenity of adjoining areas.

The size of the house and numbers of bedrooms is such that the property is only able to accommodate a limited number of children at one time - up to 3 as a maximum. OFSTED and Childrens Services would also have a set of criteria for standards of accommodation which would needed to be adhered to for children in care.

The applicant and the company is a Registered Provider and has care homes in Bolton and most recently, in Bury (121 Lowercroft Road application reference 64816). Under current legislation the home would be regularly inspected by OFSTED. In addition, the operation would be inspected by the Children's Services Department.

Childrens Services - The Council's Childrens Services have no objection to the application stating that 4Pureheart Heart Ltd is a provider that is fully regulated by Ofsted as an operator of Childrens homes.

Scale and intensification of use - Given the occupation of the property by 3 children and up to three adults would not go beyond what would be generally expected of a family residing in a four bed property.

In terms of staff numbers and shift patterns, the arrangement to provide 2 full time staff over a 24hr period and an additional manager during the day, would not be dissimilar to the normal comings and goings of two working parents with children, possibly up to 17yrs, carrying out daily work, school and leisure activities and routines. It is thus considered that the proposed use would not change the character of the site or surroundings beyond its current residential use.

It is therefore considered that the scale of the proposed use would not have an adverse or detrimental impact on the character of the surrounding area or adjacent properties.

A number of uses fall under the umbrella of Use Class C2 (residential institutions), including a hospital or nursing home, residential school, college or training centre. The size of the property and the existing parking and access may be substandard for such other C2 Uses which would also have greater impact. As such it is considered reasonable to control the use for a child care facility only and for a maximum of 3 children at any one time. Appropriate conditions to this effect would therefore be included.

Visual Amenity and character of Green Belt - There would be no significant alterations to the house with the layout remaining as existing. Externally an additional hardstanding, to accommodate a turning area or parking, would be created at the side of the house.

The dwelling has a relatively long driveway (approximately 17m) leading to the detached garage, situated behind the house. A revised layout plan, submitted in response to concerns about the limitations of on-site parking, indicates a new area of hardstanding (approximately 6m by 5.5m) to accommodate additional parking, giving a total of 3 spaces on site and a turning facility which would be formed behind the existing boundary wall and proposed new planting. As with the existing arrangement, refuse bins would be stored to the side/rear of the house and brought to the road side on collection day. At the rear the yard area would provide outdoor amenity space as it was when the property was a

dwellinghouse.

It is therefore considered that there would be no detrimental impact on the character of the residential use or appearance of the existing dwelling, which would for all intents and purposes, continue to function as any other dwelling in the area. As the physical characteristics of the property would not significantly change, it would retain the capability of use as a dwelling in the future.

In terms of Green Belt and Special Landscape policies, the site is suburban in character and within the village envelope. It is not considered that the area of hardstanding that would be created would have a significant and detrimental impact on the character and openness of the Green Belt and Special Landscape Area.

As such, it is considered that the proposal would comply with UDP Policies EN1/2, EN9/1, OL1, OL1/5, CF3 and CF3/1 relating to care homes.

Residential amenity - The impact of the care home on the residential amenity of surrounding residents is a critical factor in the assessment. The main factors within this assessment are the potential for noise and disturbance from activities within and around the premises from the occupiers and visitors. Another factor is the numbers of vehicles generated by the use, particularly later at night and early mornings with associated opening and closing of doors, revving engines etc.

With regard to noise and disturbance from occupants of the property, whilst they do not constitute a household as such, with three children and up to three adults within the premises (two overnight), the character and the scale of the use would be generally consistent with that of a 'normal' family household occupying a 4 bedroomed house with the associated day to day activities, including the comings and goings from family/friends visiting or deliveries made. With the children under the supervision by qualified carers who could quickly deal with any problems that may arise, it is not considered that there would be excessive noise and disturbance beyond normal 'domestic' activities. Indeed with the constant supervision, it may well be less than that experienced with a family of similar size.

The applicant is a registered and experienced care provider and would be under the supervision of both the Local Authority and OFSTED with regular inspections to ensure the care home is run in accordance with prescribed standards. Living within a community environment and providing the children with a 'normal' lifestyle is also one of the reasons for accommodating children within a house and residential environment, and it is therefore in the interest of the applicant that they integrate within the immediate community rather than be a cause of conflict.

Objections which have been received by local residents allude to the 'type' of children the home could accommodate and the potential disruption or risk they could pose to the area. Whilst noise and disturbance and the potential for anti-social behaviour or even crime and fear of such, are material considerations, such considerations have to be made with all other considerations taken into account in a balanced assessment. The fact that a child is in need of care does not mean that they would necessarily be of an anti-social disposition or bring any disorder or criminal activities to an area. If there is a case of anti-social behaviour, given the supervision in place, this is likely to be limited and short-lived. Indeed, given the investment within the operation and the regulation by both Ofsted and the Local Authority, it would be in the interests of the operator to ensure that the care home is run in a professional manner that would not give rise to complaints by surrounding residents. One of the objectives of the care home would be to ensure that the children were able to live as part of the local community, functioning as much as possible as a 'normal household' to bring stability and to be able to live as part of the community.

With regard to the impact of vehicles on residential amenity, the most noticeable difference would be during the daily 'change of shift' of the staff.

Staff would work a double shift, one during the day and one overnight shift. Two members of staff would be present at all times. Staff work on rota, completing the double shift including an overnight stay. Shifts start at 7:45am and those staff arriving at that time will finish work the following day. Meaning there is one handover per 24 hour period. New staff would arrive around 7:45am whilst the previous day's staff will leave by 8:15am. This means that there is a 30 minute window for the staff handover. In addition, the home manager would attend the property, their working hours are 9am-5pm and as such they do not crossover with the handover.

There would be some visits by social workers and these would normally be bi-monthly and, as with the majority of visitors, pre-arranged with carers. The applicant states that is usual practice to have family visits away from the premises, thereby reducing potential traffic generation. It is also indicated that, unlike some families, any teenagers staying at the property would not have a car. The care facility would not cater for children with physical disabilities and as such, special medical attention and visits by medical staff would not be required.

As a 4 bedroomed dwelling, the property could easily be occupied by 3 car owners, plus any additional trips to the site by visitors, friends/family and from deliveries or any servicing requirements associated with day to day living.

There would also be the usual servicing vehicles or potentially deliveries made to the site but this would be typical of everyday life and not dissimilar to that of a family in a dwelling of this size.

Given the nature and number of occupants and the operating procedures that would be in place, it is not considered that the proposal would generate excessive noise and disturbance so as to cause serious and detrimental harm to the residential amenity of surrounding neighbours. As such the proposal would be acceptable and comply with the NPPF and UDP Policies CF3, CF3/1 and EN7/2 with regard to residential amenity.

Highways and Parking - The property has an existing detached garage with access off Newquay Avenue and there are no proposals to change this arrangement. It is proposed to create an additional hardstanding in the side garden area for additional parking and turning within the site. The turning head outside the premises, at the end Newquay Avenue is relatively small but there are no parking restrictions on the road itself. Most properties in the area have their own driveways but it is evident that there is on-street parking along the road.

With a maximum of three children and up to three staff (two staying overnight), it is not considered that the proposed use would generate significantly more traffic than could currently be experienced at the property if a family were in occupation. Given the existing garage and driveway and the proposed additional hardstanding to allow for parking for at least three cars with turning, the proposal layout is considered suitable for the scale of use proposed. Whilst the majority of staff in care homes may arrive by car, some may utilise alternative forms of transport as many conventional households do. Although visitors could park on the road, this would be no more likely than anyone visiting a family, indeed with pre-arranged visits, it is more likely that the parking arrangements would be managed better than in a more ad-hoc family situation.

The traffic team have assessed the scheme and have no objection to the proposal subject to a condition requiring the parking and turning arrangements to be implemented prior to the use commencing. It is considered that the proposed development would not significantly increase traffic to the area or generate levels of parking beyond what would reasonably be expected of a family dwelling of this size. Vehicle parking facilities are therefore considered adequate and the proposal is therefore considered to comply with UDP Policies EN1/2, CF3/1, HT2/4 and HT6/2.

Servicing - The property would be serviced as a regular household would be with the

regular waste collections.

Greater Manchester Police - The police have recommended a number of measures that could be introduced to ensure the facility can be operated safely and securely. It is considered appropriate to attach an informative setting out the recommended measures. A suitable condition would also be attached to any approval requiring details of any external lighting to be submitted to the Local Planning Authority prior to occupation to ensure that it would be suitable in this residential setting.

Facilities for children - Ainsworth Village has a primary school and a range of shops, services and recreational facilities, including park and play area. There are bus services to Bury and Bolton and carers, as with any parents with children living in the village, are able to take children to events or facilities further afield. There are high schools in Bury and Tottington.

Public safety - Children in care homes come from a wide range of backgrounds and experiencing a range of issues. With the proper safeguarding procedures in place, regulated by Ofsted and Children's services, it would be unreasonable to assume the children occupying the property are a serious danger to the public or even criminally minded. Whilst the concerns about the proposed care home amongst residents are recognised to a point, the proposal has to be assessed on policies and material considerations.

With regard to the safety of children, the carers would be suitably qualified to ensure that the proper safeguarding measures are in place and the children are adequately supervised both in the property and outside.

Conditions - It would be reasonable to attach a condition, restricting the use to that of a care home only to prevent a further change to other uses with the C2 use class.

A condition would limit the number of staff and children residing in the property at any one time to prevent any over intensification of use of the C2 care facility.

A condition would require the hardstanding to be completed prior to the use/occupation commencing.

A condition would require details of any external lighting to be submitted and approved prior to use/occupation commencing.

Objections - Where relevant to the assessment of the planning application, the issues raised by the objectors have been addressed in the above report. Although there may be a high proportion of elderly people in the area, government guidance suggests that a suitable mix of tenures and residents is preferable to concentrations of similar groups in a particular area. In addition to elderly people within the area, there are also families and single person households.

With regard to Ofsted and ratings, this care home would be assessed on its own merits in line with current guidelines. Should Ofsted, and/or the Local Authority Children's services, find a problem with the care home, it would take the necessary action which could, in extreme circumstances, include closing the home.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were

incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings - Existing and proposed floor plans and site location plan 31/BA/COU/B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The premises to which this approval relates shall be used for residential accommodation and care for people in need of care and for no other purpose (including any other purpose in Class C2 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (or in any provision equivalent to that class in any statutory instrument revoking or re-enacting that Order with or without modification).
Reason: To ensure the intensification and scale of uses in the property does not extend beyond acceptable levels which would cause impact to residential amenity and highway safety in respect of the associated parking, access and servicing requirements or general activity and disturbance pursuant to policies EN1/2 Townscape and Built Design, CF3 Social Services, CF3/1 Residential Care Homes and Nursing Homes, HT2/4 Car Parking and New Development and HT6/2 Pedestrian/Vehicular Conflict of the Bury Unitary Development Plan.
4. There shall be no more than 3 children accommodated at the property hereby approved and no more than 3 staff, including the home manager, on duty at any one time.
Reason. To ensure the appropriate provision of facilities and standards of accommodation are provided for future occupiers and to ensure the use of the property is not over intensified to unacceptable levels which would incur additional activities and disturbances to local residents beyond that which would be accepted for a residential dwelling in a residential area pursuant to Bury Unitary Development Plan Policies EN1/2 Townscape and Built Design, CF3 Social Services and CF3/1 Residential Care Homes and Nursing Homes.
5. The car parking and turning facilities indicated on approved plan reference 31/BA/COU Revision B shall be surfaced and made available for use to the written satisfaction of the Local Planning Authority prior to use hereby approved commencing and thereafter maintained at all times.
Reason. To ensure adequate off-street car parking provision and to minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Policy HT2/4 Car Parking and New Development.
6. Details of the planting along the boundary with Newquay Avenue shall be submitted to, and approved by the Local Planning Authority prior to the commencement of the development. The approved scheme shall thereafter be implemented not later than 6 months from the date the property is first occupied and any plants removed, dying or becoming severely damaged or becoming severely diseased within three years of planting shall be replaced by plants of a similar size or species to those originally required to be planted.

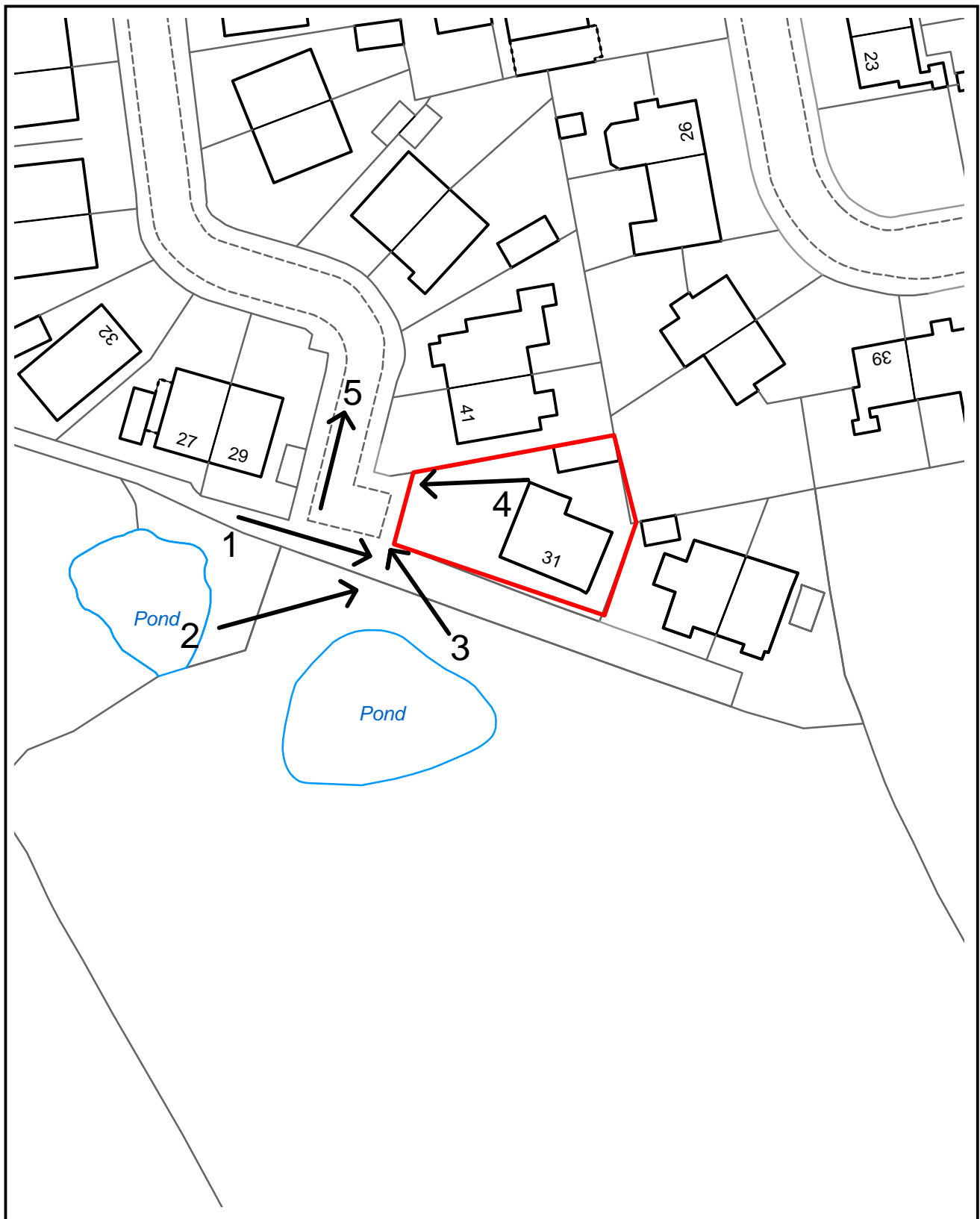
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies EN1/2 Townscape and Built Design and EN1/3 Landscape Provision.

7. Prior occupation, details of all external lighting and/or CCTV to be installed at the premises, shall be submitted to the Local Planning Authority for written approval. Only the approved details shall be implemented and remain in situ thereafter.

Reason. In the interests of security, public safety and residential amenity pursuant to the NPPF and UDP Policies EN1/2, EN1/5, EN7 and CF3.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65569

**ADDRESS: 31 Brookfield Avenue
Ainsworth**



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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65569

Photo 1



Photo 2



65569

Photo 3

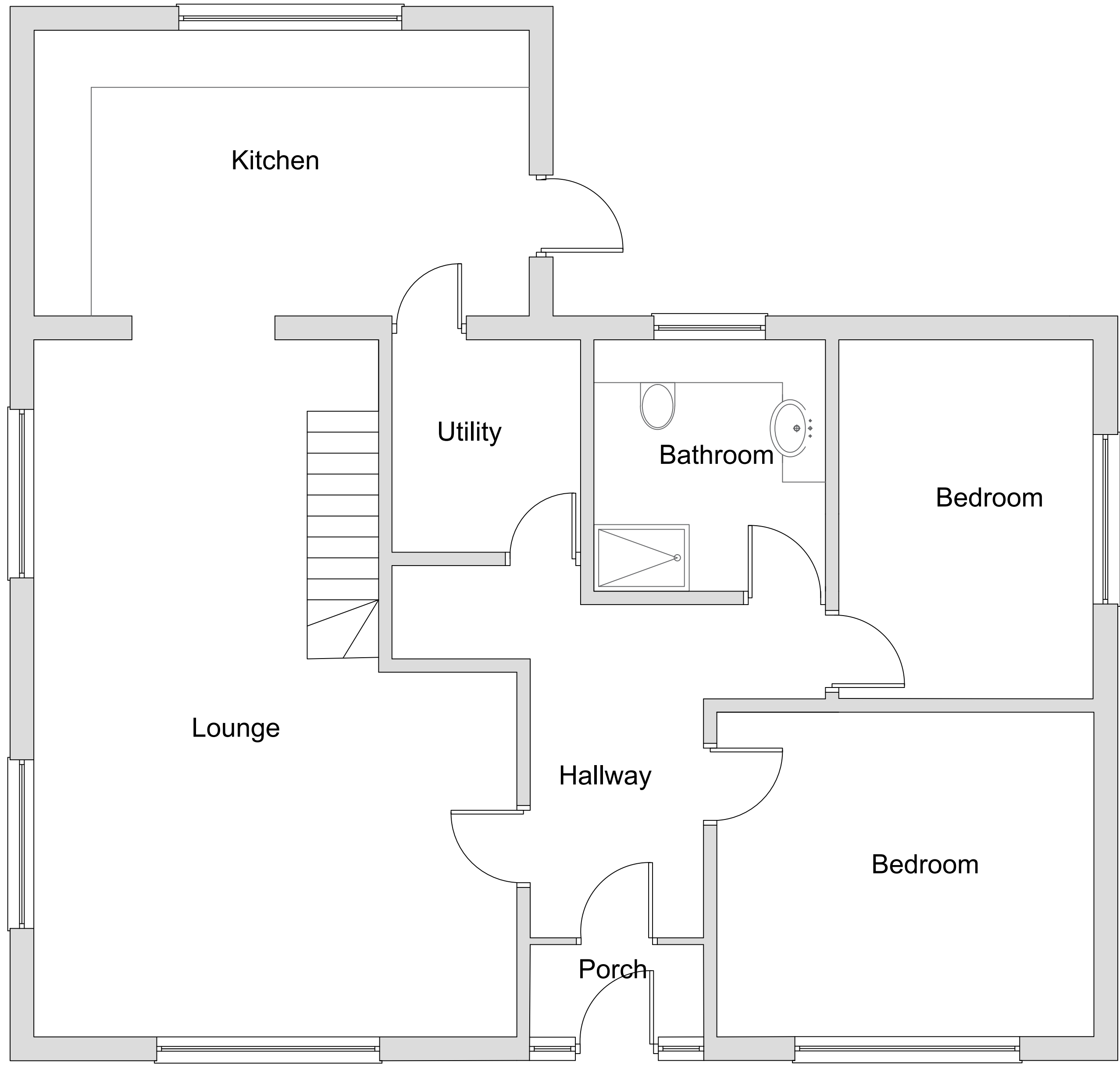


Photo 4

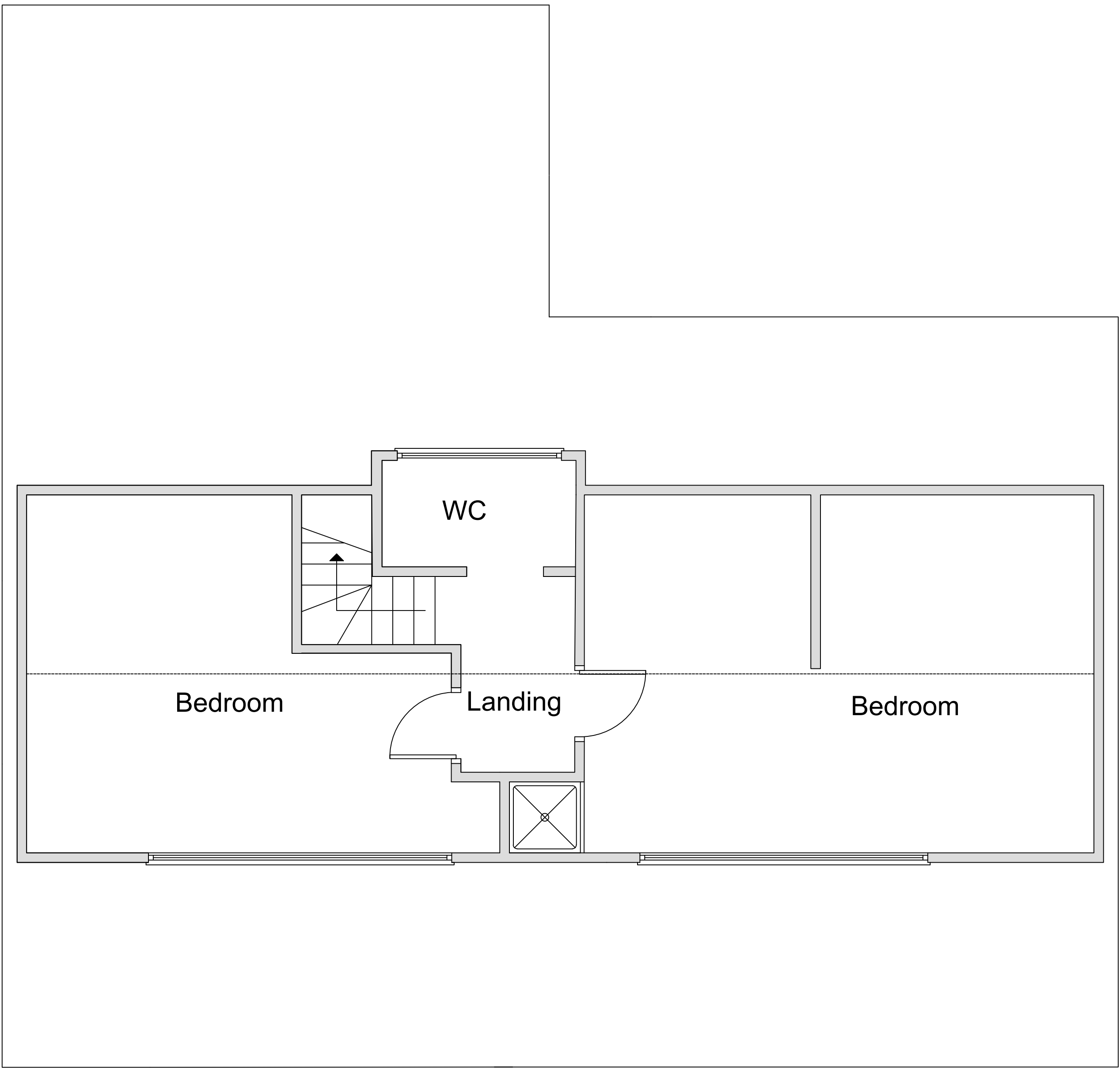


Photo 5

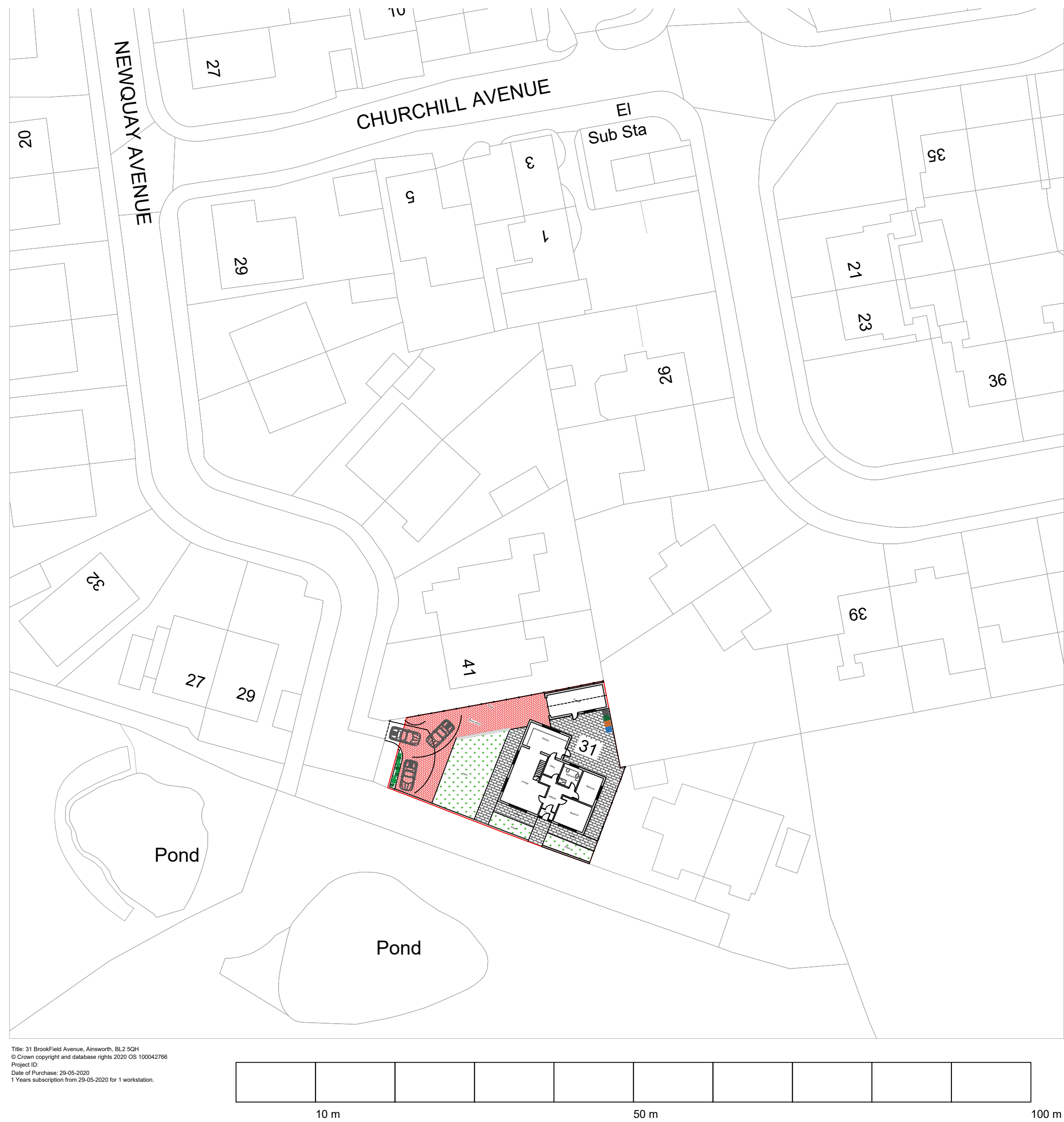




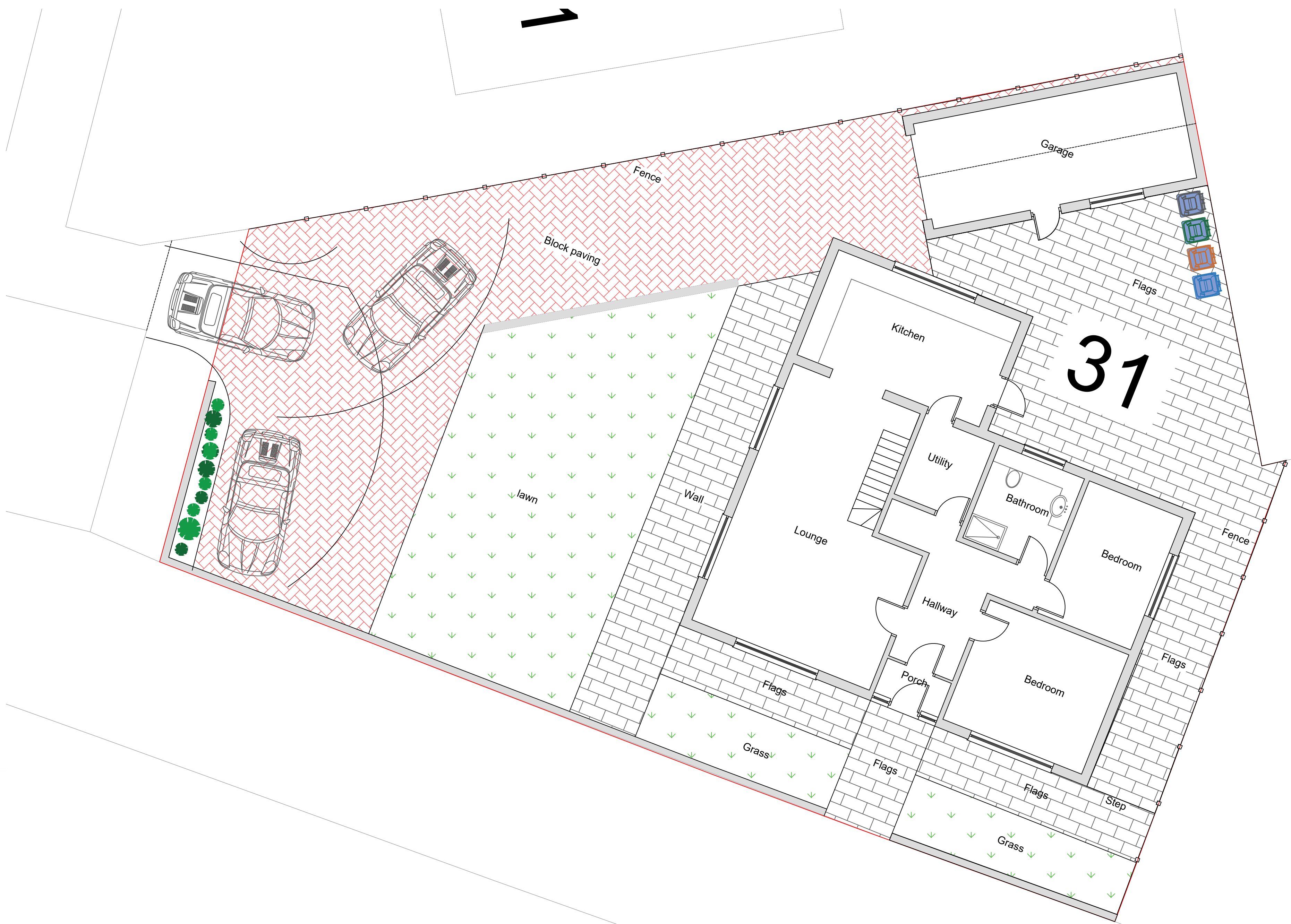
Ground Floor
Existing & Proposed Plans @ 1:100



First Floor



Site Location plan @ 1:1250



Site Layout plan @ 1:200

DO NOT SCALE OFF THIS DRAWING

B	09.07.20	MH	Planting added to boundary.
A	09.07.20	MH	Car swepth path added. Lawn changed.
Revision:	Date:	By:	Details of Revision:

client

Paul Davidson

project

31 Brookfield Avenue, Ainsworth, BL2 5QH

project no

31/BA/COU

drawing no

Existing & proposed floor plans & site location plan.

rev

B

description -

Change of Use from C3 to C2.

scale

As shown @ A3

date drawn

29.05.2020

drawn by

MH

authorised by

MH

purpose of issue

planning ☒ building regs ☐ tender ☐ comment ☒ approval ☒ construction ☐

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Ward: Prestwich - St Mary's

Item 10

Applicant: M Abbariki

Location: Land at rear of 27 Duckworth Road, Prestwich, Manchester, M25 9GF

Proposal: Variation of condition 2 (approved plans) of planning permission 63374 to remove ash tree (T2)

Application Ref: 65589/Full

Target Date: 04/08/2020

Recommendation: Approve with Conditions

Description

The application site is located between Nos 24 and 26 Lowther Road. The site contained a detached garage at the rear of the site with an area of hardstanding between the garage and Lowther Road. There are metal railings and gates along the footpath with Lowther Road. Vehicular access is taken from Lowther Road.

The site is bounded by residential properties. No. 24 is approximately 0.5 metre lower than the application site and No. 26 is approximately 0.5 metre higher than the application site.

Planning permission was granted for the demolition of the single storey garage on site and the erection of a small single dwelling. The dwelling would be single storey with living space in the roof and would be constructed from brick with a slate roof. There would be a pitched roof for the front part of the proposed dwelling and a flat roof at the rear. Access would be taken from Lowther Road. Development has commenced on this proposal.

The proposed development involves the variation of the approved plans condition (No. 2) to allow the removal of an ash tree at the rear of the building. The previously approved site plan showed the tree to be retained. A arboricultural report was submitted with the application and recommends that the tree should be removed.

Relevant Planning History

02176/E - Proposed demolition of an existing disused garage and erection of 1 no dwelling at land to rear of 27 Duckworth Road, Prestwich. Enquiry completed - 17 August 2018.

63374 - Erection of 1 no. dwelling at land to rear of 27 Duckworth Road, Prestwich. Approved with conditions - 20 February 2019.

Adjacent site

33644 - Conservatory extension at the front at 24 Lowther Road, Prestwich. Approved with conditions - 17 December 1997.

41156 - Porch at side at 24 Lowther Road, Prestwich. Approved with conditions - 4 September 2003.

Publicity

The neighbouring properties were notified by means of a letter on 18 June 2020 and a press notice was published in the Bury Times on 25 June 2020. Site notices were posted on 26 June 2020.

4 letters have been received from the occupiers of 24, 26 and 28 Lowther Road, which have raised the following issues:

- The tree is located on the Northern boundary of the plot and is therefore jointly owned by the applicant and the owners of the adjacent property at 24 Lowther Road, as such the tree cannot be removed or any work undertaken on the tree without the permission of the owners of 24 Lowther Road.
- The two new site plans submitted with this application show a change to the North West boundary such that the tree is no longer shown on the boundary but on the applicant's property, such an arbitrary change to the boundary cannot be acceptable and any change to the boundary must be agreed with the owners of the adjacent property at 24 Lowther Road.
- I object to the removal of the tree without the permission of the joint owners of the tree at 24 Lowther Road.
- During excavation works carried out at the above, significant damage has been caused to my property.
- Looking at the original plans I now see the boundary has moved. I assume the applicant has gained permission and has now gained full ownership of the tree, which was previously shared. Mr Price who I thought owned half the tree does not want it removed and I agree.
- This Ash is a perfect tree for this location as its airy canopy allows the sunlight through so as not to take light away from the properties. It could live for up to 400 years or more! The nuthatches we see in our gardens live in this tree, owls and redstarts would also use it for nesting, Bull-finches eat the seeds.
- This tree is invaluable to lots of wildlife species and to us for its environmental service of purifying our air! A whole boundary row of several Holly trees have already been removed from this plot for the purpose of building. Please don't chop this beautiful Ash tree down!
- Before the revised application, I spoke to the case officer who clearly stated that the tree was not to be removed.
- The tree is on my land, which Bury have known from the beginning.
- The developer has removed less than a foot of soil and seen the enormous size of this large ash tree, which roots travel under the garage and onto my land.
- I will once again state that I will sue Bury Council and the developer if any part of the tree is removed or damaged as it is on my land.

The objectors have been notified of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

None required.

Pre-start Conditions - Not relevant.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/2	Woodland and Tree Planting
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (Residential) - The principle of the proposed development was established with the grant of planning consent in February 2019. Therefore, the proposed development is in accordance with Policy H1/2 of the Bury Unitary Development Plan.

Design and layout and impact upon conservation area - Whilst the application site is not located within the St Marys Conservation Area, it does face the Conservation Area boundary and as such, the proposal may have an impact upon the setting of the Conservation Area. Therefore, it is necessary to consider the proposal in terms of the Town and Country Planning Listed Building and Conservation Act.

Sections 66 and 72 of the Town & Country Planning (Listed Buildings & Conservation Areas Act) 1990 imposes a statutory duty upon the Local Planning Authority (LPA) to recognise, positively manage and ensure the preservation or enhancement of Listed Buildings and Conservation Areas.

Chapter 12 of the National Planning Policy Framework (NPPF) sets out the Government's planning policy position in relation to development involving heritage matters. Paras 128 to 141 set out how the significance of listed buildings and Conservation Areas must be considered in relation to development proposals including, but limited to, the judgement of harm, significance, loss to heritage assets (Designated ones and non-designated ones).

UDP Policies EN2/1, EN2/2 and EN2/3 seek to protect the character of listed buildings and conservation areas and to ensure that proposals preserve or enhance the character of such sensitive buildings and areas. In applying the tests of the policy to proposals, nature of the development, materials, relationships between visual qualities and the surrounding areas et al form part of the assessment process to ensure a high, conservation approach is adopted that is designed to preserve or enhance.

The design of the proposed dwelling has not been amended since the previous approval. As such, the proposed dwelling would preserve the surrounding properties and would not be a prominent feature in the streetscene. Therefore, the proposed development would be in accordance with Policies EN1/2, H2/1, H2/2, EN2/1 and EN2/2 of the Bury Unitary Development Plan and the NPPF.

The proposed dwelling would have a rear garden, which would have an acceptable level of private amenity space. There would be space within the rear and side garden for bin storage. Therefore, the proposed development would not be a prominent feature in the streetscene and would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

Impact upon residential amenity - SPD6 provides guidance on aspect standards between residential properties and as such, would be relevant in this case. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows, 13 metres between a habitable room window and a two storey blank wall and 6.5 metres between a habitable room window and a single storey blank wall.

The proposed dwelling has not been amended since the previous approval, which complied with the aspect standards. Therefore, the proposed development would not have a

significant adverse impact upon the amenity of the neighbouring properties.

Trees - The previous approval (63374) stated that the ash tree (labelled T2 in the arboricultural report) at the rear of the site, which is located on the boundary with No. 24 Lowther Road, would be retained. The development has begun in accordance with permission 63374 and it has become apparent that the tree can no longer be retained due to the close proximity of the construction works. The applicant has submitted an arboricultural report, which confirms that this tree would have to be removed to facilitate the development.

The tree is a category B tree and is located at the rear of the site. Whilst it is acknowledged that the tree is of moderate quality, the view of the tree has always been obscured by the existing garage and would have been obscured by the proposed dwelling. As such, the tree contributes little to the streetscene.

The Council's arboriculturalist agrees with the conclusions of the arboricultural report in that the tree should be removed and a replacement tree provided.

The applicant has confirmed that a replacement tree would be planted to the front of the proposed dwelling and is shown on the proposed site plan. As such, the proposed development would not have an adverse impact upon the character of the area and would be in accordance with Policy EN8/2 of the Bury Unitary Development Plan.

Highways issues - The proposed development would not impact upon the previously approved access, turning and parking arrangements. Therefore, the proposed development would not be detrimental to highway safety and would be in accordance with Policies EN1/2, H2/1, H2/2 and HT2/4 of the Bury Unitary Development Plan.

Response to objectors

- The agent has provided revised plans, which have amended the red edge to reflect the land in his ownership.
- Photo 3 on the committee report shows the position of the tree in November 2018 and it is on the development's side of the fence. Irrespective of this, land ownership matters are not material considerations for this application.
- The issues relating to the removal of the tree have been addressed in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered AS01, 19023 01A, 19023 02A and the development shall not be carried out except in accordance with the drawings

hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. The external finishing materials shall be those materials approved as part of condition 3 of permission 63374.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. The remediation strategy approved as part of condition 4 of permission 63374 must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and
A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
5. The car parking indicated on approved plan reference AS02 shall be surfaced and made available for use prior to the dwelling hereby approved being occupied and thereafter maintained at all times.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
6. The turning facilities indicated on approved plan reference AS02 shall be provided before the dwelling is first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.
Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:
Policy H2/1 - The Form of New Housing Development
Policy H2/2 - The Layout of New Housing Development
Policy EN1/2 - Townscape and Built Design
7. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, nor shall the flat roof be used as a balcony without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan H2/3 Extensions and Alterations, H2/1 - The form of New Residential Development and H2/2 - The Layout of New Residential Development.
8. The development hereby approved shall be carried out in accordance with the Construction Traffic Management Plan approved as part of condition 9 of permission 63374.
The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.
Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of

deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

9. The development hereby approved shall be carried out in accordance with the visibility splays approved as part of condition 10 of permission 63374. The splays subsequently approved shall be implemented prior to the dwelling hereby approved being occupied and subsequently maintained free of obstruction above the height of 0.6m.

Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy H2/1 - The Form of New Housing Development

Policy H2/2 - The Layout of New Housing Development

Policy EN1/2 - Townscape and Built Design.

10. Subject to the amendments required as part of Condition 2, the vehicular and pedestrian access arrangements indicated on approved plan reference AS02, incorporating the provision of a sliding gate, boundary wall foundations that do not encroach under the adjacent adopted highway and all associated highway remedial works, shall be implemented before the development is first occupied.

Reason. To ensure good highway design in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy H2/1 - The Form of New Housing Development

Policy H2/2 - The Layout of New Housing Development

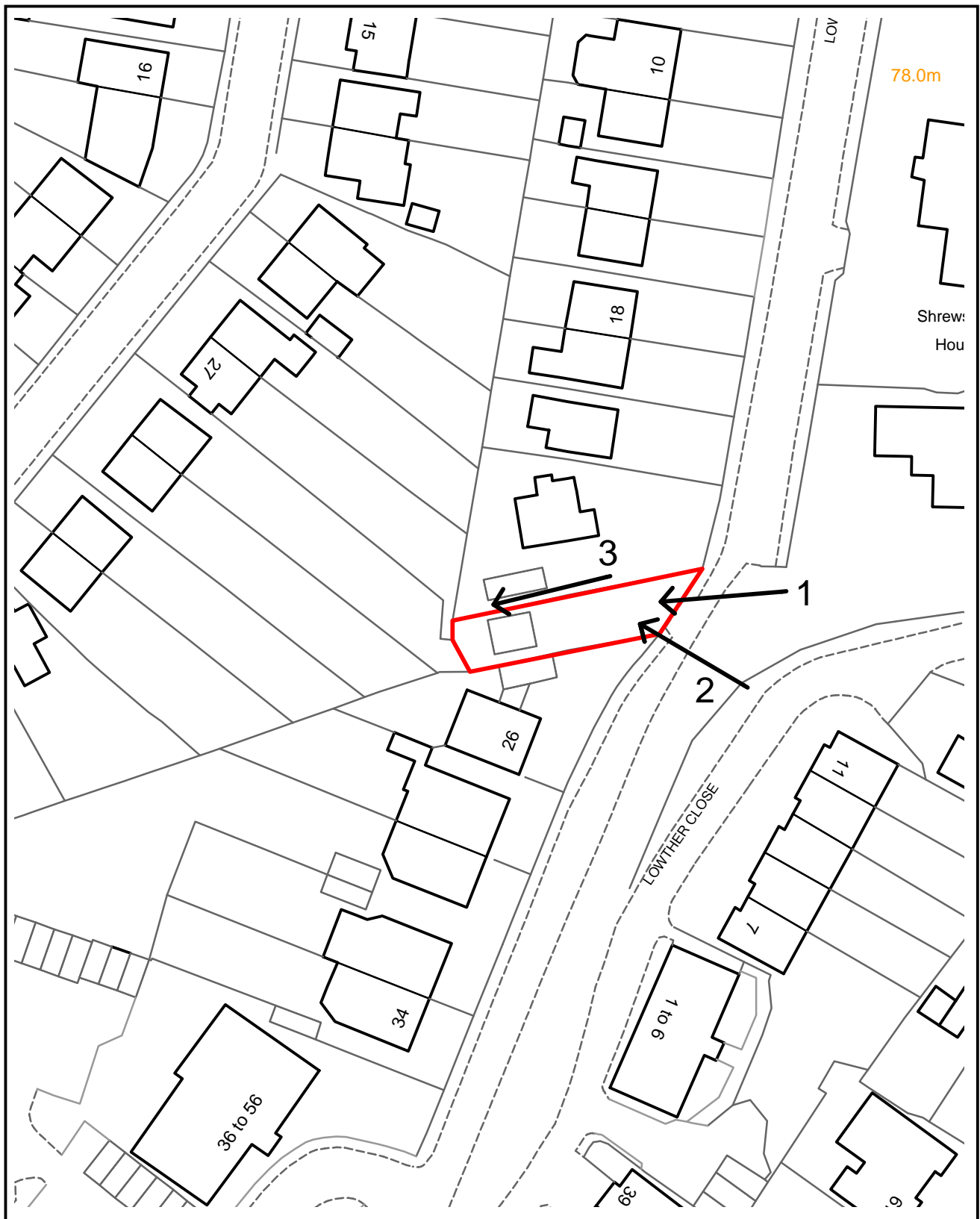
Policy EN1/2 - Townscape and Built Design.

11. The replacement tree, identified on plan reference 19023 01A shall be planted within the first planting season after the dwelling hereby approved is first occupied. The tree shall be at least 14 - 16 cm in girth. Any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

For further information on the application please contact **Helen Leach** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 65589

ADDRESS: Land at 27 Duckworth Road
Prestwich

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

Photo 1



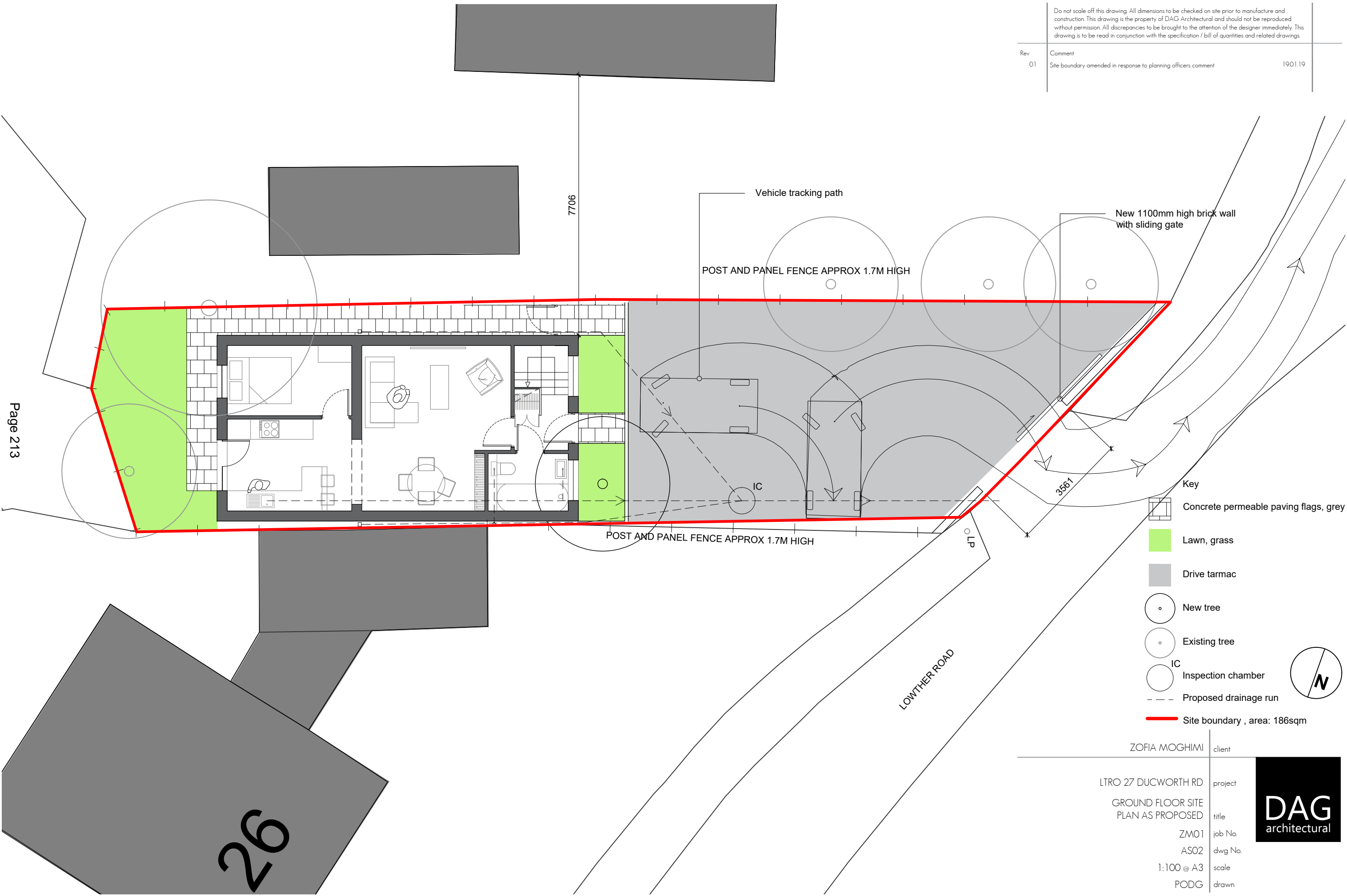
Photo 2



65589

Photo 3





Do not scale off this drawing. All dimensions to be checked on site prior to manufacture and construction. This drawing is the property of DAG Architectural and should not be reproduced without permission. All discrepancies to be brought to the attention of the designer immediately. This drawing is to be read in conjunction with the specification / bill of quantities and related drawings.		
Rev	Comment	
01	Site boundary amended in response to planning officers comment	19.01.19

- Key
- Concrete permeable paving flags, grey
 - Lawn, grass
 - Drive tarmac
 - New tree
 - Existing tree
 - IC Inspection chamber
 - Proposed drainage run
 - Site boundary , area: 186sqm

ZOFIA MOGHIMI	client	
LTRO 27 DUCWORTH RD	project	
GROUND FLOOR SITE PLAN AS PROPOSED	title	
ZM01	job No.	
AS02	dwg No.	
1:100 @ A3	scale	
PODG	drawn	

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REPORT FOR NOTING

Agenda Item 5

Bury
COUNCIL

**Agenda
Item**

5

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	28 July 2020
SUBJECT:	DELEGATED DECISIONS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	The report lists: Recent delegated planning decisions since the last PCC
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A
Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS**DIRECTOR:**

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Planning Control Committee of the delegated planning decisions made by the officers of the Council.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-None**Contact Details:-**

David Marno, Head of Development Management
Planning Services, Department for Resources and Regulation
3 Knowsley Place
Bury BL9 0EJ

Tel: 0161 253 5291

Email: d.marno@bury.gov.uk

Planning applications decided using Delegated Powers

Between 15/06/2020 and 19/07/2020



Ward: **Bury East**

Application No.: 65385 **App. Type:** FUL 30/06/2020 Approve with Conditions
Location: Fairfield General Hospital, Rochdale Old Road, Bury, BL9 7TD
Proposal: Link bridge at first floor level to connect Wards 24 and 25 to the main hospital street corridor

Application No.: 65433 **App. Type:** FUL 29/06/2020 Approve with Conditions
Location: 27 Benson Street, Pimhole, Bury, BL9 7EP
Proposal: Single storey extension at rear

Application No.: 65466 **App. Type:** FUL 24/06/2020 Approve with Conditions
Location: Fairfield General Hospital, Rochdale Old Road, Bury, BL9 7TD
Proposal: Extension at level 03 built on the existing flat roof to the South.

Application No.: 65509 **App. Type:** ADV 09/07/2020 Approve with Conditions
Location: 60 Bolton Street, Bury, BL9 0LL
Proposal: 1 no. non illuminated fascia sign

Application No.: 65519 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: 24 Woodgate Avenue, Woodgate Hill, Bury, BL9 7RU
Proposal: Two storey rear extension and first floor/two storey side extension

Application No.: 65521 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 18 Parsons Lane, Bury, BL9 0JZ
Proposal: Assessment, renovation, repair and replacement of roof slates/membrane, leadwork and sundries

Application No.: 65522 **App. Type:** LBC 10/07/2020 Approve with Conditions
Location: 18 Parsons Lane, Bury, BL9 0JZ
Proposal: Listed building consent for assessment, renovation, repair and replacement of roof slates/membrane, leadwork and sundries

Ward: **Bury East - Moorside**

Application No.: 65349 **App. Type:** FUL 30/06/2020 Approve with Conditions
Location: 37 Lowes Road, Bury, BL9 6PW
Proposal: Two storey rear extension; Two storey side extension

Application No.:	65449	App. Type:	FUL	24/06/2020	Approve with Conditions
Location:	Howarth Timber & Building Supplies, Bright Street, Bury, BL9 6AQ				
Proposal:	Installation of new external door with roller shutter to Lucas Street elevation				
Application No.:	65450	App. Type:	LDCP	17/06/2020	Lawful Development
Location:	Cricketers Inn, 415 Walmersley Road, Bury, BL9 5EU				
Proposal:	Lawful development certificate for the proposed change of use of a single dwellinghouse (Class C3(a)) to a care home (Class C2) for no more than 6 persons including carers				
Application No.:	65470	App. Type:	GPDE	24/06/2020	Prior Approval Not Required - Extension
Location:	25 Woodman Drive, Bury, BL9 5HQ				
Proposal:	Prior notification of proposed single storey rear extension				
Application No.:	65482	App. Type:	FUL	25/06/2020	Approve with Conditions
Location:	Ferngrove House, 1 Ferngrove, Rochdale Old Road, Bury, BL9 7LS				
Proposal:	Change of use from house in multiple occupation (HMO) for five persons (Class C4) to HMO for seven persons (Sui Generis), loft conversion with rear dormer and alterations to external elevations				
Application No.:	65485	App. Type:	FUL	03/07/2020	Approve with Conditions
Location:	7 Back Walmersley Road West, Bury, BL9 5DA				
Proposal:	Change of use of first floor from flat to office (Class Use B1 (a))				
Application No.:	65511	App. Type:	FUL	09/07/2020	Approve with Conditions
Location:	35 Danesmoor Drive, Bury, BL9 6ER				
Proposal:	Two storey side extension				
Application No.:	65524	App. Type:	FUL	08/07/2020	Approve with Conditions
Location:	Land at Moorgate & Peter Street, Bury, BL9 6AB				
Proposal:	Formation of public pay and display car park including resurfacing works, improvements to existing access, formation of new boundary features and removal of unauthorised access.				

Ward: Bury East - Redvales

Application No.:	65348	App. Type:	FUL	29/06/2020	Approve with Conditions
Location:	68 Cornwall Drive, Bury, BL9 9EX				
Proposal:	Dormer to front; Bay window and new entrance to front; Two storey side extension; Single/two storey rear extensions				
Application No.:	65357	App. Type:	FUL	25/06/2020	Approve with Conditions
Location:	Ground Floor Flat, 100 Manchester Road, Bury, BL9 0TH				
Proposal:	Conversion of the ground floor to form an additional flat				
Application No.:	65461	App. Type:	FUL	30/06/2020	Approve with Conditions
Location:	119 Cornwall Drive, Bury, BL9 9EX				

Proposal: Raise ridge height of roof and roof extension with two storey extension at side and single storey extensions at front, side and rear. Creation of new access and parking area to front of property.

Application No.: 65494 **App. Type:** FUL 17/07/2020 Approve with Conditions
Location: 107 Gigg Lane, Bury, BL9 9JB

Proposal: Single storey extension at side/rear

Application No.: 65500 **App. Type:** TEL 08/07/2020 Prior Approval Required and Refused
Location: Land at junction of Redvales Road and Manchester Road, Bury, BL9 9PU
Proposal: Proposed MBNL 25.00m Slimline Lattice Tower on 6.00m x 6.00m reinforced concrete base and associated ancillary works.

Ward: **Bury West - Church**

Application No.: 65323 **App. Type:** P3CPA 24/06/2020 Prior Approval Required and Granted
Location: 171 Ainsworth Road, Bury, BL8 2RU
Proposal: Prior approval for proposed change of use from shop (Class A1) to restaurant/cafe (Class A3)

Application No.: 65432 **App. Type:** FUL 25/06/2020 Approve with Conditions
Location: Bury Elton Gasworks, Victoria Street, Bury, BL8 1LE
Proposal: Engineering works comprising the partial infilling of voids, created as a result of the demolition of gasholders on the site

Application No.: 65434 **App. Type:** FUL 18/06/2020 Approve with Conditions
Location: 20 Lodge Side, Bury, BL8 2SW
Proposal: Two/single storey extension at rear and conversion of garage to living accommodation

Application No.: 65526 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 62 Newington Drive, Bury, BL8 2DZ
Proposal: Loft conversion with front and rear dormer extensions; Extended drive and pavement crossing

Application No.: 65544 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 22 Haslam Hey Close, Bury, BL8 2LA
Proposal: Single storey rear extension

Ward: **Bury West - Elton**

Application No.: 65447 **App. Type:** FUL 25/06/2020 Approve with Conditions
Location: 89 Bankhouse Road, Bury, BL8 1DY
Proposal: Single storey side extension

Application No.: 65458 **App. Type:** FUL 03/07/2020 Approve with Conditions
Location: 270 Tottington Road, Bury, BL8 1SJ
Proposal: Conversion of existing dwelling to 2 no. flats

Application No.: 65528 **App. Type:** TEL 16/07/2020 Prior Approval Required and Refused
Location: Brandlesholme Road, Bury, BL8 1JN
Proposal: Proposed telecommunications installation: Proposed 20m Phase 8 Monopole C/W wrapround Cabinet at base and associated ancillary works

Ward: **North Manor**

Application No.: 65160 **App. Type:** FUL 08/07/2020 Approve with Conditions
Location: 939 Walmersley Road, Bury, BL9 5LL
Proposal: Demolition of existing dwelling and erection of 1 no. replacement dwelling with front boundary fence/gate

Application No.: 65435 **App. Type:** FUL 16/06/2020 Approve with Conditions
Location: 6 Greenmount Drive, Tottington, Bury, BL8 4HA
Proposal: Single storey extension at side/rear

Application No.: 65448 **App. Type:** FUL 24/06/2020 Split Decision
Location: 313 Holcombe Road, Tottington, Bury, BL8 4BB
Proposal: Proposal A: Two/single storey extension at rear

Proposal B: Change of use of land at side to extend residential curtilage; single storey extension at side; Formation of parking area at side and erection of 1.8 metre high boundary fence/gate

Application No.: 65474 **App. Type:** LBC 09/07/2020 Refused
Location: 35 Mount Pleasant, Nangreaves, Bury, BL9 6SR
Proposal: Listed building consent for 5 no. replacement timber window frames

Application No.: 65512 **App. Type:** FUL 03/07/2020 Approve with Conditions
Location: 162 Summerseat Lane, Ramsbottom, Bury, BL0 9TT
Proposal: Two storey extension at side; single storey front extension

Application No.: 65513 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 9 St Austell Drive, Tottington, Bury, BL8 4EY
Proposal: Single storey side extension and update to rear fenestration

Application No.: 65523 **App. Type:** FUL 13/07/2020 Approve with Conditions
Location: 9 Hillstone Close, Tottington, Bury, BL8 4EZ
Proposal: Dormer at front and and first floor rear extension; Render to external elevations and widening of existing driveway

Application No.: 65550 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: 7 Hawthorn Avenue, Ramsbottom, Bury, BL0 9UZ
Proposal: Single storey extension at rear

Application No.: 65551 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 36 Greenheys Crescent, Tottington, Bury, BL8 4QD
Proposal: Single storey front extension

Application No.: 65574 **App. Type:** LDCP 10/07/2020 Lawful Development
Location: 69 Fernview Drive, Ramsbottom, Bury, BL0 9XG
Proposal: Lawful development certificate for proposed part demolition of a single skin, brick built garage with lean-to wooden shed. To be replaced by wooden framed summer room with integral shed, uPVC doors and windows, with log-lap cladding. Flat roof (of the original garage) to be replaced by dual pitched roof. Brick wall at boundary to our adjacent neighbours (67 Fernview Dr), to be retained as part of the new structure.

Ward: **Prestwich - Holyrood**

Application No.: 65418 **App. Type:** FUL 24/06/2020 Approve with Conditions
Location: 25 Guest Road, Prestwich, Manchester, M25 3DJ
Proposal: Single storey extension at rear and installaton of new window openings to side elevation

Application No.: 65444 **App. Type:** FUL 24/06/2020 Approve with Conditions
Location: 98 Warwick Avenue, Whitefield, Manchester, M45 6TX
Proposal: Two storey extension at side: Front porch

Application No.: 65445 **App. Type:** FUL 22/06/2020 Approve with Conditions
Location: 14 Orford Road, Prestwich, Manchester, M25 3DB
Proposal: Single storey extension at rear including new flat roof across and 2no. flat roof lights

Application No.: 65453 **App. Type:** FUL 24/06/2020 Approve with Conditions
Location: 68 Heys Road, Prestwich, Manchester, M25 1JY
Proposal: Two/single storey rear extension; 0.31m high decking at rear

Application No.: 65502 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: 14 Guest Road, Prestwich, Manchester, M25 3DL
Proposal: Erection of boundary wall at rear

Application No.: 65531 **App. Type:** TEL 09/07/2020 Prior Approval Required and Refused
Location: Polefields/Bury Old Road, Prestwich, M25 1WJ
Proposal: The installation of a 20m monopole mast together with the installation of equipment cabinets and ancillary development thereto.

Ward: **Prestwich - Sedgley**

Application No.: 65016 **App. Type:** FUL 17/06/2020 Approve with Conditions
Location: 91 Albert Avenue, Prestwich, Manchester, M25 0LU
Proposal: Single storey extensions at rear

Application No.: 65307 **App. Type:** FUL 22/06/2020 Approve with Conditions
Location: 13 Windsor Road, Prestwich, Manchester, M25 0DZ
Proposal: Single storey extension at side and rear

Application No.: 65356 **App. Type:** FUL 09/07/2020 Approve with Conditions
Location: 59 Meade Hill Road, Prestwich, Manchester, M8 4LW
Proposal: Single storey rear extension; Single storey side extension; Front porch extension; Hip to gable roof extension with front and rear dormers; Outbuilding in rear garden

Application No.: 65404 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: 65 Sheepfoot Lane, Prestwich, Manchester, M25 0DN
Proposal: Garage conversion, porch at front, bay window at front, hip to gable roof extension and dormer at rear

Application No.: 65442 **App. Type:** FUL 03/07/2020 Approve with Conditions
Location: Broadfield Court, Park View Road, Prestwich, Manchester, M25 1QF
Proposal: Air source heat pump at roof level

Application No.: 65451 **App. Type:** FUL 26/06/2020 Approve with Conditions
Location: 36 Salisbury Drive, Prestwich, Manchester, M25 0HU
Proposal: Single storey extension at rear and loft conversion

Application No.: 65472 **App. Type:** FUL 01/07/2020 Approve with Conditions
Location: 21 Breeze Mount, Prestwich, Manchester, M25 0AH
Proposal: Single storey extension at side and rear

Application No.: 65510 **App. Type:** FUL 03/07/2020 Approve with Conditions
Location: 95 Bury Old Road, Prestwich, Manchester, M25 0FQ
Proposal: Single storey extension at side and rear

Application No.: 65527 **App. Type:** TEL 13/07/2020 Prior Approval Required and Granted
Location: Scholes Lane, Prestwich, Manchester, M25 0PD
Proposal: Proposed telecommunications installation: Proposed 20m Phase 8 Monopole C/W wraparound Cabinet at base and associated ancillary works

Ward: **Prestwich - St Mary's**

Application No.: 65420 **App. Type:** FUL 22/06/2020 Approve with Conditions
Location: 55 Duckworth Road, Prestwich, Manchester, M25 9GF
Proposal: Single storey rear extension and loft conversion with front and rear dormers

Application No.: 65481 **App. Type:** FUL 25/06/2020 Approve with Conditions
Location: 9A Highfield Road, Prestwich, Manchester, M25 3AW
Proposal: Loft conversion; Hip roof to gable with increase in roof height; additional windows to front, rear and sides

Application No.: 65548 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: 44 Prestwich Hills, Prestwich, Manchester, M25 9PY
Proposal: Single storey extension at rear

Ward: **Radcliffe - East**

Application No.: 61145 **App. Type:** OUT 25/06/2020 Approve with Conditions
Location: Station Works, Bury Road, Radcliffe, Manchester, M26 9UR
Proposal: Outline application for a mixed use development comprising two apartment blocks and employment building (Use class B1) with associated car parking, refuse and amenity space (all matters reserved)

Application No.: 65422 **App. Type:** FUL 25/06/2020 Approve with Conditions
Location: Land between 73 & 77 Starling Road, Radcliffe, Manchester, M26 4LW
Proposal: Erection of detached bungalow

Application No.: 65452 **App. Type:** FUL 25/06/2020 Approve with Conditions
Location: 157 Starling Road, Bury, BL8 2HF
Proposal: Single storey extension at rear, single storey bay window extension at side, new roofs to existing bay windows at front and alterations to driveway/vehicular access at front

Application No.: 65503 **App. Type:** FUL 08/07/2020 Approve with Conditions
Location: Land at Siddall Street, Radcliffe, Manchester, M26 4AX
Proposal: Variation of condition no. 2 (approved plans) of p/p 64256 to remove brick piers from front and rear elevations

Application No.: 65507 **App. Type:** FUL 09/07/2020 Approve with Conditions
Location: 20 Farcroft Avenue, Radcliffe, Manchester, M26 2XJ
Proposal: Demolition of existing rear lean-to conservatory and the erection of a single storey side and single storey rear extension.

Application No.: 65545 **App. Type:** FUL 09/07/2020 Approve with Conditions
Location: Tudor Close, 1 Tudor Grange, Radcliffe, Greater Manchester, M26 4AH
Proposal: Conversion of garage to living space with 2no. UPVC windows and door; New timber fence boundary elements; Millboard planters, pergola, shed, bin store and hardstanding/seating areas

Application No.: 65555 **App. Type:** FUL 10/07/2020 Approve with Conditions
Location: 7 Deansgate, Radcliffe, Manchester, M26 2SH
Proposal: Installation of new shop front

Ward: **Radcliffe - North**

Application No.: 65373 **App. Type:** FUL 18/06/2020 Approve with Conditions
Location: Land to rear of 41 Bury Old Road and west of Arthur Lane, Ainsworth, Bolton, BL2 5PF
Proposal: Demolition of existing stable building and erection of 1 no. dwelling

Application No.:	65376	App. Type:	FUL	16/06/2020	Approve with Conditions
Location:	Land off Brinks Lane, Radcliffe, Bolton, BL2 6QD				
Proposal:	Removal of existing temporary buildings and formation of hardstanding for loading and unloading of horses and erection of stables on concrete hardstanding				
Application No.:	65412	App. Type:	FUL	22/06/2020	Approve with Conditions
Location:	Ainsworth Nursing Home, Knowsley Road, Ainsworth, Bolton, BL2 5PT				
Proposal:	Conversion of wash house to 1 no. dwelling with single storey extensions and dormer to rear				
Application No.:	65468	App. Type:	FUL	18/06/2020	Approve with Conditions
Location:	39 Boundary Drive, Radcliffe, Bolton, BL2 6SE				
Proposal:	Proposed rear orangery and canopy				
Application No.:	65471	App. Type:	FUL	19/06/2020	Approve with Conditions
Location:	77 Boundary Drive, Radcliffe, Bolton, BL2 6SE				
Proposal:	Single storey rear extension				
Application No.:	65498	App. Type:	GPDE	24/06/2020	Prior Approval Not Required - Extension
Location:	7 Cockey Moor Road, Bury, BL8 2HD				
Proposal:	Prior notification of proposed single storey rear extension				

Ward: **Radcliffe - West**

Application No.:	65425	App. Type:	FUL	25/06/2020	Approve with Conditions
Location:	10 Sedgefield Road, Radcliffe, Manchester, M26 1YE				
Proposal:	First floor extension at side				
Application No.:	65462	App. Type:	LDCP	25/06/2020	Refused
Location:	52 Greendale Drive, Radcliffe, Manchester, M26 1UQ				
Proposal:	Lawful development certificate for proposed single storey side extension approx. 8.1m x 2.4m				
Application No.:	65480	App. Type:	FUL	08/07/2020	Approve with Conditions
Location:	Garage site, land off Coleridge Avenue, Radcliffe, Manchester, M26 3QU				
Proposal:	Variation of condition no. 2 (approved plans) of planning permission 62970: To allow new gates, repositioning of 1no. garage and the replacement of 10 no. garages with community garden				

Ward: **Ramsbottom + Tottington - Tottington**

Application No.:	65342	App. Type:	FUL	03/07/2020	Approve with Conditions
Location:	Land Adjacent to 1 Whittles Cottage, Turton Road, Tottington, Bury, BL8 3QQ				
Proposal:	Demolition of existing stables building and erection of a replacement stables building with open air riding arena and associated works				

Application No.: 65398 **App. Type:** FUL 17/06/2020 Approve with Conditions
Location: Land at side of 18 Chestnut Avenue, Tottington, Bury, BL8 3EE
Proposal: Amendment to planning permission 62019 for erection of 1 no. dwelling showing alternative parking arrangements and fencing to rear garden

Application No.: 65421 **App. Type:** FUL 26/06/2020 Refused
Location: Land at Turton Road, Tottington, Bury, BL8 3QQ
Proposal: Multi purpose agricultural building and hardcored agricultural track

Application No.: 65486 **App. Type:** PIP 15/06/2020 Refused
Location: Land at Watling Street (between Affetside Primary School & Mums Harris Farm), Affetside, Bury, BL8 3QR
Proposal: Application for permission in principle for up to 3 no. dwellings

Application No.: 65492 **App. Type:** FUL 30/06/2020 Approve with Conditions
Location: Mums Harris Farm, Watling Street, Tottington, Bury, BL8 3QR
Proposal: Demolition of existing single/two storey extension, conservatory and porch and erection of two/single storey extension at side; Conversion of attached barn to additional living accommodation with increase in barn roof height

Ward: **Ramsbottom and Tottington - Ramsbottom**

Application No.: 65394 **App. Type:** FUL 18/06/2020 Approve with Conditions
Location: Former Redisher Works, Holcombe Old Road, Ramsbottom, BL0 4NQ
Proposal: Substitution of house types to plots 2 and 7 to incorporate a single storey ground floor annex, following grant of p/p ref. 59715

Application No.: 65446 **App. Type:** FUL 16/06/2020 Approve with Conditions
Location: 41 Oak Avenue, Ramsbottom, Bury, BL0 9UY
Proposal: Single storey extension at side and rendering to all elevations with new window replacement and off street parking

Application No.: 65477 **App. Type:** FUL 07/07/2020 Approve with Conditions
Location: 10 Tagg Wood View, Ramsbottom, Bury, BL0 9XP
Proposal: Single storey extension at side/rear

Application No.: 65495 **App. Type:** FUL 08/07/2020 Approve with Conditions
Location: Higher Redisher Farm, Redisher Lane, Ramsbottom, Bury, BL8 4HX
Proposal: Domestic stables and store

Application No.: 65514 **App. Type:** FUL 09/07/2020 Approve with Conditions
Location: 3 Ada Street, Ramsbottom, Bury, BL0 9PH
Proposal: Single storey extension at side/rear

Ward: **Whitefield + Unsworth - Besses**

Application No.: 65379 **App. Type:** FUL 16/07/2020 Approve with Conditions
Location: Lord Clive Pub, 92 Mersey Drive, Whitefield, Manchester, M45 8LF
Proposal: Demolition of existing building and construction of a three storey block of apartments consisting of 27 no. units

Application No.: 65392 **App. Type:** FUL 17/06/2020 Approve with Conditions
Location: 140 Bury Old Road, Whitefield, Manchester, M45 6AT
Proposal: Change of use from estate agent (A2) to denture clinic (D1)

Application No.: 65419 **App. Type:** FUL 16/06/2020 Approve with Conditions
Location: 15 Hilltop Grove, Whitefield, Manchester, M45 8FQ
Proposal: Two/single storey extension at rear

Application No.: 65484 **App. Type:** LDCP 03/07/2020 Lawful Development
Location: 3 Balmoral Avenue, Whitefield, Manchester, M45 6AY
Proposal: Lawful development certificate for proposed erection of outbuilding

Application No.: 65499 **App. Type:** FUL 30/06/2020 Approve with Conditions
Location: 137 Thatch Leach Lane, Whitefield, Manchester, M45 6EP
Proposal: Single storey extension at front and single storey extension at rear

Ward: **Whitefield + Unsworth - Pilkington Park**

Application No.: 65370 **App. Type:** FUL 16/06/2020 Approve with Conditions
Location: 5 Meadow Croft, Whitefield, Manchester, M45 7ND
Proposal: First floor extension above existing garage with conversion of garage to living accommodation and alterations to car port to form car port/bin store; Reconfiguration of entrance hallway with new entrance porch; Render to external elevations

Application No.: 65377 **App. Type:** FUL 30/06/2020 Approve with Conditions
Location: 20 Woodhall Avenue, Whitefield, Manchester, M45 7QF
Proposal: Two/single storey extension at rear; Loft conversion with rear dormer

Application No.: 65426 **App. Type:** FUL 29/06/2020 Approve with Conditions
Location: 170 Bury New Road, Whitefield, Manchester, M45 6QF
Proposal: Change of use from restaurant/café (A3) to licenced premises (A4)

Application No.: 65440 **App. Type:** FUL 18/06/2020 Approve with Conditions
Location: 18 Mansion Avenue, Whitefield, Manchester, M45 7SS
Proposal: Single storey extension at rear

Application No.: 65463 **App. Type:** FUL 18/06/2020 Approve with Conditions
Location: 35 Ringley Chase, Whitefield, Manchester, M45 7UA
Proposal: Raise ridge height of height of roof with dormers and side extension to existing detached garage; New boundary wall and sliding entrance gate

Application No.: 65476 **App. Type:** FUL 24/06/2020 Approve with Conditions
Location: 18 Parkstone Avenue, Whitefield, Manchester, M45 7QH
Proposal: Raise ridge height of roof with loft conversion to existing bungalow

Application No.: 65547 **App. Type:** LDCP 09/07/2020 Lawful Development
Location: 351 Bury New Road, Whitefield, Manchester, M45 7SU
Proposal: Lawful development certificate for new single storey extension to the rear of the property

Ward: **Whitefield + Unsworth - Unsworth**

Application No.: 65439 **App. Type:** FUL 09/07/2020 Approve with Conditions
Location: Brick House Farm, Griffe Lane, Bury, BL9 8QS
Proposal: Change of house type of approved dwelling on plot 8 (p/p ref. 63895) with addition of single storey side and rear lean-to, use of basement for garage and enlargement of basement, conversion of attic to bedroom

Application No.: 65454 **App. Type:** TEL 19/06/2020 Prior Approval Required and Granted
Location: Bury New Road SW, Whitefield, Manchester, M45 7SY
Proposal: Installation of a 20m slim-line column supporting 6 no. antennas, 2 no. transmission dishes, 2 no equipment cabinets and ancillary development thereto including a GPS module and 3 no. Remote Radio Units (RRUs).

Application No.: 65505 **App. Type:** FUL 13/07/2020 Approve with Conditions
Location: 12 Bent Fold Drive, Bury, BL9 8NG
Proposal: Convert loft space into bedroom and bathroom with access via extended staircase; Gable end extension at side with flat roof dormer at rear of house.

Application No.: 65520 **App. Type:** FUL 15/07/2020 Approve with Conditions
Location: 50 Pilsworth Way, Bury, BL9 8RE
Proposal: Upgrade the existing gravel car park with new tarmacadam surfacing, surface water drainage, line marking and amenity lighting

Total Number of Applications Decided: **98**

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REPORT FOR NOTING

Agenda Item 6

Bury
COUNCIL

**Agenda
Item**

6

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	28 July 2020
SUBJECT:	PLANNING APPEALS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	<p>Planning Appeals:</p> <ul style="list-style-type: none"> - Lodged - Determined <p>Enforcement Appeals</p> <ul style="list-style-type: none"> - Lodged - Determined
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A

Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS

DIRECTOR:

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Committee of the Planning Appeals lodged against decisions of the authority and against Enforcement Notices served and those that have been subsequently determined by the Planning Inspectorate.

Attached to the report are the Inspectors Decisions and a verbal report will be presented to the Committee on the implications of the decisions on the Appeals that were upheld.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-

Contact Details:-

David Marno, Head of Development Management
Planning Services, Department for Resources and Regulation,
3 Knowsley Place ,Bury BL9 0EJ

Tel: 0161 253 5291

Email: d.marno@bury.gov.uk

**Planning Appeals Decided
between 15/06/2020 and 19/07/2020**



Application No.: 65364/P3KPA

Appeal Decision: Withdrawn

Decision level: DEL

Date: 14/07/2020

Recommended Decision: Prior Approval Required

Appeal type: Written Representations

Applicant: Mrs Alison Allcock

Location: 379 Bury New Road, Prestwich, Manchester, M25 1AW

Proposal: Prior approval for change of use from office space to childcare facility/nursery

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BURY COUNCIL
DEPARTMENT FOR BUSINESS, GROWTH AND INFRASTRUCTURE
PLANNING SERVICES

PLANNING CONTROL COMMITTEE

28 July 2020

SUPPLEMENTARY INFORMATION

Item:01 Land to the south of Moorbottom Road, Holcombe, Bury, BL8 4NS

Application No. 64786

Erection of agricultural storage building, retention of field shelter, pig arc and replacement gate & gate post

Publicity

Objection - 1 Lumb Carr Road, Holcombe

- The planning authority are required to take archaeology into account when determining planning applications. In this instance it is Greater Manchester Archaeological Advisory Service
- Animal husbandry regulations need to be met - adequate water supply, appropriate stock level for poor pasture.
- The applicant lives too remotely to breed animals.
- This application should be withdrawn and a field shelter, in a more appropriate, less unsightly position applied for.

Support - 39 Valley Mill Lane, Bury (x2)

- I am fully supportive of these changes that need to be made, to enable the animals to have shelter for winter and have dry food available and a safe loving environment.(just as humans hierarchy needs) .
- Having seen these animals they are well looked after and kept , and you can clearly see that it's not for profit but for the love , care and devotion of the animals , unlike a lot of places to be able to look out your window and see fields and animals I believe is a luxury not many have , we should embrace the gift we have been given of these wonderful animals and there surroundings.
- I think that this is a brilliant idea, I was pleased when I first saw the sheep and alpacas brought onto the land, and I totally support that as the seasons are changing a few instalments need to be made on the land to ensure the animals are kept safe and healthy during the colder months! I don't see how there could be any objection, the land owners keep to themselves and are pleasant, and do not use the animals for any personal gain and have them to truly look after them. Which I think is a wonderful thing, I don't see how anyone could object to ensuring animals have the correct facilities and care they need? Clearly the land owners are trying to do best by the animals which I can support!

Comments received from the agent in response to the objections received.

- Both the Applicants and I feel many of the representations received against this application have unduly influenced the planning committee by stating inaccurate information of the intentions of the Applicant. Quite a number of the objections the planning department have received don't relate to planning policy and are completely unfounded and untrue.
- For the avoidance of doubt, this will be a small private agricultural development comprising of a modest agricultural building. The Applicant's use of the land meets the definition of agriculture under Section 336 of the Town and Country Planning Act 1990 (grazing livestock). The agricultural building will solely be used for private housing of the Applicant's grazing livestock (sheep and alpacas) and their feed and bedding. The building will also provide secure storage of agricultural implements used to manage the agricultural land. It is as simple as that.
- Currently the Applicants have assembled various small temporary shelters on the land to house livestock (sheep when giving birth to lambs). Theses temporary buildings will be removed from the land if this application is approved.

- Many of the representations received alleged alternative uses such as a zoo, petting farm, residential property etc. These are completely unfounded and only serve to prejudice the determination of the application by the planning committee. It is a flaw in the planning system that members of the public can submit untrue and unfounded statements which potentially prejudice an application without any repercussions to them.
- The application site is located in the Green Belt and the Conversation area. Firstly, in Green Belt terms a building(s) for the purpose of agriculture is appropriate development (para 145 of the National Planning Policy Framework). Therefore, the development is appropriate development in the Green Belt. Secondly, policy EN2/2 (conversation area) doesn't prevent development in a conversation area it simply states that "Development within a Conservation Area will only be acceptable if it preserves or enhances the special character or appearance of the area". Through careful consultation with the planning officer and statutory consultees this development has been deemed not to have an adverse effect on the character or appearance of the conversation area by virtue of its size, appearance and design.
- It is important to note that the character and appearance of the application site is agriculture and this is an agricultural development modest in size therefore the purposed use of the building doesn't conflict with the use of the land.
- It has been insinuated by objectors that the building is enormous in size. I grew up a livestock farm in the Yorkshire Dale National Park (a conversation area itself) and worked in the agricultural industry for 15 years as a Chartered Surveyor and the proposed building of 12.19m x 6.09m (40ft x 20ft) is not a large agricultural building and appropriate to the need of the Applicant.
- Mr Swithenbank stated during the first planning committee meeting his desire that the land remained as agricultural land maintained by grazing. Any person with any agricultural knowledge or experience will tell you that livestock require shelter and a building to store their food in hence why agricultural buildings are an exception to development in the Green Belt - they are needed in rural areas which is often Green Belt. Without the provision of a building for shelter and protection from the elements, livestock (especially those giving birth to offspring) would be left out in the element (rain, snow extreme heat) without shelter and cause unnecessary suffering. I would suggest there are few if any livestock owners that don't have the provision of a building to keep their livestock safe and protected from extreme weather conditions.
- We are aware that almost any planning application in the vicinity of the application site receives strong objection from local residents and people from outside the area regardless of its nature. If you want the appearance of the landscape to remain as it is then agriculture plays a vital role and appropriate agricultural development to support the continue agricultural practices in the area must be supported.
- In summary:
 - This is an application for a modest agricultural building on agricultural land
 - Agricultural buildings are appropriate development in the Green Belt
 - The Applicant has worked with the planning department and agreed their suggestions in terms of siting, landscaping
 - The Planning department in recommending this application have carried out a thorough and robust assessment and found the proposal to comply with Green Belt and Conservation Policies
 - Many of the objections received are unfounded or don't relate to relevant Planning Policies
 - Any form of development has an impact on appearance and openness but that impact has been mitigate by accepting the suggestions made by the

- planning department
- If the committee decide against the recommendation of planning department it is likely a planning appeal will be lodged with a costs application.

Response to objections - all issues have been covered in the main report

Item:02 Asda Stores Ltd, Pilkington Way, Radcliffe, Manchester, M26 3DA
Application No. 64949
 Erection of drive thru coffee shop

Consultations

Traffic Section - No objections, subject to the inclusion of conditions relating to a construction traffic management plan, car parking and a delivery management plan.

Publicity

29 letters have been received from the occupiers of 46 Dingle Close, 75 Church Street, 12 Butterworth Street, 155 Outwood Road, 10 Coronation Gardens, 27 Holly Grove, 26 New Road, 1 Osborne Close, 1 Devonshire Place, 11 Great Hall Close, 46 Bury Street, 5 Mulbery Close, 134 Outwood Road, 39 Venns Road, 20 Grindsbrook Road, 99 Ferngrove, 29 Unsworth Street, 19 Naseby Walk, 16 The Crescent, 133 Christie Lane, 5 Bridgemere Close, 29 Chestnut Fold, 49 Shire Gardens, 27, 30 Chapeltown Road, 29 Bury Road, which have raised the following issues:

- Concern about the likely increase in traffic within the Asda site, which has one access.
- An increase in littering. There is already a considerable amount of litter from take away food in the area.
- Concern about the number of fast food outlets within Radcliffe competing for the same business. Many of these outlets encourage unhealthy eating and we should be trying to reduce obesity.
- I support smaller independent local businesses that do something for the people in the community. Local coffee shops are so much more than a place to grab a coffee - locals meet here, older residents get a chance to see and speak to other people.
- There are enough coffee shops in Radcliffe.
- We should support the local businesses.
- The site is next to McDonalds, which already offers coffee at its drive thru
- We don't want a big chain to take the business and take people away from the town centre.
- As the council is looking at ways to develop and breathe life into the centre of Radcliffe this proposal will only serve to draw people to Asda and to the drive through.
- I see no benefit to this coffee shop. Listen to the people who use and care about the town.
- You shouldn't drink and drive - you will spill coffee and burn yourself.

Response to objectors

- The issue of competition is not a material planning consideration and cannot be taken into consideration.
- The issues relating to traffic have been addressed in the main report.

Item:03 Margaret Haes Riding Centre, Moor Road, Ramsbottom, Bury, BL8 4NX
Application No. 64955

Retention of welfare unit with associated landscaping to form welfare and security accommodation
(Temporary consent for 5 years)

Publicity

Objections

1 Lumb Carr Road

- The Planning Application should be withdrawn - due to breach of both Conservation and Green Belt principles - for which there is no balance of need.

Higher Barn Farm

- I object to the relocation of the shipping container. The relocation drawings shows a clear lack of respect and understanding of the staff, volunteer's and most disappointedly the disabled. Creating a narrow corridor to the entrance of the unit by moving the 40 ft container closer to the outdoor arena is far from suitable for the disabled and their carers and the massive volume of the unit relocated in the revised position would create yet another corridor running along side a steep banking, this area in particular would be very difficult for staff etc to keep a watchful on whilst working in the main areas . I would consider these to be VERY SPECIAL material considerations that will assist the viability and user experience of a charity dedicated to young and disabled riders.
- Surely common sense will prevail !

Response to objections - all issues have been covered in the main report.

Item:04 Land at Green Street, Bury, BL8 1TF Application No. 65293

Erection of 3 no. dwellings including associated parking and groundworks

Consultations

Traffic Section - No objections, subject to the inclusion of conditions relating to a construction traffic management plan, car parking and a dilapidation survey.

Conditions

Therefore, conditions 9 and 10 should be amended and condition 11 should be added in relation to a dilapidation survey:

9. The car parking indicated on approved plan reference RAD/2115/20/2/RevA shall be demarcated as indicated, surfaced in a material, or incorporate measures, that prevents the discharge of surface water onto the adopted highway and be made available for use prior to the development hereby approved being occupied and thereafter maintained at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

10. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved by the Local Planning Authority and shall confirm/provide the following:

- Access route for construction traffic from the highway network and restricted to a

- size of vehicle that can be accommodated on Green Street that serves the site;
- Hours of operation and number in relation to construction vehicle movements;
- Parking on site of operatives' and construction vehicles together with storage on site of construction materials, including any requisite phasing of the development to accommodate this.
- Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

11. Notwithstanding the details shown indicatively on approved plan reference RAD/2115/20/2/RevA, the development hereby approved shall not be commenced unless and until a dilapidation survey of the footways and carriageways abutting the site have been submitted to the Local Planning Authority and the scope of the proposed highway works required as a result of the proposed development have been agreed on site with the Highway Authority. The works subsequently approved, incorporating the reconstruction of the Green Street footway abutting the site, the demarcation of the limits of the adopted highway and all associated highway remedial works required as a result of the proposed development and statutory undertakers connections to the dwellings, shall be implemented to a specification to be agreed and to the satisfaction of the Local Planning Authority prior to occupation of the development hereby approved.

Reason. To ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EN1/2 - Townscape and Built Design

Policy H2/1 - The Form of New Residential Development

Policy H2/2 - The Layout of New Residential Development.

Item:05 Radcliffe Metrolink Car Park, Spring Lane, Radcliffe, Manchester, M26 2ST Application No. 65354

The erection of an additional deck and ramp to form a second floor to car park, providing an additional 115 no. spaces; landscaping scheme and lighting

The application is deferred to the September Planning Control Committee meeting.

Item:06 Land off Claybank Drive, (off Victoria Street), Tottington, Bury Application No. 65459

Variation of condition no. 2 (approved plans) of planning permission 63275 to amend house on Plot 1 (north) by raising roof eaves/ridge by 600mm, addition of pitched roof

dormer to front elevation, additional windows to attic space and removal of hipped end to roof on west elevation.

Nothing further to report.

Item:07 Whitefield Metrolink, Stanley Road, Whitefield, Manchester, M45 8AB

Application No. 65465

Reconfiguration of the bus turning head and the erection of an additional deck and ramp to form a second floor to car park, providing in total an additional 123 no. spaces; landscaping scheme and lightning

The application is deferred to the September Planning Control Committee meeting.

Item:08 Land adjacent to 23 Meadway, Bury, BL9 9TY Application No. 65469

Reserved matters for layout, scale, appearance and landscaping approval following grant of Outline approval ref. 61369 for 2 no. detached dwellings

Nothing further to report

Item:09 31 Brookfield Avenue, Radcliffe, Bolton, BL2 5QH Application No. 65569

Change of use from dwellinghouse (Class C3) to residential care home (Class C2) with additional parking.

Publicity

For the avoidance of any doubt, at the time of the publication of the committee report, the following representations had been received:

- 1 letter of support from the occupier of 70 Church Street;
- A petition objecting to the proposal from 6 signatories
- A total of 44 letters objecting to the scheme from the occupiers of 1, 7, 19, 21, 25, 27, 29, 33, 35 and 37 Brookfield Avenue, 17, 18, 26, 28, 31, 32, 33, 35, 37, 39 and 41 Newquay Avenue, 14, 16, 22, 24, 26, 69A, 75 and 82 Church Street, 26 Harley Avenue, 36 and 38 Broomfield Close, 2 and 4 Kingsbridge Avenue, 37 Moorside, 1 Barnsdale Close, 7 Metcalfe Terrace, 1 Churchill Avenue, 11 Deansgate, 1A Devon Drive, 1 Edgeworth Avenue, 6 Thurlestone Avenue, 16 Bradley Fold Road and Ainsworth Community Association.
- The occupier of No. 35 Brookfield Avenue has submitted a letter from his local MP, Christian Wakeford in support of his objection.

Following the publication of the committee report, 3 letters have been received from the occupiers of 35 Newquay Avenue, which have raised the following issues:

- I find it odd that Children's Services did not respond in relation to the Lowercroft Road facility and I think that there might be a conflict of interests should Children's Services be making use of the services provided by 4pureheart. Why would they respond to one and not the other, especially as staff are working from home currently?
- Children's Services go to some lengths to indicate that they have never used the provider and have no connection to them. This seems to be something of an overkill response given that the only public reference to a "potential conflict of interest" was one sentence made by myself. Again, I find myself contemplating; if this is their response, what was the question asked of them?
- It is clear that Children's Services have made no attempt to look into the

background of 4pureheart or the Ofsted reports. Children's Services and Bury Council may consider the response to be suitably diligent, but I do not, I find it pathetic.

- It appears in this instance that everyone is kicking the can down the street towards Ofsted and no one is prepared to look into the issues themselves as it is Ofsted that is accountable. What a gutless attitude. Especially after the death of Victoria Climbié 20 years ago exposed this and was supposed to be resolved.
- Personally, given the severity of the Ofsted reports I was surprised that the Bolton facility was allowed to continue operating. "Inadequate" is too small a word to describe their findings. Have you yourself read the Ofsted reports? Does this not concern you in the slightest?
- The information on the website with regard to the planning process is insufficient and lacking in transparency. I want full information regarding the meeting.
- I note that the number of constraints on the website has increased to 9. Attaching these at a later date may give the appearance that a level of due diligence has been undertaken. Are these policies available to the general public?
- What procedures/policies are in place for the LPA to verify and monitor that these constraints are being adhered to?
- How would this development be enforced? I cannot find any specific documents within the UDP that detail the requirements of the constraints.
- GM Police provided comments on how the facility can be protected from crime being committed against it. This does not reflect the concerns from residents and I wonder what question was put to them?
- Anything stated by the applicant is treated as fact, so I will paraphrase - From the response of GM Police it can be assumed that the perceived threat of crime and antisocial behaviour arises from only the residents of Ainsworth. This is an unfair stigmatisation of the marginalised elderly and infirm residents of the area. Just because they are elderly and infirm it does not automatically mean that they will perpetrate crimes against the care home.

Response to objectors

- The Local Planning Authority (LPA) has to assess the land use of the proposed use and its impacts upon the wider environment. The planning acts are clear that the planning system should not replicate conditions or controls of other legislation, but should trust the regulatory bodies to act. In this case, the Local Planning Authority is not kicking the can down the street, but allowing the appropriate bodies, Children's Services and Ofsted, to monitor and manage the facility in accordance with their rules and regulations. Should Ofsted, and/or the Local Authority Children's services, find a problem with the care home, it would take the necessary action which could, in extreme circumstances, include closing the home.
- Where relevant to the assessment of the planning application, the issues raised by the objectors have been addressed in the main report. Although there may be a high proportion of elderly people in the area, government guidance (paragraph 61 of the NPPF) suggests that a suitable mix of tenures and residents is preferable to concentrations of similar groups in a particular area. In addition to elderly people within the area, there are also families and single person households.
- Any conditions attached to the proposed development would be enforced by the Enforcement Section in accordance with the Enforcement Charter.
- Children's Services were consulted and invited to comment as a consultee and received the same letter/e-mail as any other consultee.
- The operation of a facility in Bolton is for Bolton Council and Ofsted to monitor and manage.

- All reports and plans are available on the website for scrutiny. The Council has been open and transparent about the application. The objector has received notice of the meeting.
- The constraints referred to are the relevant policies from the UDP, SPD and NPPF. These are all publicly available.
- GM Police were consulted with the same letter as any other consultee.
- Neither the applicant or the Local Planning Authority have suggested that the elderly (or any) residents of Ainsworth would perpetrate crimes against the care home.

Item:10 Land at rear of 27 Duckworth Road, Prestwich, Manchester, M25 9GF
Application No. 65589

Variation of condition 2 (approved plans) of planning permission 63374 to remove ash tree (T2)

Publicity

5 letters have been received from the occupiers of 26, 28, 112 Lowther Road and 6, 7, Lowther Close, which have raised the following issues:

- After speaking with a building surveyor, it seems that the removal of a row of holly trees in an embankment were removed between the properties of 26 Lowther Road and 27 Duckworth Road before the planning approval has led to subsidence at 26 Lowther Road.
- I wonder what further damage could happen if the extremely large ash tree with its established root system is removed.
- Such beautiful old trees should be protected.
- The removal of the tree is completely out of character with an area known for its greenery and diversity in trees and wildlife. Its removal would be detrimental to the area's aesthetics and character
- The tree has been in place for many years, where as residents come and go. It would be short sighted to approve this application and set a destructive precedent for others.
- God knows what damage it will cause to the adjoining properties. They have already severely damaged one house and from what I hear Bury Council is not interested in that damage. The worst council in Greater Manchester I think.
- I would question the environmental integrity of the Council if this needless and reckless application is approved.

Response to objectors

- The issues of how the proposed dwelling is constructed is a matter for the Building Regulations and is not a material planning consideration.
- The issue of how the property can be built safely is a matter for the Health and Safety Executive and is not a material planning consideration.
- The issues relating to the loss of the tree and the impact upon the character of the area have been addressed in the main report.
- Condition 11 requires a replacement tree to be planted in the front garden, which will contribute to the visual amenity of the area.

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